



7275 W. MAIN STREET, KALAMAZOO, MI 49009-9334
269-216-5220 Fax 375-7180
www.oshtemo.org

**NOTICE
OSHTEMO CHARTER TOWNSHIP
PLANNING COMMISSION - REGULAR MEETING**

**MEETING WILL BE HELD IN PERSON
AT OSHTEMO TOWNSHIP HALL
7275 W MAIN STREET**
Masks Are Optional in Oshtemo Township Buildings

(Meeting will be available for viewing through <https://www.publicmedianet.org/qavel-to-qavel/oshtemo-township>)

**THURSDAY, DECEMBER 14, 2023
6:00 P.M.**

AGENDA

1. Welcome and Call to Order
2. Pledge of Allegiance
3. Approval of Agenda
4. Public Comment on Non-Agenda Items
5. Approval of Minutes: November 27, 2023
6. **Public Hearing – Rezoning – 9177 West G Avenue**
James and Robert Endres, on behalf of the James A and Marilyn C Endres Trust, are requesting to rezone approximately 15 acres of 9177 West G Avenue from AG: Agricultural District to RR: Rural Residential District.
7. **Public Hearing – Special Use and Site Plan – Long John Silvers**
Long John Silvers is requesting site plan and special exception use approval to redevelop 5481 W Main Street with one 2,288 square foot building to serve as a fast food restaurant with drive-through.
8. **Public Hearing – Ordinance Amendment – Keeping of Livestock and Honeybees**
Consideration to adopt amendments to 57.80 - Keeping of Livestock and Honey Bees for recommendation to the Township Board.
9. Work Session:
 - a. Continued Discussion: Private Street Ordinance (Oshtemo Streets and Mobility Ordinance)
10. Other Updates and Business
11. Adjournment

**Policy for Public Comment
Township Board Regular Meetings, Planning Commission & ZBA Meetings**

All public comment shall be received during one of the following portions of the Agenda of an open meeting:

- a. Citizen Comment on Non-Agenda Items or Public Comment – while this is not intended to be a forum for dialogue and/or debate, if a citizen inquiry can be answered succinctly and briefly, it will be addressed or it may be delegated to the appropriate Township Official or staff member to respond at a later date. More complicated questions can be answered during Township business hours through web contact, phone calls, email (oshtemo@oshtemo.org), walk-in visits, or by appointment.
- b. After an agenda item is presented by staff and/or an applicant, public comment will be invited. At the close of public comment there will be Board discussion prior to call for a motion. While comments that include questions are important, depending on the nature of the question, whether it can be answered without further research, and the relevance to the agenda item at hand, the questions may not be discussed during the Board deliberation which follows.

Anyone wishing to make a comment will be asked to come to the podium to facilitate the audio/visual capabilities of the meeting room. Speakers will be invited to provide their name, but it is not required.

All public comment offered during public hearings shall be directed, and relevant, to the item of business on which the public hearing is being conducted. Comment during the Public Comment Non-Agenda Items may be directed to any issue.

All public comment shall be limited to four (4) minutes in duration unless special permission has been granted in advance by the Supervisor or Chairperson of the meeting.

Public comment shall not be repetitive, slanderous, abusive, threatening, boisterous, or contrary to the orderly conduct of business. The Supervisor or Chairperson of the meeting shall terminate any public comment which does not follow these guidelines.

(adopted 5/9/2000)
(revised 5/14/2013)
(revised 1/8/2018)

Questions and concerns are welcome outside of public meetings during Township Office hours through phone calls, stopping in at the front desk, by email, and by appointment. The customer service counter is open from Monday-Thursday, 8 a.m.-1 p.m. and 2-5 p.m., and on Friday, 8 a.m.–1 p.m. Additionally, questions and concerns are accepted at all hours through the website contact form found at www.oshtemo.org, email, postal service, and voicemail. Staff and elected official contact information is provided below. If you do not have a specific person to contact, please direct your inquiry to oshtemo@oshtemo.org and it will be directed to the appropriate person.

Oshtemo Township Board of Trustees		
<u>Supervisor</u>		
Cheri Bell	216-5220	cbell@oshtemo.org
<u>Clerk</u>		
Dusty Farmer	216-5224	dfarmer@oshtemo.org
<u>Treasurer</u>		
Clare Buszka	216-5260	cbuszka@oshtemo.org
<u>Trustees</u>		
Neil Sikora	760-6769	nsikora@oshtemo.org
Kristin Cole	375-4260	kcole@oshtemo.org
Zak Ford	271-5513	zford@oshtemo.org
Michael Chapman	375-4260	mchapman@oshtemo.org

Township Department Information			
<u>Assessor:</u>			
Kristine Biddle	216-5225	assessor@oshtemo.org	
<u>Fire Chief:</u>			
Greg McComb	375-0487	gmccomb@oshtemo.org	
<u>Ordinance Enforcement:</u>			
Rick Suwarsky	216-5227	rsuwarsky@oshtemo.org	
<u>Parks Director:</u>			
Vanessa Street	216-5233	vstreet@oshtemo.org	
Rental Info	216-5224	oshtemo@oshtemo.org	
<u>Planning Director:</u>			
Iris Lubbert	216-5223	ilubbert@oshtemo.org	
<u>Public Works Director:</u>			
Anna Horner	216-5228	ahorner@oshtemo.org	

OSHTEMO TOWNSHIP PLANNING COMMISSION

DRAFT MINUTES OF A SPECIAL MEETING HELD NOVEMBER 27, 2023

Agenda

PUBLIC HEARING – OSHTEMO TOWNSHIP HOUSING PLAN 2023

Consideration to approve the Oshtemo Township Housing Plan 2023 for incorporation into the Township’s Master Plan, for recommendation to the Township Board.

A special meeting of the Oshtemo Charter Township Planning Commission was held Monday, November 27, 2023, commencing at approximately 6:00 p.m. at the Oshtemo Township Hall, 7275 West Main Street.

ALL MEMBERS

WERE PRESENT:

Phil Doorlag, Chair
Deb Everett
Zak Ford, Board Liaison
Scot Jefferies
Micki Maxwell, Vice Chair
Scott Makohn
Alistair Smith

Also present: Iris Lubbert, Planning Director, Jim Porter, Township Attorney, and Emily Petz of the W.E. Upjohn Company.

Curt Aardema of AVB and another guest were also present.

Call to Order and Pledge of Allegiance

Chairperson Doorlag called the meeting to order at 6:00 p.m. and invited those present to join in the Pledge of Allegiance.

Approval of Agenda

Chairperson Doorlag determined there were no changes to the agenda and asked for a motion.

Mr. Ford **made a motion** to approve the Agenda as presented. Mr. Jefferies **seconded the motion**. The **motion was approved** unanimously.

Public Comment on Non-Agenda Items

There were no comments on non-agenda items.

Approval of Minutes: November 16, 2023

Chairperson Doorlag asked if there were additions, deletions, or corrections to the Minutes of the Meeting of November 16, 2023.

Hearing none, he asked for a motion.

Mr. Ford **made a motion** to approve the Minutes of the Meeting of November 16, 2023 as presented. Ms. Maxwell **seconded the motion**. The **motion was approved** unanimously.

Chairperson Doorlag moved to the next agenda item on the agenda.

PUBLIC HEARING – OSHTEMO TOWNSHIP HOUSING PLAN 2023
Consideration to approve the Oshtemo Township Housing Plan 2023 for incorporation into the Township’s Master Plan, for recommendation to the Township Board.

Ms. Emily Petz of the W.E. Upjohn Institute for Employment Research reviewed the final draft of the Oshtemo Township Housing Plan 2023 for consideration of approval. The housing plan addressed existing housing conditions within the community, as well as suggested strategies to improve current conditions and address future housing needs. The project consisted of four phases: project initiation and establishment of baseline data, building alignment, plan research and design, and lastly plan delivery and dissemination.

She distributed and reviewed for Commissioners the final draft of the Oshtemo Township Housing Plan 2023, which covers a housing needs assessment, market demands, results from the Oshtemo Township Housing Survey, and the Oshtemo portion of the countywide housing survey, and goals and objectives. She noted community and committee engagement helped to guide the direction and focus.

In order to ensure the Housing Plan is supported to the fullest extent, it will be incorporated as part of the Township’s Master Plan. To accomplish this goal, the plan must go through the state mandated public hearing process for community master plans, which includes a public hearing before both the Planning Commission and Township Board.

Per state regulations, neighboring jurisdictions were notified of the Township’s intent to create and adopt a housing plan. When the draft plan was completed, they were provided a 63-day opportunity to provide input on the plan. No letters or emails with feedback were received during the comment period.

Chairperson Doorlag thanked Ms. Petz for her presentation and asked whether Commissioners had any questions or comments.

Mr. Smith asked if the final draft was any different than the draft that was sent out for the 63 day opportunity for input.

Ms. Lubbert said it was the same document, with the exception of minor changes which were mostly the correction of typographical errors.

Ms. Everett commented the goals and objectives of the plan are good, but some seem to imply we can accomplish all of them now. As that can’t be done, she wondered if there should be a disclaimer to that effect.

Ms. Lubbert indicated a disclaimer that addresses her concern was added at the beginning of the “Goals” section.

Attorney Porter said it will take multiple steps to implement the goals of the Plan over a substantial period of time. Some goals and objectives can be addressed within the next five years; changing the zoning ordinance will achieve a great deal.

Chairperson Doorlag thanked Ms. Petz for her presentation, opened the meeting for a public hearing and asked if any member of the public wished to comment.

Mr. Curt Aardema, AVB, congratulated the Commission on their long journey to produce a great document and felt it represented a good approach. He appreciated the opportunity to participate during the process and agreed the zoning component will be important in order to meet many of the objectives of the Plan. He would like to see the Township move forward and noted AVB has some great models that could be used in Oshtemo Township.

Hearing no further comments, the Chair closed the public hearing and asked for a motion.

Mr. Ford **made a motion** to approve the Oshtemo Township Housing Plan 2023 as presented for incorporation into the Township's Master Plan, and to forward it with a recommendation for approval to the Township Board. Ms. Maxwell **seconded the motion**. The **motion was approved** unanimously.

OTHER UPDATES AND BUSINESS

There were no other updates or business to be considered.

ADJOURNMENT

With there being no further business to consider, Chairperson Doorlag adjourned the meeting at approximately 6:42 p.m.

Minutes prepared:
November 29, 2023

Minutes approved:
_____, 2023

[This page left intentionally blank]



December 5, 2023

Mtg Date: December 14, 2023

To: Planning Commission

From: Leeanna Harris, Zoning Administrator

Applicant: James and Robert Endres

Owner: James A and Marilyn C Endres Trust

Property: 9177 West G Avenue, Parcel Number 3905-05-230-012

Zoning: AG: Agricultural District

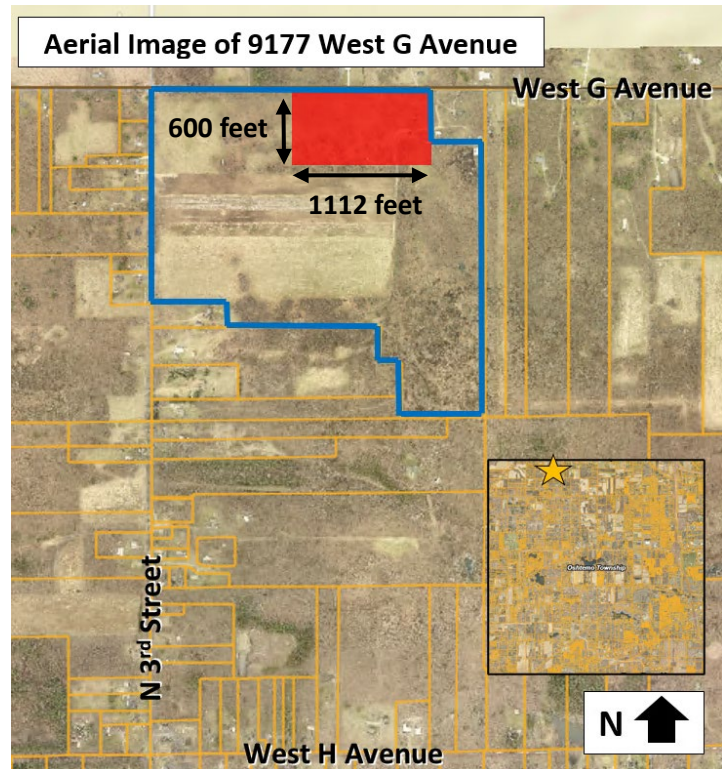
Request: Rezone approximately 15 acres of parcels to RR: Rural Residential

Section(s): Article 5 — Rural Residential District
Article 4 — Agricultural District

PROJECT SUMMARY:

The applicants, James and Robert Endres, on behalf of the James A and Marilyn C Endres Trust, are requesting to rezone approximately 15 acres of 9177 West G Avenue from AG: Agricultural District to RR: Rural Residential District. The rezoning area, highlighted in red to the right, under consideration begins approximately 1,112 feet from the west property line and is about 1,112 feet wide and 600 feet deep. The parent parcel is outlined in blue to the right. The parent parcel has a property area of approximately 120 acres with roughly 1,700 feet of frontage on North 3rd Street and about 2,500 feet on West G Avenue. 9177 West G Avenue currently serves an agricultural land use.

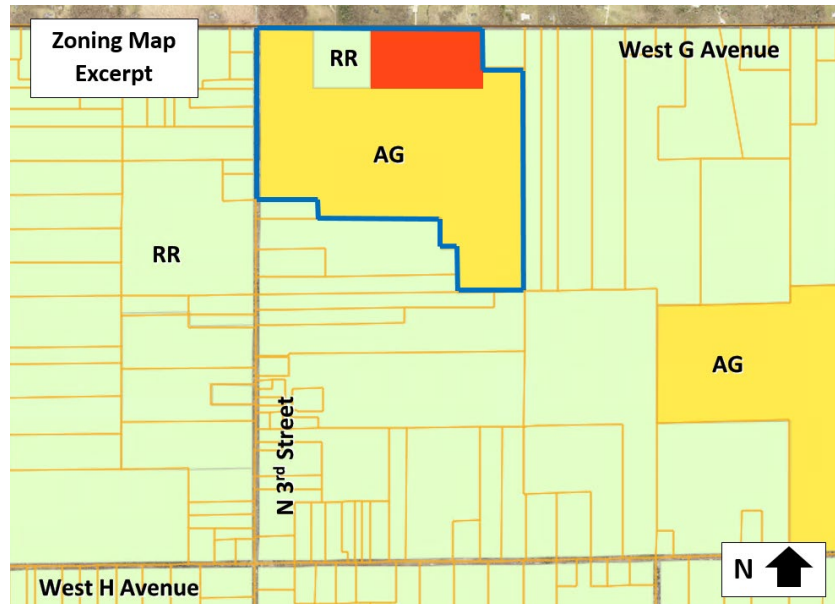
The applicants' intent is to rezone the desired area and to split these 15 acres from the parent parcel to create two new parcels. It should be noted that this parent



parcel has gone through a previous rezoning request in 2020 and is proposing to continue two of the divisions as outlined in the survey document attached. However, due to the minimum area requirement of 40 acres for AG: Agricultural District properties, a split would not be possible without the rezoning to RR: Rural Residential. Staff completed a preliminary review of the proposed land division and determined it would meet Township requirements for newly created parcels within the RR: Rural Residential zoning classification; however, since the request is for a rezoning, those materials will be formally considered at the time the applicant submits a land division application for Township review and approval.

OVERVIEW:

Currently zoned AG: Agricultural District, the subject property is located on the south side of West G Avenue, between North 3rd Street and N 6th Street. The subject property in question is outlined in dark blue in the Zoning Map to the right, with the requested rezoning area highlighted in red. Properties immediately surrounding the site currently possess agricultural and residential land uses. RR: Rural Residential zoning abuts the subject site to the east, west, and south. This property previously had two portions (approximately 11 acres) rezoned in 2020 to RR: Rural Residential; 3.86 acres were divided from the parent parcel while the remaining 7.66 acres are still within the parent parcel. Detached structures, farmland, and dense vegetation currently occupy the site. Approximately one-quarter of a mile to the southeast is another AG: Agricultural District zoned property.

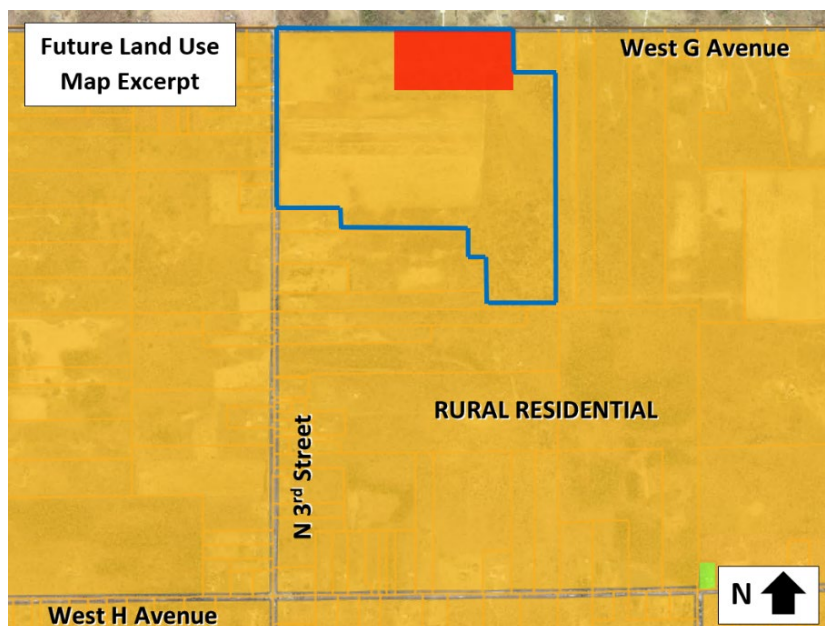


CONSIDERATIONS:

The Zoning Enabling Act, which allows Townships to zone property, does not provide any required standards that a Planning Commission must consider when reviewing a rezoning request. However, there are some generally recognized factors that should be deliberated before a rezoning decision is made. These considerations along with staff’s analysis are outlined below and in the following pages.

1. Master Plan Designation

The Township’s adopted 2017 Future Land Use Map designates 9177 West G Avenue and the surrounding properties in this portion of the Township as RR: Rural Residential, shown in orange on the map to the right, with the subject parcel outlined in blue and the area in question shown in red. According to the Township’s Master Plan, Rural Residential includes developments such as low-density housing on scattered sites. The Future Land Use Map identifies that the subject property is within an area that is intended to transition to Rural Residential from its present agricultural zoning. The



proposed rezoning would be consistent with the site’s current activity, Future Land Use Map, and the Township’s Zoning Ordinance. Rezoning the subject site would follow the Township’s vision in terms of land use for this area.

2. Consistency of the Zoning Classification in the General Area

Rezoning 15 acres of 9177 West G Avenue from AG: Agricultural District to RR: Rural Residential District would be consistent with current zoning in nearby parcels. The site is currently adjacent to several other properties which serve low density land uses. These uses of land include single-family dwellings as well as areas used for farmland.

3. Consistency and Compatibility with General Land Use Patterns in the Area

On both sides of West G Avenue, a pattern of residences or farmlands have been established. With the request being consistent and compatible with land use patterns in the area, and this use being in accordance with the Future Land Use map, the rezoning will allow the property to match the established character of the area.

4. Utilities and Infrastructure

Neither public water nor sewer are present along N 3rd Street or West G Avenue in this part of the Township, and the extension of such is not planned at any point in the near future. Given the anticipated low intensity residential use for the area subject to this rezoning request and the nature of uses permitted within the RR district, the absence of utilities here should not negatively affect reasonable land use. Regarding the transportation network, the intersection to the west at West G Avenue and North 3rd Street is a 4-way stop. The next intersection to the east where West G Avenue crosses North 6th Street is a 4-way stop as well. Staff is of the opinion that if this rezoning is approved, it will not negatively affect the area or add undue burden to the existing infrastructure than what is currently experienced.

5. Reasonable Use under Current Zoning Classification

The property is currently zoned AG: Agricultural District. The existing subject parcel is actively farmed and can continue according to the Michigan Right-to-Farm Act. Given the amount of road frontage and acreage present, as well as the restrictions currently in place for AG zoned properties, reasonable use is still possible without the rezoning. However, given the applicant’s desire to complete a land division in order to accommodate two single-family homes and the Township’s minimum 40-acre size for agriculturally zoned parcels, these opportunities are limited. The rezoning achieves the Township’s goals for this area and fits the applicant’s vision for the site.

6. Effects on Surrounding Property

Staff do not anticipate any negative impacts occurring onto surrounding properties if the subject site is rezoned to the RR: Rural Residential District. All properties adjacent to the subject site are similar as each property consists of an existing primary residence, farmland, or a combination of these. Although the applicants indicated that single-family homes would be built on the rezoned area, the agricultural use of the parent property would not be changing. With there being an existing pattern of rural character and uses in the area, the rezoning would complement adjacent zoning districts and their present land uses.

RECOMMENDATION:

Staff recommends that the Planning Commission forward a recommendation of approval to the Township Board for the rezoning of the area of 9177 West G Avenue noted in this staff report from the AG: Agricultural District to the RR: Rural Residential District for the following reasons:

1. The proposed rezoning is in accordance with the Township's Future Land Use Plan.
2. The requested rezoning is consistent and compatible with the surrounding land uses and adjacent zoning classifications.
3. Existing infrastructure and utilities can easily accommodate the expected future residential land use.
4. Township staff do not anticipate any negative impacts on surrounding properties as a result of the rezoning.

Attachments: application, survey, Article 4 – Agricultural District, Article 5 – RR Rural Residential District.



7275 W. Main Street, Kalamazoo, Michigan 49009-9334
Phone: 269-375-4260 Fax: 269-375-7180

PLEASE PRINT

PROJECT NAME & ADDRESS _____

PLANNING & ZONING APPLICATION

Applicant Name: James & Robert Endres
Company: _____

Address: 9177 West G Ave
Kalamazoo MI 49009

E-mail: Jendres1956@comcast.com

Telephone: 269-370-4701 Fax: _____

Interest in Property: _____

OWNER*:

Name: James A & Marilyn C Endres
Address: Trust

E-mail: _____

Phone & Fax: _____

Fee Amount _____
Escrow Amount _____

NATURE OF THE REQUEST: (Please check the appropriate item(s))

- Pre-Application Review
- Site Plan Review – I088
- Administrative Site Plan Review – I086
- Special Exception Use – I085
- Zoning Variance – I092
- Site Condominium – I084
- Accessory Building Review – I083
- Rezoning – I091
- Subdivision Plat Review – I089
- Interpretation – I082
- Other: _____

BRIEFLY DESCRIBE YOUR REQUEST (Use Attachments if Necessary): _____

Looking to Rezone two parcels E & F from AG to R1R
Parent Parcel
See Attached

LEGAL DESCRIPTION OF PROPERTY (Use Attachments if Necessary):

Sec Attached A

PARCEL NUMBER: 3905- 05-230-012

ADDRESS OF PROPERTY: 9177 West G Ave Kalamazoo MI 49009

PRESENT USE OF THE PROPERTY: Farming

PRESENT ZONING: Ag **SIZE OF PROPERTY:** Approx: 120 acres

NAME(S) & ADDRESS(ES) OF ALL OTHER PERSONS, CORPORATIONS, OR FIRMS HAVING A LEGAL OR EQUITABLE INTEREST IN THE PROPERTY:

Name(s)	Address(es)
<u>James & Marilyn Trust</u>	<u>9177 West G Ave Kal. MI 49009</u>

SIGNATURES

I (we) the undersigned certify that the information contained on this application form and the required documents attached hereto are to the best of my (our) knowledge true and accurate. I (we) acknowledge that we have received the Township's Disclaimer Regarding Sewer and Water Infrastructure. By submitting this Planning & Zoning Application, I (we) grant permission for Oshemo Township officials and agents to enter the subject property of the application as part of completing the reviews necessary to process the application.

<u>James Endres - Trustee</u>	<u>10-12-23</u>
<u>Wallace Endres - Trustee</u>	<u>10-12-23</u>

Owner's Signature (*If different from Applicant)

Date

James Endres

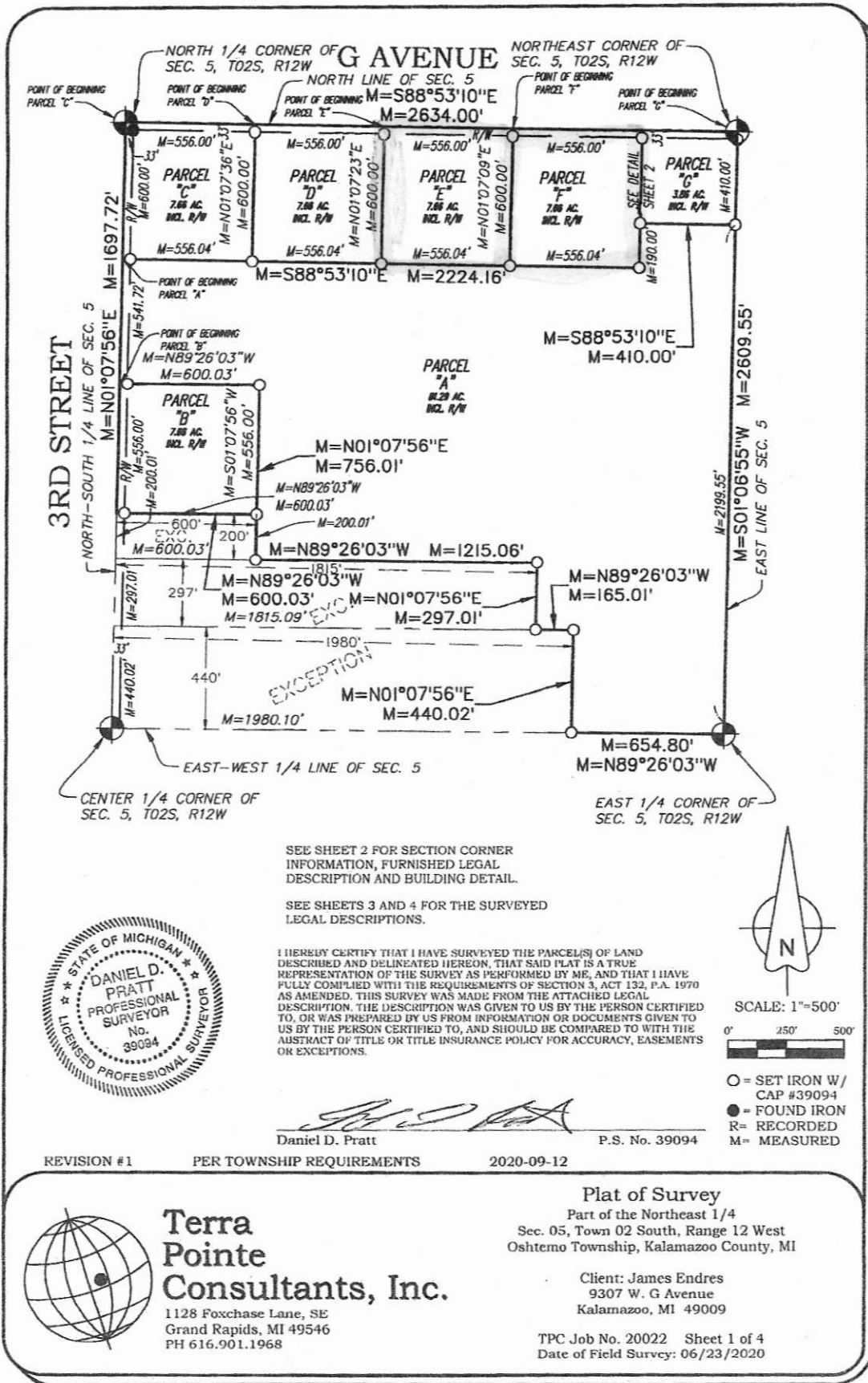
10-12-23

Applicant's Signature

Date

- Copies to:
- Planning - 1
- Applicant - 1
- Clerk - 1
- Deputy Clerk - 1
- Attorney - 1
- Assessor - 1
- Planning Secretary - Original

PLEASE ATTACH ALL REQUIRED DOCUMENTS



[Signature]
Daniel D. Pratt P.S. No. 39094

REVISION #1 PER TOWNSHIP REQUIREMENTS 2020-09-12



Terra Pointe Consultants, Inc.
1128 Foxchase Lane, SE
Grand Rapids, MI 49546
PH 616.901.1968

Plat of Survey
Part of the Northeast 1/4
Sec. 05, Town 02 South, Range 12 West
Oshtemo Township, Kalamazoo County, MI

Client: James Endres
9307 W. G Avenue
Kalamazoo, MI 49009

TPC Job No. 20022 Sheet 1 of 4
Date of Field Survey: 06/23/2020

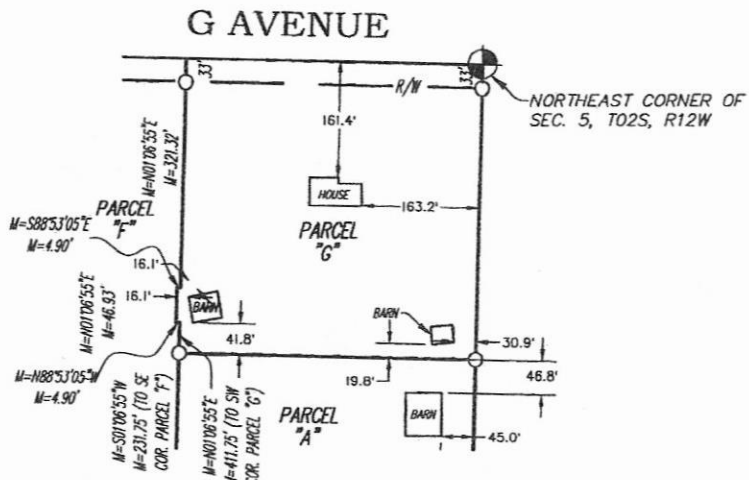
FURNISHED LEGAL DESCRIPTION:

Parcel of land situate and being in the Township of Oshtemo, County of Kalamazoo and State of Michigan, and described as follows:

The Northeast quarter of Section 5, Town 2 South, Range 12 West, EXCEPTING the South 26 2/3 rods of the West 120 rods of said Northeast quarter of section 5, and also EXCEPTING the North 18 rods if the South 44 2/3 rods of the West 110 rods of said Northeast quarter of section 5, and also EXCEPTING the North 200 feet of the South 937 feet of the West 600 feet of said Northeast quarter of section 5, and SUBJECT to public road over the Westerly 33 feet of said parcel and SUBJECT to public road of the North 33 feet of said parcel.

Computed area 123.45 acres, including road right of way.

SECTION CORNER INFORMATION:			
SOUTHWEST COR. SEC 28, T7N, R9W (E-11)			
FOUND KENT CO. MONUMENT			
FD. REMON N&T NE/S OAK	S21 E	106.44'	
FD. REMON N&T E/S P. POLE	N28°W	72.11'	
FD. REMON N&T SW/S FCE POST	N27°E	70.24'	
FD. REMON N&T SE/S FCE POST	S55°W	58.92'	
FD. REMON N&T SW/S OAK	N57°W	59.97'	
WEST 1/4 COR. SEC 28, T7N, R9W (E-10)			
FOUND KENT CO. MONUMENT			
FD. REMON N&T E/S OAK	N31 W	44.20'	
FD. REMON N&T SW/S OAK	S44°E	20.50'	
FD. REMON N&T N/S OAK	N84°W	38.87'	
FD. REMON N&T SE/S PINE	S03°E	58.54'	
FD. REMON N&T W/S TW OAK	S03°E	58.16'	



I HEREBY CERTIFY THAT I HAVE SURVEYED THE PARCEL(S) OF LAND DESCRIBED AND DELINEATED HEREON, THAT SAID PLAT IS A TRUE REPRESENTATION OF THE SURVEY AS PERFORMED BY ME, AND THAT I HAVE FULLY COMPLIED WITH THE REQUIREMENTS OF SECTION 3, ACT 132, P.A. 1970 AS AMENDED. THIS SURVEY WAS MADE FROM THE ATTACHED LEGAL DESCRIPTION. THIS DESCRIPTION WAS GIVEN TO US BY THE PERSON CERTIFIED TO, OR WAS PREPARED BY US FROM INFORMATION OR DOCUMENTS GIVEN TO US BY THE PERSON CERTIFIED TO, AND SHOULD BE COMPARED TO WITH THE ABSTRACT OF TITLE OR TITLE INSURANCE POLICY FOR ACCURACY, EASEMENTS OR EXCEPTIONS.

[Signature]
 Daniel D. Pratt P.S. No. 39094

SCALE: 1"=200'

○ = SET IRON W / CAP #39094
 ● = FOUND IRON
 R = RECORDED
 M = MEASURED

REVISION #1 PER TOWNSHIP REQUIREMENTS 2020-09-12



Terra Pointe Consultants, Inc.
 1128 Foxchase Lane, SE
 Grand Rapids, MI 49546
 PH 616.901.1968

Plat of Survey
 Part of the Northeast 1/4
 Sec. 05, Town 02 South, Range 12 West
 Oshtemo Township, Kalamazoo County, MI

Client: James Endres
 9307 W. G Avenue
 Kalamazoo, MI 49009

TPC Job No. 20022 Sheet 2 of 4
 Date of Field Survey: 06/23/2020

Parcel A:

Part of the Northeast one-quarter of Section 05, Town 02 South, Range 12 West, Oshtemo Township, Kalamazoo County, Michigan, described as follows: COMMENCING at the North one-quarter corner of said Section 5; thence S 01°07'56" W 600.00 feet along the North-South one-quarter line of said section to the POINT OF BEGINNING; thence S 88°53'10" E 2224.18 feet parallel with the North line of said section; thence N 01°06'55"W 190.00 feet parallel with the East line of said section; thence S 88°53'10" E 410.00 feet parallel with the North line of said section; thence S 01°06'55" W 2199.55 feet along the East line of said section to the East one-quarter corner of said section; thence N 89°26'03" W 654.80 feet along the East-West one-quarter line of said section; thence N 01°07'56" E 440.02 feet parallel with the North-South one-quarter line of said section; thence N 89°26'03" W 165.01 feet parallel with the East-West one-quarter line of said section; thence N 01°07'56" E 297.01 feet parallel with the North-South one-quarter line of said section; thence N 89°26'03" W 1215.06 feet parallel with the East-West one-quarter line of said section; thence N 01°07'56" E 756.01 feet parallel with the North-South one-quarter line of said section; thence N 89°26'03" W 600.03 feet parallel with the East-West one-quarter line of said section to a point on the North-South one-quarter line of said section; thence N 01°07'56" E 541.72 feet along the North-South one-quarter line of said section to the point of beginning. Parcel contains 81.29 acres including road right of way.

Parcel B:

Part of the Northeast one-quarter of Section 05, Town 02 South, Range 12 West, Oshtemo Township, Kalamazoo County, Michigan, described as follows: COMMENCING at the North one-quarter corner of said Section 5; thence S 01°07'56" W 1141.72 feet along the North-South one-quarter line of said section to the POINT OF BEGINNING; thence S 89°26'03" E 600.03 feet parallel with the East-West one-quarter line of said section; thence S 01°07'56" W 556.00 feet parallel with the North-South one-quarter line of said section; thence N 89°26'03" W 600.03 feet parallel with the East-West one-quarter line of said section to a point on the North-South one-quarter line of said section; thence N 01°07'56" E 556.00 feet along the North-South one-quarter line of said section to the point of beginning. Parcel contains 7.66 acres including road right of way.

Parcel C:

Part of the Northeast one-quarter of Section 05, Town 02 South, Range 12 West, Oshtemo Township, Kalamazoo County, Michigan, described as follows: BEGINNING at the North one-quarter corner of said Section 5; thence S 88°53'10" E 556.00 feet along the North line of said section; thence S 01°07'36" W 600.00 feet; thence N 88°53'10" W 556.04 feet parallel with the North line of said section to a point on the North-South one-quarter line of said section; thence N 01°07'56" E 600.00 feet along the North-South one-quarter line of said section to the point of beginning. Parcel contains 7.66 acres including road right of way.



I HERELY CERTIFY THAT I HAVE SURVEYED THE PARCEL(S) OF LAND DESCRIBED AND DELINEATED HEREON, THAT SAID PLAT IS A TRUE REPRESENTATION OF THE SURVEY AS PERFORMED BY ME, AND THAT I HAVE FULLY COMPLIED WITH THE REQUIREMENTS OF SECTION 3, ACT 132, P.A. 1970 AS AMENDED. THIS SURVEY WAS MADE FROM THE ATTACHED LEGAL DESCRIPTION. THE DESCRIPTION WAS GIVEN TO US BY THE PERSON CERTIFIED TO, OR WAS PREPARED BY US FROM INFORMATION OR DOCUMENTS GIVEN TO US BY THE PERSON CERTIFIED TO, AND SHOULD BE COMPARED TO WITH THE ABSTRACT OF TITLE OR TITLE INSURANCE POLICY FOR ACCURACY, EASEMENTS OR EXCEPTIONS.

[Signature]
Daniel D. Pratt

P.S. No. 39094

REVISION #1

PER TOWNSHIP REQUIREMENTS

2020-09-12



**Terra
Pointe
Consultants, Inc.**

1128 Foxchase Lane, SE
Grand Rapids, MI 49546
PH 616.901.1968

Plat of Survey
Part of the Northeast 1/4
Sec. 05, Town 02 South, Range 12 West
Oshtemo Township, Kalamazoo County, MI

Client: James Endres
9307 W. G Avenue
Kalamazoo, MI 49009

TPC Job No. 20022 Sheet 3 of 4
Date of Field Survey: 06/23/2020

Parcel D:

Part of the Northeast one-quarter of Section 05, Town 02 South, Range 12 West, Oshtemo Township, Kalamazoo County, Michigan, described as follows: COMMENCING at the North one-quarter corner of said Section 5; thence S 88°53'10" E 556.00 feet along the North line of said section to the POINT OF BEGINNING; thence continuing S 88°53'10" E 556.00 feet along the North line of said section; thence S 01°07'23" W 600.00 feet; thence N 88°53'10" W 556.04 feet parallel with the North line of said section; thence N 01°07'36" E 600.00 feet to the point of beginning. Parcel contains 7.66 acres including road right of way.

Parcel E:

Part of the Northeast one-quarter of Section 05, Town 02 South, Range 12 West, Oshtemo Township, Kalamazoo County, Michigan, described as follows: COMMENCING at the North one-quarter corner of said Section 5; thence S 88°53'10" E 1112.00 feet along the North line of said section to the POINT OF BEGINNING; thence continuing S 88°53'10" E 556.00 feet along the North line of said section; thence S 01°07'09" W 600.00 feet; thence N 88°53'10" W 556.04 feet parallel with the North line of said section; thence N 01°07'23" E 600.00 feet to the point of beginning. Parcel contains 7.66 acres including road right of way.

Parcel F:

Part of the Northeast one-quarter of Section 05, Town 02 South, Range 12 West, Oshtemo Township, Kalamazoo County, Michigan, described as follows: COMMENCING at the North one-quarter corner of said Section 5; thence S 88°53'10" E 1668.00 feet along the North line of said section to the POINT OF BEGINNING; thence continuing S 88°53'10" E 556.00 feet along the North line of said section; thence S 01°06'55" W 321.32 parallel with the East line of said section; thence N 88°53'05" W 4.90 feet; thence S 01°06'55" W 46.93 parallel with the East line of said section; thence S 88°53'05" E 4.90 feet; thence S 01°06'55" W 231.75 parallel with the East line of said section; thence N 88°53'10" W 556.04 feet parallel with the North line of said section; thence N 01°07'09" E 600.00 feet to the point of beginning. Parcel contains 7.66 acres including road right of way.

Parcel G:

Part of the Northeast one-quarter of Section 05, Town 02 South, Range 12 West, Oshtemo Township, Kalamazoo County, Michigan, described as follows: BEGINNING at the Northeast corner of said Section 5; thence S 01°06'55" W 410.00 along the East line of said section; thence N 88°53'10" W 410.00 feet parallel with the North line of said section; thence N 01°06'55" E 41.75 parallel with the East line of said section; thence N 88°53'05" W 4.90 feet; thence N 01°06'55" E 46.93 parallel with the East line of said section; thence S 88°53'05" E 4.90 feet; thence N 01°06'55" E 321.32 parallel with the East line of said section to a point on the North line of said section; thence S 88°53'10" E 410.00 feet along the North line of said section to the point of beginning. Parcel contains 3.86 acres including road right of way.



I HEREBY CERTIFY THAT I HAVE SURVEYED THE PARCEL(S) OF LAND DESCRIBED AND DELINEATED HEREON, THAT SAID PLAT IS A TRUE REPRESENTATION OF THE SURVEY AS PERFORMED BY ME, AND THAT I HAVE FULLY COMPLIED WITH THE REQUIREMENTS OF SECTION 3, ACT 132, P.A. 1970 AS AMENDED. THIS SURVEY WAS MADE FROM THE ATTACHED LEGAL DESCRIPTION. THE DESCRIPTION WAS GIVEN TO US BY THE PERSON CERTIFIED TO, OR WAS PREPARED BY US FROM INFORMATION OR DOCUMENTS GIVEN TO US BY THE PERSON CERTIFIED TO, AND SHOULD BE COMPARED TO WITH THE ABSTRACT OF TITLE OR TITLE INSURANCE POLICY FOR ACCURACY, EASEMENTS OR EXCEPTIONS.

Daniel D. Pratt

P.S. No. 39094

REVISION #1

PER TOWNSHIP REQUIREMENTS

2020-09-12



**Terra
Pointe
Consultants, Inc.**

1128 Foxchase Lane, SE
Grand Rapids, MI 49546
PH 616.901.1968

Plat of Survey
Part of the Northeast 1/4
Sec. 05, Town 02 South, Range 12 West
Oshtemo Township, Kalamazoo County, MI

Client: James Endres
9307 W. G Avenue
Kalamazoo, MI 49009

TPC Job No. 20022 Sheet 4 of 4
Date of Field Survey: 06/23/2020

ARTICLE 4

4 – AG: AGRICULTURAL DISTRICT

Contents:

4.10 STATEMENT OF PURPOSE

4.20 PERMITTED USES

4.30 PERMITTED USES WITH CONDITIONS

4.40 SPECIAL USES

(Amended by ord. no. 611; adopted May 28th, 2019, effective June 6th, 2019. Amended by ord.no. 647; adopted January 24, 2023.)

4.10 STATEMENT OF PURPOSE

This district classification is designed for areas where the principal use of land is for farming operations as defined in the Michigan Right to Farm Act. The district is intended to preserve the farming operations historically present in the Township and allow additional operations in keeping with the Township character. Activities within the district are to be carefully managed so as to achieve conservation of soil, water and nutrients.

4.20 PERMITTED USES

- A. Farm operations as defined in the Michigan Right to Farm Act when conducted in conformance with the generally-accepted agricultural and management practices adopted by the Michigan Commission of Agriculture.
- B. One dwelling may be established for each 200 feet of continuous public road frontage and three acres of land.
- C. Farm labor housing in compliance with the Michigan Health Code, as amended, when occupied by employees of the farm operation and their families. All such structures shall be setback a minimum of 100 feet from all property lines.
- D. Truck gardens, greenhouses, nurseries located on unplatted parcels.
- E. Houses of worship.
- F. Veterinarian clinics.
- G. Accessory buildings and uses customarily incidental to the foregoing.
- H. Essential services.
 - I. Family day care home.
 - J. Adult Foster Care Facility.
 - K. Foster Family Home.
 - L. Qualified Residential Treatment Programs (QRTP)

4.30 PERMITTED USES WITH CONDITIONS

- A. Temporary outdoor events (not lasting more than one day).
- B. Home occupations.
- C. Agribusiness.
- D. Agritourism, Category 1

(Amended by ord. no. 611; adopted May 28th, 2019, effective June 6th, 2019)

4.40 SPECIAL USES

- A. Buildings and regulator stations for essential services.
- B. Group day care home.

C. Riding stables.

D. Kennels for the breeding, raising and/or boarding of dogs or cats.

E. Temporary outdoor events (lasting more than one day).

F. Bed and Breakfast Inns.

G. Communication towers.

H. Earth removal, quarrying, gravel processing, mining, related mineral extraction businesses, and landfill gas recovery processing facilities.

I. Wind energy conversion systems.

J. Agritourism, Category 2.

K. Agritourism, Category 3.

■

ARTICLE 5

5 – RR: RURAL RESIDENTIAL DISTRICT

Contents:

5.10 STATEMENT OF PURPOSE

5.20 PERMITTED USES

5.30 PERMITTED USES WITH CONDITIONS

5.40 SPECIAL USES

(Amended by ord. no. 611; adopted May 28th, 2019, effective June 6th, 2019. Amended by ord. no. 632; adopted February 10th, 2021, effective February 16th, 2021. Amended by ord.no. 647; adopted January 24, 2023.)

5.10 STATEMENT OF PURPOSE

This district is intended to protect the quality of the overall environment of Oshtemo Township while satisfying the desire for a semi-rural lifestyle within areas which are not considered suitable for agricultural uses due to soil limitations or land fragmentation, but which are also not suitable for traditional residential subdivisions as a result of utility system limitations, street capacity, or topography or other natural features. Use of nontraditional land development options, such as the Open Space Community provisions of [Article 43](#) or Open Space Preservation Residential Development Option of [Article 44](#) to conserve open space, fallow land, wooded areas, and wetlands, is encouraged.

5.20 PERMITTED USES

- A. Private, one-family dwellings.
- B. Libraries
- C. Fire stations and other Township buildings
- D. Cemeteries, excluding crematories
- E. House of Worship
- F. Accessory buildings and uses customarily incidental to the foregoing.
- G. Essential services, excluding buildings and regulator stations.
- H. Family Day Care Home.
- I. Adult Foster Care Facility.
- J. Foster Family Home.
- K. Qualified Residential Treatment Programs (QRTP)

5.30 PERMITTED USES WITH CONDITIONS

- A. Nonprofit educational, noncommercial recreational and noncommercial business centers.
- B. Temporary outdoor events (not lasting more than one day).
- C. Home occupations.
- D. Agribusiness
- E. Agritourism, Category 1

5.40 SPECIAL USES

- A. Golf courses, parks, and outdoor recreational areas.
- B. Use of existing buildings formerly utilized in the daily operation of a farm (on or before March 12, 2003) on a parcel that is no longer operated as a functioning farm, as defined in the Michigan Right to Farm Act, for a landscaping contractor business or large-item storage subject to a finding by the Building Official that said building is suitable for

the proposed use. No outdoor storage of equipment or items such as snow plows, lawn mowers, trailers or boats may occur unless expressly approved during the Special Use and Site Plan review process.

C. Public and private schools; may have a Child Care Center as an accessory use.

D. Veterinarian clinics.

E. Kennels, in unplatted areas, for the breeding, raising and/or boarding of dogs or cats.

F. Shooting ranges and private clubs operating in connection therewith.

G. Buildings and regulator stations for essential services.

H. Group day care home.

I. Temporary outdoor events (lasting more than one day).

J. Bed and Breakfast Inns.

K. Communication towers.

L. Earth removal, quarrying, gravel processing, mining, related mineral extraction businesses, and landfill gas recovery processing facilities.

M. Wind energy conversion systems.

N. Agritourism, Category 2

O. Agritourism, Category 3

■

December 07, 2023



Mtg Date: December 14, 2023

To: Oshtemo Township Planning Commission

From: Colten Hutson, Zoning Administrator

Applicant: Scott Bradley, Charter Foods, Inc.
Greg Minshall, Matrix Consulting Engineers, Inc.

Owner: ATT AVON LLC

Property: 5481 W Main Street, Parcel Number 05-13-401-030

Zoning: C: Local Business District

Request: Site plan and special use approval to construct one 2,288 square foot building to serve as a fast food restaurant with drive through.

Section(s): Section 64: Site Plan Review
Section 65: Special Uses

PROJECT SUMMARY:

Long John Silvers is requesting site plan and special exception use approval to redevelop the site with one 2,288 square foot building to serve as a fast food restaurant with drive through located at 5481 W Main Street. The applicant is seeking to upgrade the property through a complete reconstruction of the site in order to revitalize it and bring it up to code. The approximate half-acre site is located on the corner of W Main Street and Maple Hill Drive, immediately east of US-131. The site under consideration is outlined in light blue on the map to the right.

The subject project site falls within the C: Local Business District zoning designation. Drive through services for businesses are considered a special exception use within the C: Local Business District. Any proposed special exception use is subject to review and approval from the Oshtemo Township Planning Commission.



ANALYSIS:

When reviewing this request, there are two sets of criteria that need to be considered: the general site plan review criteria outlined in Section 64 and the general special use review criteria outlined in Section 65.30. Below is an analysis of the proposal against these two code sections. Overall, most of the requirements of Section 64 and Section 65.30 have been met.

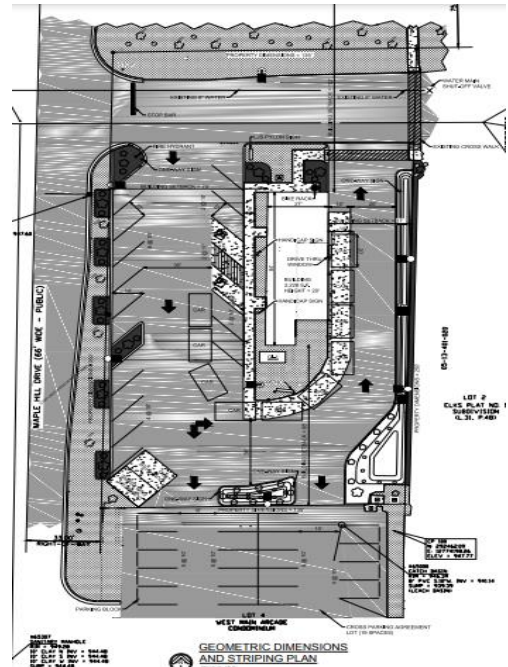
Section 64: Site Plan Review

General Zoning Compliance:

Zoning: The subject site is zoned C: Local Business District and is located in the northeast quadrant of the Township. The proposed Long John Silvers site abuts a strip mall to the south, a fast food restaurant to the east, a standard sit down restaurant to the west, and a tire service shop to the north. Zoning wise, C: Local Business District is adjacent to the north, south, east, and west. Restaurants providing drive through services are considered a special exception use within the C: Local Business District. All general zoning requirements have been met. A snapshot of the proposed site plan is provided on the right.

Access and Circulation

Access: The subject property currently has a private service drive traveling east/west that intersects through the north end of the property. The main point of access, however, is through the curb cut located in the northwest corner of the site adjacent to Maple Hill Drive. Additionally, the site can also be accessed through the property to the south as one-way access into the site has been provided. Most of the circulation aisle widths are proposed to be 20' wide, which is the minimum width required under Section 52.50 of the zoning ordinance for one-way travel. **There are still some issues with striping and other pavement markings located within the circulation aisles. Staff is confident that such issues can be addressed administratively and plans be submitted to the Township for review and approval post the pending decision by the Oshtemo Township Planning Commission.**



Drive through: The drive through component of this request is proposed to be primarily located along the south and east sides of the building. A majority of the drive through is proposed to be made up of concrete material. Drive throughs servicing a restaurant use are required by Section 52.90 of the zoning ordinance to provide a minimum of five stacking spaces from each individual point of service. A total of ten stacking spaces throughout the drive through have been provided, five from the order station on the south side of the building where the menu board is located and five from the service window on the east side of the building where the food is picked up. All drive through stacking spaces are proposed to be 10' x 20', which meets the required minimum dimensions outlined in Section 52.90 of the zoning ordinance. Although most drive-through requirements have been satisfied, there are some issues with striping as well. **As a condition of**

approval, the entire drive-thru lane, including areas necessary for stacking spaces, shall be demarcated and striping be shown on the civil site plan drawings.

Parking: A total of 16 parking spaces are currently proposed to be located on the Long John Silvers site, two of which are ADA accessible. All parking stalls are proposed to be 10' x 20'. Restaurant uses are required to provide one parking space for every 70 square feet of net floor area plus one per each employee on the largest shift OR one parking space per each three persons allowed within the maximum occupancy load as established by the Township building code plus one per each employee on the largest shift, whichever is greater. After calculating the parking needed for the proposed use on-site with the number for square footage being greater than based on occupancy load, a total of 31 parking spaces would be required. Although the site itself is short by 15 parking spaces, there is a cross parking easement agreement in place between the Long John Silvers lot and the property to the south, which provides Long John Silvers access to the south property's northerly 19 parking spaces. With the cross parking easement agreement in place, the applicant was able to meet such parking requirements by proposing a combined total of 35 parking spaces, which is shown on the site plan and is well under the maximum number of spaces allowed for sites of this nature.

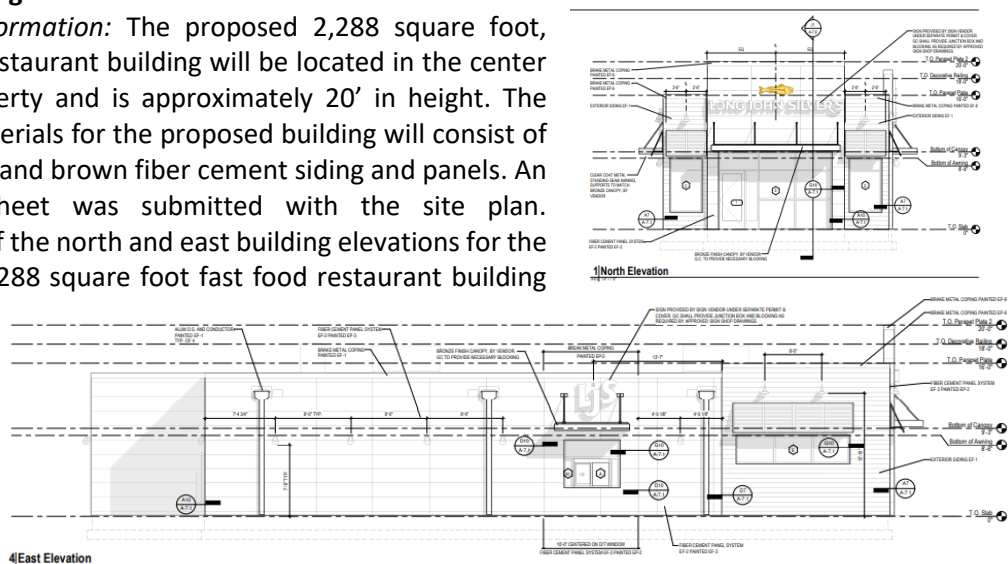
Easements: A cross parking easement agreement exists with the strip mall property to the immediate south which provides additional parking to the Long John Silvers property. Easements for public water and sewer are also present along the northern property line.

Sidewalk: The Township's adopted Non-motorized Plan does identify a sidewalk adjacent to the subject site on the south side of W Main Street. This stretch of W Main Street already offers a 5' wide sidewalk. Additionally, a sidewalk connector from said non-motorized facility to the principal building is proposed in the northeast corner of the site in order to provide pedestrian access. With said non-motorized facility already having been installed, and the applicant proposing a sidewalk connection from the principal building on-site to said existing non-motorized facility, no further action on this item is needed.

Building Design

Building Information: The proposed 2,288 square foot, one-story restaurant building will be located in the center of the property and is approximately 20' in height. The exterior materials for the proposed building will consist of blue, white, and brown fiber cement siding and panels. An elevation sheet was submitted with the site plan. Snapshots of the north and east building elevations for the proposed 2,288 square foot fast food restaurant building

can be found on the images to the right.



Lot Dimensions: The overall project site is about a half-acre in size and is located within the Elk's Plat No. 1. The subject lot possesses approximately 250' of road frontage adjacent to Maple Hill Drive and 135' of road frontage adjacent to W Main Street. The lot exceeds both the property area (min. 13,200 square feet) and frontage (min. 120') requirements for Commercial lots located in a plat and serviced by public water and sewer. The site's dimensions satisfy zoning ordinance requirements.

Setbacks: Per Section 50.60.C of the zoning ordinance, properties located within Commercial zoning districts are required to have a minimum front yard setback of 70'. If properties are located along a designated highway within the Township, they are subject to additional setback requirements if greater than what is outlined for the underlying zoning district. The subject property possesses two front yards, one along Maple Hill Drive, and the other along W Main Street, which happens to be a designated highway. The minimum front yard setback from the edge of the Maple Hill Drive right-of-way is 70'. The minimum setback requirement from W Main Street is 170' from the center of the street right-of-way. However, there is a provision in the zoning ordinance which allows for a reduction in the front yard setback based on the setback of existing buildings in proximity to the subject site. The Firestone Tire Shop, which has a front yard setback of 100' measured from the center of the W Main Street right-of-way, is located between 175' and 300' away from the subject site. Per Section 50.60.A of the zoning ordinance, staff is allowed to take their front yard setback of 100' and add 2/3 of the difference between the prescribed setback distance of 170' and the setback of the existing building of 100'. $2/3$ of 70' is 46.2'. $100' + 46.2' = 146.2'$. Therefore, the minimum permitted front yard setback is 146.2' from the center of the W Main Street right-of-way. The building is setback 70' from the front property line along Maple Hill Drive and 153' from the center of the street right-of-way on W Main Street. Properties zoned as Commercial are also subject to have a minimum side and rear yard setback of 20' or equal to the height of the abutting side of the building at its highest point as measured from the grade of the property line, whichever is greater. The height of the building is 20' tall, which requires a minimum setback in the side and rear yards of 20', which is met. The minimum setbacks for the front yard, side yard, and rear yard are satisfactory.

Screening: The applicant is proposing to install a 7'6" tall dumpster enclosure in the southwest corner of the site. The dumpster enclosure is proposed to be made of concrete block with steel metal tube doors. The color is proposed to match the building. With respects to fences, other than installing temporary tree protection fencing along the west property line, no fences are proposed at this time.

Lighting: A photometric plan was provided and meets most zoning ordinance requirements; however, there are issues regarding the placement for some of the pole mounted lights which will require repositioning. **Staff is confident that a revised lighting plan can be reviewed and approved administratively and recommend the Planning Commission include such as a condition of approval.**

Signs: One-way directional signs are being added to the site. Any future additional signage will require a sign permit application be submitted to the Township for review and approval.

Landscaping: A number of details are still missing on the landscaping plan. **An updated landscaping plan meeting all applicable requirements of Article 53 of the zoning ordinance shall be submitted to the Township and be reviewed and approved administratively.**

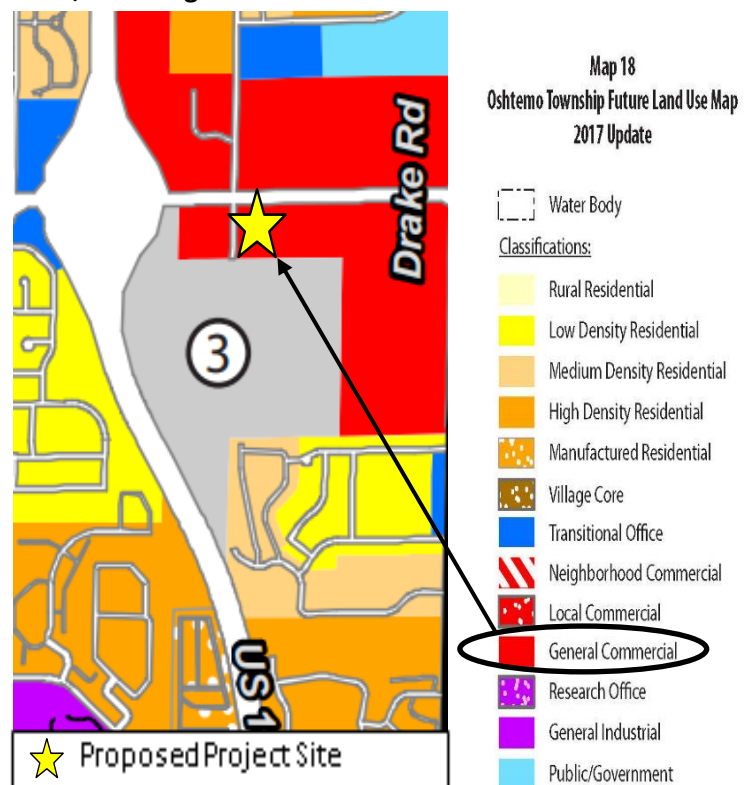
Engineering: Prein & Newhof and the Oshtemo Public Works Department have reviewed the proposal and have noted that there are some engineering concerns that have not yet been addressed. **However, the remaining engineering concerns are minor enough to be reviewed and approved administratively and recommend the Planning Commission include such as a condition of approval.**

Fire Department: The Fire Marshal has reviewed the site plan and overall is happy with the proposed site plan. The Fire Marshal mentioned that the circulation for fire apparatus throughout the subject property has improved.

Section 65.30: Special Use Review Criteria

- A. Master Plan/Zoning Ordinance: The proposed use will be consistent with the purpose and intent of the Master Plan and Zoning Ordinance, including the District in which the use is located.**

The Township’s Future Land Use Plan categorizes this area—west of Drake Road, east of Maple Hill Drive, and south of W Main Street, as *General Commercial*. Restaurants with drive through services are permissible with special use approval from the Planning Commission under the C: Local Business District zoning classification. This property is currently zoned C: Local Business District. From a zoning perspective, the proposed use would be consistent with the most recent use on-site and is in accordance with the Township’s Zoning Ordinance.



- B. Site Plan Review: The Site Plan Review Criteria of Section 64**

A site plan has been provided.
See evaluation under Section 64: Site Plan Review.

- C. Impacts:**

1. The proposed use would be compatible, harmonious and appropriate with the existing or planned character and uses of adjacent properties; meaning the proposed use can coexist with neighboring uses in a stable fashion over time such that no neighboring use is unduly negatively impacted.

Although no longer in use, a fast food restaurant formerly operated by Long John Silvers already exists on-site. Several other fast food restaurants are within a close perimeter of a couple hundred feet from the subject property, which include Wendys, Burger King, McDonalds, Starbucks, and Steak ‘n Shake. With the proposed use continuing to operate as it has in prior years, is in proximity of other fast food restaurant uses, and being in accordance with the Zoning Ordinance, staff does not foresee that the proposed use will negatively affect neighboring uses.

2. Potentially adverse effects arising from the proposed use on adjacent properties would be minimized through the provision of adequate parking, the placement of buildings, structures and entrances, as well as the location of screening, fencing, landscaping, buffers or setbacks.

Staff does not foresee a significant impact of the proposed use on neighboring properties. The proposed use is an allowable use within the zoning district it is planned for. A restaurant with a drive through service has operated at the subject property in the past. Entrances on the site will continue to be used in a similar fashion. The proposed site plan meets the minimum setback and parking requirements. Adequate screening, buffers, and more are provided and discussed further in the Site Plan Review section of this staff report.

3. The proposed use would not be detrimental, hazardous, or disturbing to existing or future adjacent uses or to the public welfare by reason of excessive traffic, noise, smoke, odors, glare, or visual clutter.

A restaurant providing drive through services has occupied the site for nearly 20 years. Although the existing building will be demolished and the property will be revamped, the use will remain the same. Best construction practices and guidelines will be followed, and the site will be built to satisfy building code and zoning ordinance requirements. The amount of traffic will be consistent with what the subject site has produced in the past. The current conditions on-site include a boarded up, vacant building with overgrown vegetation. The revitalization of the site will benefit the property itself, surrounding businesses, and the community. Staff anticipates that the proposed project will not generate such negative impacts on adjacent properties.

D. Environment: The natural features of the subject property shall only be cleared or altered to the extent necessary to accommodate site design elements, particularly where the natural features assist in preserving the general character of the area.

The proposed site modifications, as presented in the site plan, will minimally impact the existing natural features on the site. The subject site is currently well built out since it previously served as a fast food restaurant. The only type of greenspace that is present now are the canopy trees along the northern and western property lines, which will remain intact. Additional landscaping including understory canopy trees and shrubbery are proposed to be located at the north and west ends of the site. The stormwater basin proposed in the southeast corner of the site also includes plantings as well.

E. Public Facilities: Adequate public and/or private infrastructure and services already exist or would be provided, and will safeguard the health, safety, and general welfare of the public.

Public water and sewer are located on the north and east ends of the site and are both available. Regarding the transportation network, the intersection to the immediate west at Maple Hill Drive

and W Main Street is controlled by a traffic signal. If the special exception use request is granted, it will not negatively affect the area as the use is not changing from what was previously there. A 5' wide sidewalk connecting the existing 5' wide non-motorized facility adjacent to W Main Street to the newly proposed fast food restaurant building on-site is proposed.

F. Specific Use Requirements: The Special Use development requirements of Article 49.

No specific use requirements exist for drive through services for businesses. Therefore, this section does not apply.

RECOMMENDATION:

Planning Department staff recommend that the Planning Commission approve the proposed site plan and special exception use for a 2,288 square foot restaurant with drive through located at 5481 W Main Street with the following conditions.

- 1) A Soil Erosion and Sedimentation Control (SESC) permit from the Kalamazoo County Drain Commissioner's Office will be required prior to building permit issuance.
- 2) The engineer's seal shall be provided on an updated set of civil site plan drawings and submitted to the Township prior to building permit issuance.
- 3) The parcel ID for the subject site and lot size information shall be noted under general project and site data on Sheet T-1.0 of the plan set prior to building permit issuance.
- 4) Finalization of striping, pavement markings, and drive through details shall be subject to the administrative review and approval of the Township Planning Department prior to building permit issuance.
- 5) Finalization of grading details, site drainage, and any other engineering details shall be subject to the administrative review and approval of the Township Engineer prior to building permit issuance.
- 6) An updated landscaping plan meeting the requirements outlined in Section 53 of the Zoning Ordinance shall be submitted to the Township for review and approval prior to building permit issuance.
- 7) A revised lighting plan meeting the requirements outlined in Section 54 of the Zoning Ordinance shall be submitted to the Township for review and approval prior to building permit issuance.
- 8) The watermain connection shall be coordinated with the City of Kalamazoo Department of Public Services – Anna Crandall (269) 337-8055. Once approved by the City of Kalamazoo, the plans and a copy of the permit for final approval shall be provided to the Township prior to construction.
- 9) Copies of any necessary recorded easements shall be provided to the Township prior to issuing a certificate of occupancy.

Attachments: Application, Environmental Checklist, Hazardous Substance Form, Architectural Drawings, Civil Site Plan Set, Lighting Cut Sheets, and Cross Parking Agreement

[This page left intentionally blank]



CHARTER TOWNSHIP
Established 1839

7275 W. Main Street, Kalamazoo, Michigan 49009-9334
Phone: 269-375-4260 Fax: 269-375-7180

PLEASE PRINT

PROJECT NAME & ADDRESS Long John Silvers - 5481 W. Main St.
Kalamazoo, MI 49009

PLANNING & ZONING APPLICATION

Applicant Name: Greg Minshall

Company: Matrix Consulting Engineers inc.

Address: 1601 E Cesar E Chavez

E-mail: greg@minshallconsulting.com

Telephone: 517-719-2898 Fax: N/A

Interest in Property: Engineer

THIS
SPACE
FOR
TOWNSHIP
USE
ONLY

OWNER*: ATT Avon, LLC c/o A&C Ventures Inc.

Name: Karyl Carter, Portfolio Asset Manager

Address: 465 First St. W, 2nd Floor
Sonoma, CA 95476

E-mail: Sbradley@charterfoods.net

Phone & Fax: 615-310-6589

Fee Amount _____

Escrow Amount _____

NATURE OF THE REQUEST: (Please check the appropriate item(s))

- | | |
|---|---|
| <input type="checkbox"/> Pre-Application Review | <input type="checkbox"/> Accessory Building Review - I083 |
| <input checked="" type="checkbox"/> Site Plan Review - I088 | <input type="checkbox"/> Rezoning - I091 |
| <input type="checkbox"/> Administrative Site Plan Review - I086 | <input type="checkbox"/> Subdivision Plat Review - I089 |
| <input type="checkbox"/> Special Exception Use - I085 | <input type="checkbox"/> Interpretation - I082 |
| <input type="checkbox"/> Zoning Variance - I092 | <input type="checkbox"/> Other: _____ |
| <input type="checkbox"/> Site Condominium - I084 | |

BRIEFLY DESCRIBE YOUR REQUEST (Use Attachments if Necessary): It is our request
to submit the following project for site plan review.

LEGAL DESCRIPTION OF PROPERTY (Use Attachments if Necessary):

North 250' of lot 3, Elk Plat No. 1, According to the Plat thereof as recorded in Liber 31 of Plats of Page 48, Kalamazoo County Records. Being the same property conveyed by warranty deed Concord Capital Management, L.L.C. A Michigan Liability Company to B.R. Associates, Inc.

PARCEL NUMBER: 3905- D

ADDRESS OF PROPERTY: 5481 W. Main St. Kalamazoo, MI 49009

PRESENT USE OF THE PROPERTY: Existing Long John Silvers

PRESENT ZONING: C - Local Business SIZE OF PROPERTY: 0.775 acres

NAME(S) & ADDRESS(ES) OF ALL OTHER PERSONS, CORPORATIONS, OR FIRMS HAVING A LEGAL OR EQUITABLE INTEREST IN THE PROPERTY:

Name(s)
Scott Bradley - Development Representative

Address(es)
PO Box 430, Talbott, TN 37877

Corey Jenkins- Project Manager

PO Box 430, Talbott, TN 37877

SIGNATURES

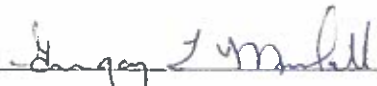
I (we) the undersigned certify that the information contained on this application form and the required documents attached hereto are to the best of my (our) knowledge true and accurate. I (we) acknowledge that we have received the Township's Disclaimer Regarding Sewer and Water Infrastructure. By submitting this Planning & Zoning Application, I (we) grant permission for Oshtemo Township officials and agents to enter the subject property of the application as part of completing the reviews necessary to process the application.



Owner's Signature (*If different from Applicant)

11/21/23

Date



Applicant's Signature

6/27/2023

Date

- Copies to:
- Planning - 1
- Applicant - 1
- Clerk - 1
- Deputy Clerk - 1
- Attorney - 1
- Assessor - 1
- Planning Secretary - Original

PLEASE ATTACH ALL REQUIRED DOCUMENTS

STATE / COUNTY ENVIRONMENTAL PERMITS CHECKLIST FOR OSHTEMO TOWNSHIP

This checklist has been designed to assist businesses in identifying and complying with state and county environmental permits and requirements. Please note that this checklist pertains only to state and county environmental permits. Additional permits and approvals may be required from Oshtemo Township or other government agencies.

This form must be completed and returned to the Township when a site plan is submitted.

This checklist is not a permit application form; businesses are responsible for obtaining information and permit application forms from the appropriate government offices. Compliance and proper registration with applicable state and county requirements is required for site plan approval in Oshtemo Township. The Township will forward a copy of this form to the Michigan Department of Environmental Quality Permit Coordinator.

Name of Business: Long John Silvers

Location of Business: 5481 W Main Street, Kalamazoo, MI 49009

Name of Business Owner: LJS OPCO One LLC

Mailing Address: 10350 Ormsby Park Place, Suite 300


City: Louisville State: Kentucky Zip: 40223

Telephone: 423-438-0216

Business Manager / Operator: Scott Bradley - Development Representative

Type of Business (type of activities to be carried out at the proposed business— include all processes and operations):
Preparation and service of food to dine-in and drive-thru customers

I affirm that the information submitted in this form is accurate.

Owner's Signature:  Date: 6/27/2023

Please circle Y (yes) or N (no) for each of the questions below. If Y (yes), contact the agency listed for further information and assistance.

1. Y N Will the proposed project discharge any type of wastewater to a storm sewer, drain, wetland, pond, lagoon, or other surface water body?
Contact: Michigan Department of Environmental Quality, Surface Water Quality Division, (269) 567-3500
2. Y N Will the proposed project discharge any liquids, sludge, wastewater and/or wastewater residuals into or onto the ground?
Contact: Michigan Department of Environmental Quality, Waste Management Division, (269) 567-3500
3. Y N Will the proposed project use or store any hazardous substances, oil or salt? Depending upon the type of substance, secondary containment and a Pollution Incident Prevention Plan (PIPP), or a material storage permit may be required.
Contact: Michigan Department of Environmental Quality, Waste Management Division, (269) 567-3500
4. Y N Will the proposed project use underground storage tanks? Existing and proposed tanks must be registered with the State of Michigan, and installed and operated in accordance with regulations of the Michigan State Police Fire Marshal Division.
Contact: Michigan Department of Environmental Quality, Storage Tank Division, (517) 335-2690
5. Y N Will the proposed project burn, landfill, transfer, or process any type of solid, non-hazardous wastes?
Contact: Michigan Department of Environmental Quality, Waste Management Division, (269) 567-3500
6. Y N Will the proposed project involve the transport, on site treatment, storage or disposal of hazardous waste generated in quantities of 1000 kilograms (250 gallons or 2200 lbs.) or more per month? If yes, one or more permits may be required.
Contact: Michigan Department of Environmental Quality, Waste Management Division, (269) 567-3500
7. Y N Will the proposed project generate between 100 kilograms/month (25 gallons or 220 lbs.) and 1,000 kilograms/month (250 gallons or 2,200 lbs.) of hazardous waste? If yes, the facility may be a small quantity generator, subject to federal and state regulations. An EPA identification number should be obtained from the Michigan Department of Environmental Quality (special forms are available) and a manifest (shipping paper) should be used to transport waste off-site.
Contact: Michigan Department of Environmental Quality, Waste Management Division, (269) 567-3500
8. Y N Will the proposed project install, construct, reconstruct, relocate, or operate any process equipment (including air pollution control equipment) which has the potential to emit air contaminants?
Contact: Michigan Department of Environmental Quality, Waste Management Division, (269) 567-3500
9. Y N Does the proposed project involve any work (dredging, filling, construction) in a river, stream, creek, ditch, wetland, or floodplain or within 500 feet of an inland lake, river, stream or ditch?
Contact: Michigan Department of Environmental Quality, Land and Water Management, (269) 567-3500
10. Y N Will the proposed project change the natural cover or topography of the land, including cut and fill activities which may contribute to soil erosion and/or sedimentation? Will the earth change disturb an area of one acre or more, or occur within 500 feet of a lake or stream? If yes, a soil erosion and sedimentation control permit may be required.
Contact: Kalamazoo County Drain Commission, Kalamazoo (269) 384-8117.

11. Y N Will an on-site wastewater treatment system or septic system be installed?
 Will septate be stored on-site prior to off-site disposal?
Contact: Sanitary Sewage—Kalamazoo County Human Services Department, Environmental Health Program (269) 373-5210
Contact: Industrial/Commercial Wastewater in any quantity, or for sanitary sewage more than 10,000 gallons/day— Michigan Department of Environmental Quality, Waste Management Division, (269) 567-3500
12. Y N If the proposed project will be connected to sanitary sewer, will any hazardous and/or industrial chemicals, wastewater or waste in any quantity be discharged to the City of Kalamazoo Wastewater Treatment Plant? If yes, pretreatment may be required.
Contact: City of Kalamazoo, Water Reclamation Plant (269) 337-8157.
13. Y N Will the proposed project construct a new water well and/or abandon an existing water well? If yes to either one, contact the appropriate agency.
Contact: Well Construction, Kalamazoo County Human Services Department (269) 373-5210.
Contact: Well Abandonment, Kalamazoo County Human Services Department (269) 373-5210.
14. Y N Is this proposed project (or any other facility/property under your ownership) currently involved in any compliance discussions with the Michigan Department of Environmental Quality or the Michigan Attorney General’s Office.
Contact: Michigan Department of Environmental Quality, Remediation & Redevelopment, Lansing District Office (517) 373-9837
15. Y N Is this proposed project (or any other facility/property under your ownership) included on the MI Act 307 Priority List, “Michigan Sites of Environmental Contamination” or subject to corrective action under the Leaking Underground Storage Tank (LUST) Program?
Contact Michigan Department of Environmental Quality, Environmental Response Division, 269-567-3500

*For assistance with permits and approvals from the Michigan Dept. of Environmental Quality, including permit coordination among DEQ Division, contact the Permit Consolidation Unit, Lansing District Office (517)373-9244.

Note: This form is a checklist of permits and/or requirements which may be needed for project development. Oshtemo Township provides this to proposed developers as an informational service. Oshtemo Township is not a regulatory agency in any of these areas. Approval of the site plan by the Township does not transfer liability from the developer to the Township in any way.

OSHTEMO TOWNSHIP

HAZARDOUS SUBSTANCE REPORTING FORM

Note: This form must be completed and submitted as part of the site plan review process.

Name of Business: Long John Silvers

Location of Business: 5481 W Main Street, Kalamazoo, MI 49009

Name of Business Owner: LJS OPCO One LLC

Mailing Address: 10350 Ormsby Park Place, Suite 300

City: Louisville State: Kentucky Zip: 40223

Telephone: 423-438-0216

I affirm that the information submitted in this form is accurate:

Owner's Signature: [Signature] Date: 6/27/2023

MANAGEMENT OF HAZARDOUS SUBSTANCES

Hazardous substance (definition): Reference Sec. 11.308, Oshtemo Township Zoning Ordinance

1. Y [N] Will the proposed project store, use, or generate hazardous substances in quantities greater than 100 kilograms per month (about 25 gallons per month) now, or in the future? If yes, please complete the rest of this form and submit with your site plan. If no, stop here and submit with your site plan.

2. Y [N] Will hazardous substances be reused and / or recycled on-site?

3. Y [N] Will any hazardous substances be stored, used, or handled on the site? If yes, identify the location, size, and type of spill containment facilities which will be used and provide details on the site plan.

[Blank lines for response to question 3]

4. Y [N] Will hazardous waste an/or liquid industrial waste be transported off-site for treatment, disposal, and / or recycling? If yes, please list the name, address and telephone number of your licensed transporter(s).

[Blank lines for response to question 4]

5. Y N Will new underground storage tanks be located less than 2,000 feet from drinking water wells serving two or more establishments, or less than 300 feet from a single family drinking well? If yes, contact Michigan Department of Environmental Quality, Storage Tank Division, (517) 373-8168 for specific requirements and restrictions.

6. Y N Will the interior of the proposed project have any general purpose floor drains? If yes, into what system will the floor drains be connected? (Provide detail on site plan)

- a. Sanitary sewer system
- b. On-site holding tank(s)
- c. A system authorized by a state approved groundwater discharge permit, with required monitoring (Contact: Michigan Department of Environmental Quality, Waste Management Division, (616) 567-3500.

Note: General purpose floor drains shall not be connected to a storm drainage system, dry well or septic system.

7. Y N Please list the hazardous substances which are expected to be used, stored, or generated on-site. Quantities should reflect the maximum volumes on site at any time. Attach additional pages, if necessary to list all hazardous substances.

<u>Common/ Trade Name</u>	<u>Chemical Components</u>	<u>Form</u>	<u>Max Quantity</u>	<u>Storage</u>
-------------------------------	--------------------------------	-------------	---------------------	----------------

- A. _____
- B. _____
- C. _____
- D. _____
- E. _____
- F. _____

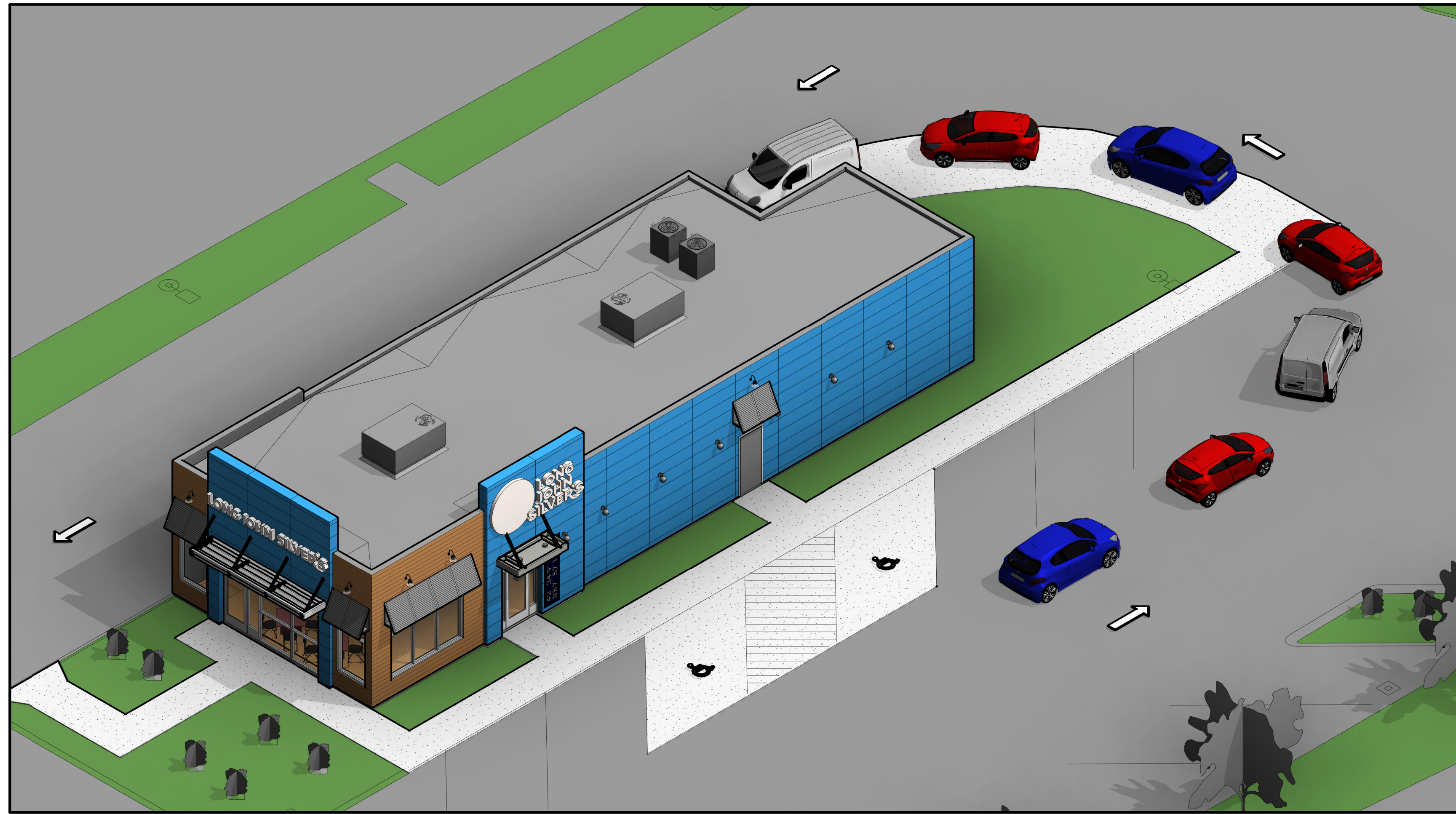
Key:

FORM		STORAGE	
Liq	Liquid	AST	Aboveground Storage Tank
P. Liq	Pressurized Liquid	UST	Underground Storage Tank
S	Solid	D	Drum
G	Gas	CY	Cylinder
PG	Pressurized Gas	MC	Metal Container
		WC	Wooden Container
		PT	Portable Tank

[This page left intentionally blank]

Long John Silver's

5481 W Main St, Kalamazoo, MI 49009



PROJECT TEAM

ARCHITECT
 SEDGEWICK + FERWEDA ARCHITECTS
 CONTACT: JEFFREY S. FERWEDA, AIA
 410 EAST COURT STREET
 FLINT, MICHIGAN 48903
 T: 810-238-9647
 JEFFREY@SFARCH.US

OWNER
 LONG JOHN SILVERS
 CONTACT: SCOTT BRADLEY, LJS DEVELOPMENT REPRESENTATIVE
 LJS OPKO ONE, LLC
 10350 ORMSBY PARK PLACE, SUITE 300
 LOUISVILLE, KENTUCKY 40223
 T: XXX-XXX-XXXX
 SBRADLEY@CHARTERFOODS.NET

ENGINEER
 MATRIX CONSULTING ENGINEERS, INC.
 CONTACT: NICK FRECHEN
 1601 E. CESAR E. CHAVEZ
 LANSING, MI 48906
 T: 517-487-2511
 ADMINISTRATOR@MATRIXCEINC.COM

BUILDING DATA

BUILDING AREA: 2228 SF
 TYPE: AREA FACTOR OCCUPANTS
 DINING ROOM 749 SF 1.15 SF 50
 QUEUING 20 SF 1.5 SF 4
 KITCHEN 861 SF 1.200 SF 5
 OFFICE 57 SF 1.100 SF 1
 ACCESSORY STORAGE 200 SF 1.300 SF 1
 ACCESSORY STORAGE & PASSAGE 312 SF 0 0
 TOTAL 61

PROPOSED USE:
 CONSTRUCTION TYPE: MBC TYPE V B (UNPROTECTED) NFPA 11-000

AREA LIMITATIONS:
 BUILDING HEIGHT: PERMITTED X MBC TABLE 504.3
 BUILDING AREA: MBC TABLE 506.2 SM LIMITED TO 43500 SQ.FT

OCCUPANT LOAD:
 MBC TABLE 1004.1.2 EDUCATIONAL
 NFPA 14.1.7 TABLE 7.3.1.2
 X SQ.FT/x = OCCUPANTS

DEAD END LIMIT:
 20' (NON FIRE SUPPRESSED) LIN FT MAX ALLOWED
 (MBC 1018.4 NFPA 14.2.5.2)

REQUIRED EXITS:
 2 REQUIRED MBC 1005.3.1 NFPA 14.2.4.2

FIRE PROTECTION:
 NFPA 101 14.3.5.1 NON SPRINKLED LIMITED TO 12,000 SQ.FT
 TRAVEL DISTANCE TO EXIT: 200 LIN FT MAX ALLOWED (MBC 1016.1)
 150 LIN FT MAX ALLOWED (NFPA 14.2.6.2)

COMMON PATH OF TRAVEL:
 75 LIN FT MAX ALLOWED (MBC 1006.2.1 NFPA 14.2.5.3.1)

EGRESS WINDOWS:
 NOT REQUIRED MBC 1030 NFPA 14.2.11.1.2

FIRE RATINGS: CORRIDORS: MBC SECTION 708 1 HR FIRE PARTITION NFPA 14.3.6
 UL U419 STUD WALLS UL U905 MASONRY WALLS
 HORIZONTAL BARRIERS: MBC SECTION 707 2 HR FIRE BARRIERS NFPA 14.2.2.5, 7.2.4.3
 UL U419 STUD WALLS UL U905 MASONRY WALLS
 STAIR ENCLOSURES: 1 HR RATED MBC 1022.1 NFPA 14.2.2.3
 FLOORS: 2 HR FIRE RATINGS UL G250 CONC/MTL DECK/STL JOIST
 WALL FINISHES: CORRIDOR CLASS 'A' ROOMS CLASS 'B' MBC TABLE 803.11 NFPA 14.3.3.2
 FLOOR FINISHES: CORRIDOR AND ROOMS CLASS II MBC SECTION 804 NFPA 14.3.3.3.1
 INCIDENTAL USES: NONE MBC TABLE 509.1 HR JANITORS CLOSET NFPA 14.3.2.1
 MULTIPLE OCCUPANCIES: FIRE SEPARATION 0 HR MBC TABLE 508.4 NFPA 14.1.4
 SPECIAL PROVISIONS:

GENERAL PROJECT AND SITE

PROJECT SUMMARY:
 PARCEL ID:
 LOT SIZE:
 PARCEL ZONING: C - LOCAL BUSINESS

SETBACKS:
 FRONT: 170FT FROM CENTER OF HWY
 REAR: 20FT
 SIDE: 20FT

LEGAL DESCRIPTION:
 THE NORTH 250 FEET OF LOT 3, ELKS PLAT NO. 1, ACCORDING TO THE PLAT THEREOF AS RECORDED IN LIBER 31 OF PLATS OF PAGE 48, KALAMAZOO COUNTY RECORDS.
 BEING THE SAME PROPERTY CONVEYED BY WARRANTY DEED FROM CONCORD CAPITAL MANAGEMENT, L.L.C., A MICHIGAN LIMITED LIABILITY COMPANY TO BR ASSOCIATES, INC., AN INDIANA CORPORATION DATED DECEMBER 29, 2003 AND RECORDED JANUARY 14, 2004 IN INSTRUMENT NO. 2004-002150.

AREA SCHEDULE		
NO.	NAME	AREA
1	1ST FLOOR	2228 SF

BUILDING AREA
 1ST FLOOR

BUILDING CODES

- BUILDING CODES TO REFERENCE
- MICHIGAN BUILDING CODE 2015 EDITION
 - MICHIGAN RESIDENTIAL BUILDING CODE 2015 EDITION
 - MICHIGAN REHABILITATION CODE 2015 EDITION - LEVEL 2
 - MECHANICAL - MICHIGAN MECHANICAL CODE 2015 EDITION
 - PLUMBING - MICHIGAN PLUMBING CODE 2018 EDITION
 - ELECTRICAL - NATIONAL ELECTRICAL CODE 2017 EDITION
 - MICHIGAN ENERGY CODE - ASHRAE 90.1-2013
 - NFPA 101 LIFE SAFETY CODE 2015 EDITION

DRAWING INDEX

SHEET NO.	SHEET NAME	ISSUE DATE	ADDENDUM 1	SITE PLAN RESUB.	SITE PLAN RESUB.	SITE PLAN RESUB.	REVISION
T-1.0	Title Sheet	11/20/23	•	•	•	•	•
C-100	Cover Sheet	11/20/23	•	•	•	•	•
C-300	Civil Site Plan - Existing Conditions	11/20/23	•	•	•	•	•
C-301	Civil Site Plan - Demolition	11/20/23	•	•	•	•	•
C-400	Civil Site Plan - Grading and Storm Sewer	11/20/23	•	•	•	•	•
C-401	Civil Site Plan - Utility Plan	11/20/23	•	•	•	•	•
C-402	Geometric Dimensions and Striping Plan	11/20/23	•	•	•	•	•
C-800	Civil Details	11/20/23	•	•	•	•	•
T-2	General Notes	11/20/23	•	•	•	•	•
T-3	Finish Schedule	09/05/23	•	•	•	•	•
AS-1	Site Amenities Plan	11/20/23	•	•	•	•	•
AS-2	Site Amenities Details	11/20/23	•	•	•	•	•
AS-3	Dumpster Enclosure Details	11/20/23	•	•	•	•	•
A-1.0	Proposed Floor Plan	11/20/23	•	•	•	•	•
A-2.0	Roof Plan	09/05/23	•	•	•	•	•
A-3.0	Reflected Ceiling Plan	09/05/23	•	•	•	•	•
A-4.0	Finish Plan	09/05/23	•	•	•	•	•
A-5.0	Interior Elevations	09/05/23	•	•	•	•	•
A-5.1	Interior Elevations	09/05/23	•	•	•	•	•
A-6.0	Exterior Elevations	11/20/23	•	•	•	•	•
A-7.0	Wall Sections	09/05/23	•	•	•	•	•
A-7.1	Wall Sections & Details	09/05/23	•	•	•	•	•
A-8.0	Door Schedule & Details	09/05/23	•	•	•	•	•
A-8.1	Window Schedule & Details	09/05/23	•	•	•	•	•
A-9.1	Used Cooking Oil Retention System	09/05/23	•	•	•	•	•
K-1.0	Equipment Plan	09/05/23	•	•	•	•	•
K-1.1	Equipment Schedule	09/05/23	•	•	•	•	•
S-1	Foundation Plan	09/05/23	•	•	•	•	•
S-2	Floor Framing Plan	09/05/23	•	•	•	•	•
S-3	Wall Sections	09/05/23	•	•	•	•	•
S-4	Sections	09/05/23	•	•	•	•	•
S-5	Sections	09/05/23	•	•	•	•	•
S-6	Typical Details	09/05/23	•	•	•	•	•
S-7	General Notes	09/05/23	•	•	•	•	•
M-100	Mechanical Symbols, Notes & Abbreviations	09/05/23	•	•	•	•	•
M-400	First Floor Plan - Mechanical New	09/05/23	•	•	•	•	•
M-401	Roof Plan - Mechanical New	09/05/23	•	•	•	•	•
M-700	Hood Details	09/05/23	•	•	•	•	•
M-701	Hood Details	09/05/23	•	•	•	•	•
M-702	Exhaust Fan Details	09/05/23	•	•	•	•	•
P-100	Plumbing, Symbols, Notes & Schedules	09/05/23	•	•	•	•	•
P-400	Plumbing Waste & Vent Floor Plan	09/05/23	•	•	•	•	•
P-401	First Floor Plan - Water & Gas New	09/05/23	•	•	•	•	•
P-700	Isometric Diagrams	09/05/23	•	•	•	•	•
P-701	Details	09/05/23	•	•	•	•	•
P-702	Details	09/05/23	•	•	•	•	•
E-100	Electrical Symbols	09/05/23	•	•	•	•	•
E-101	Electrical Fixture Schedule & Details	09/05/23	•	•	•	•	•
E-102	Electrical Specifications	09/05/23	•	•	•	•	•
E-200	Electrical Site Plan	09/05/23	•	•	•	•	•
E-400	Floor Plan - Electrical Lighting	09/05/23	•	•	•	•	•
E-500	Floor Plan - Electrical Power	09/05/23	•	•	•	•	•
E-501	Floor Plan - Electrical Power	09/05/23	•	•	•	•	•
E-600	Electrical Riser Diagram	09/05/23	•	•	•	•	•

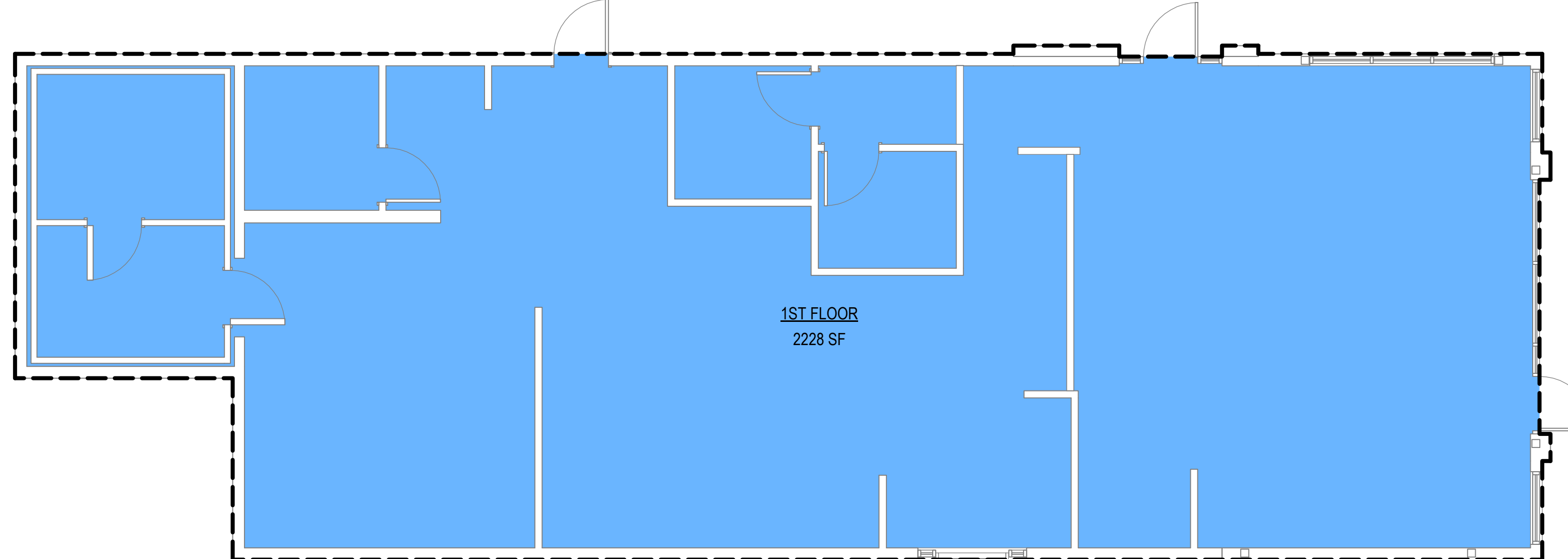
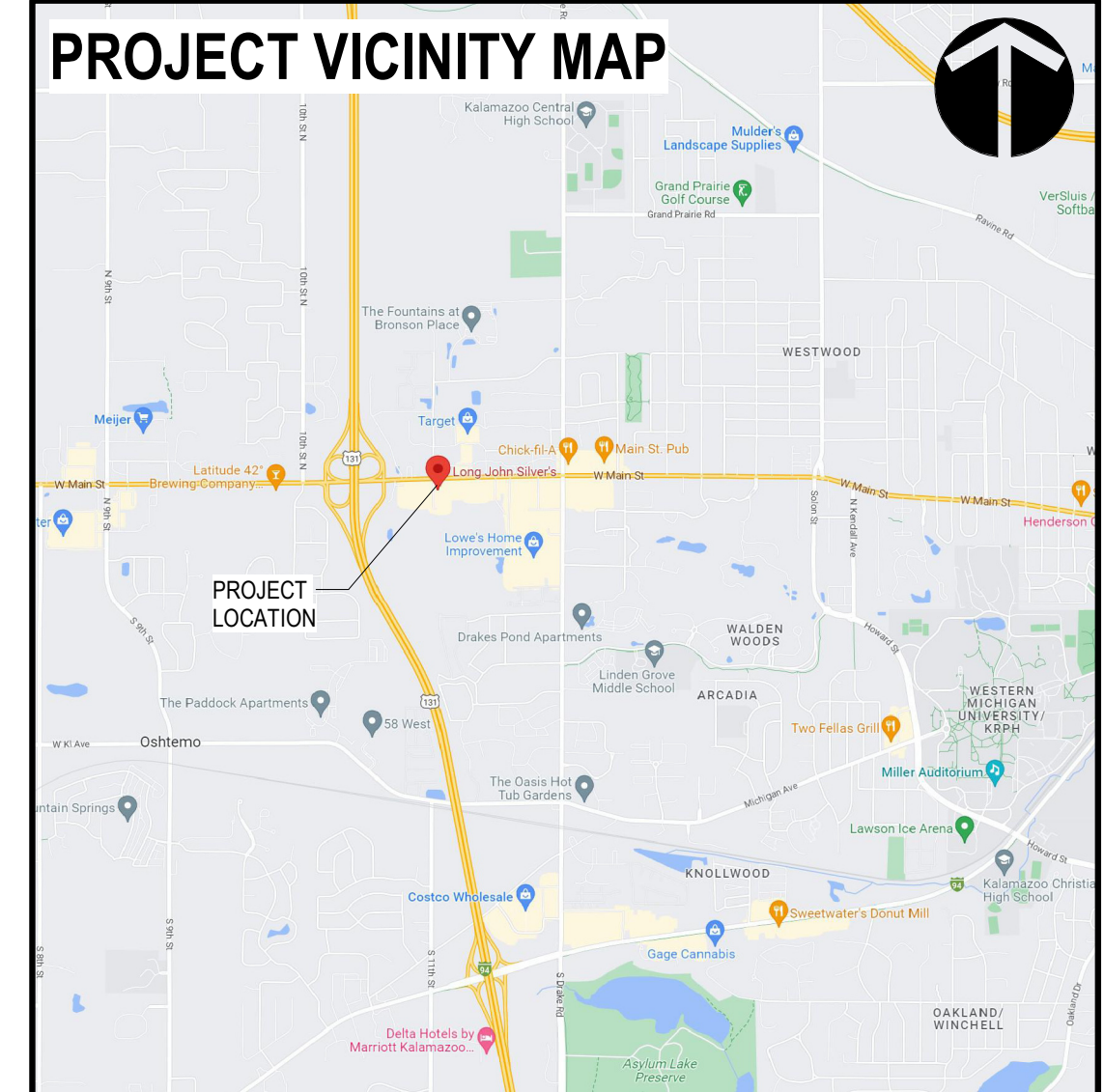
GENERAL NOTES

- DO NOT SCALE DRAWINGS. USE FIGURED DIMENSIONS ONLY.
- CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO ANY WORK.
- ALL WORK SHALL CONFORM TO ALL GOVERNING CODES AND REGULATIONS.

KEY TO DIMENSIONING:
 DIMENSIONS SHOWN ARE FROM ROUGH STUD FACE TO ROUGH STUD FACE. ROUGH FACE TO COLUMN CENTERLINE, AND COLUMN CENTERLINE TO COLUMN CENTERLINE, UNLESS NOTED OTHERWISE.

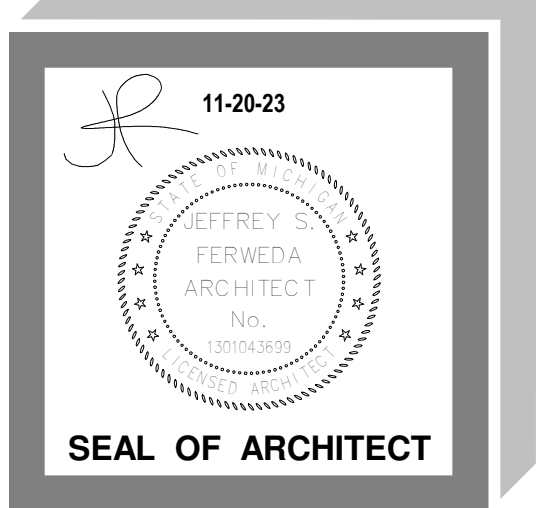
OWNERSHIP OF DRAWINGS

ALL PLANS, DRAWINGS, AND SPECIFICATIONS (THE DOCUMENTS) ARE INSTRUMENTS OF SERVICE AND SHALL REMAIN THE PROPERTY OF THE ARCHITECT WHETHER THE PROJECT FOR WHICH THEY WERE MADE IS EXECUTED OR NOT. OWNER SHALL NOT USE THE DOCUMENTS ON OTHER PROJECTS, FOR EXTENSIONS OR ADDITIONS TO THE PROJECT, OR FOR THE COMPLETION OF THE PROJECT BY OTHER EXCEPT IN AGREEMENT IN WRITING AND WITH APPROPRIATE COMPENSATION TO THE ARCHITECT. THE OWNER SHALL HOLD THE ARCHITECT HARMLESS FROM ALL CLAIMS, ACTIONS, CAUSE OF ACTIONS, LIABILITY, LOSSES, DAMAGES, COST AND EXPENSE, INCLUDING ATTORNEY'S FEES AND COSTS ARISING FROM SUCH UNAUTHORIZED USE. REPRODUCTION COPIES CAN BE PROVIDED AT THE OWNERS REQUEST AND EXPENSE.



11/20/2023 1:57:30 PM

1 Area Plan
 T-1.0 3/16" = 1'-0"



ARCHITECT
SEDGEWICK + FERWEDA ARCHITECTS
 410 East Court Street Flint, MI 48903
 810-238-9647 | www.sfarch.us
 Good clients + Good design + Great architecture

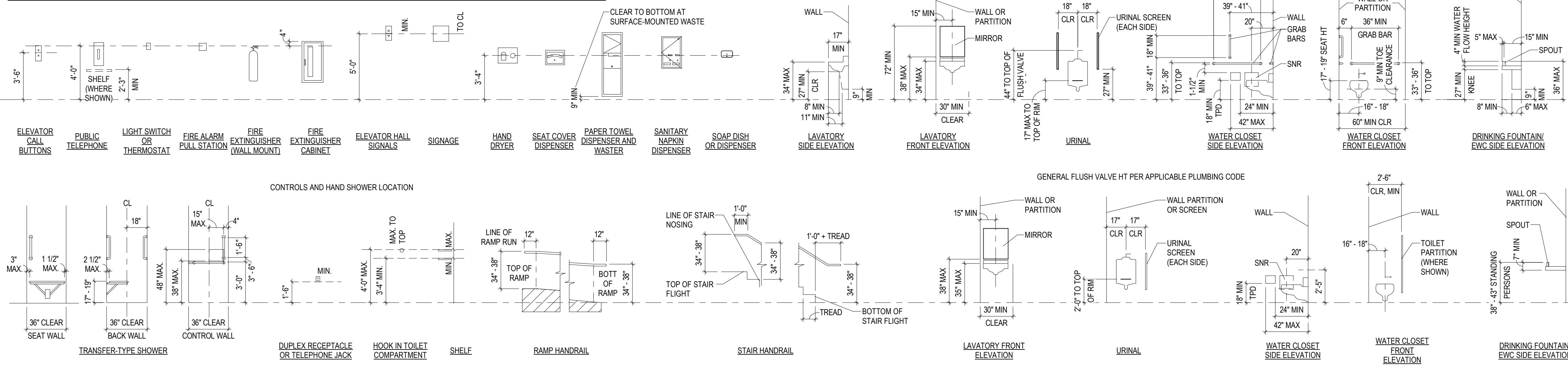


REVISIONS	
11/20/23	REVISION
10/24/23	SITE PLAN RESUBMISSION
9/5/23	SITE PLAN RESUBMISSION
8/8/23	SITE PLAN RESUBMISSION
Date	Description

Project
Long John Silver's
5481 W Main St, Kalamazoo, MI 49009
 Drawing
Title Sheet

DRAWN BY: LJK
 CHECKED BY: JSF
 PROJECT NUMBER **23-017**
 DATE **07/17/2023**
 SCALE **3/16" = 1'-0"**
 SHEET NUMBER
T-1.0

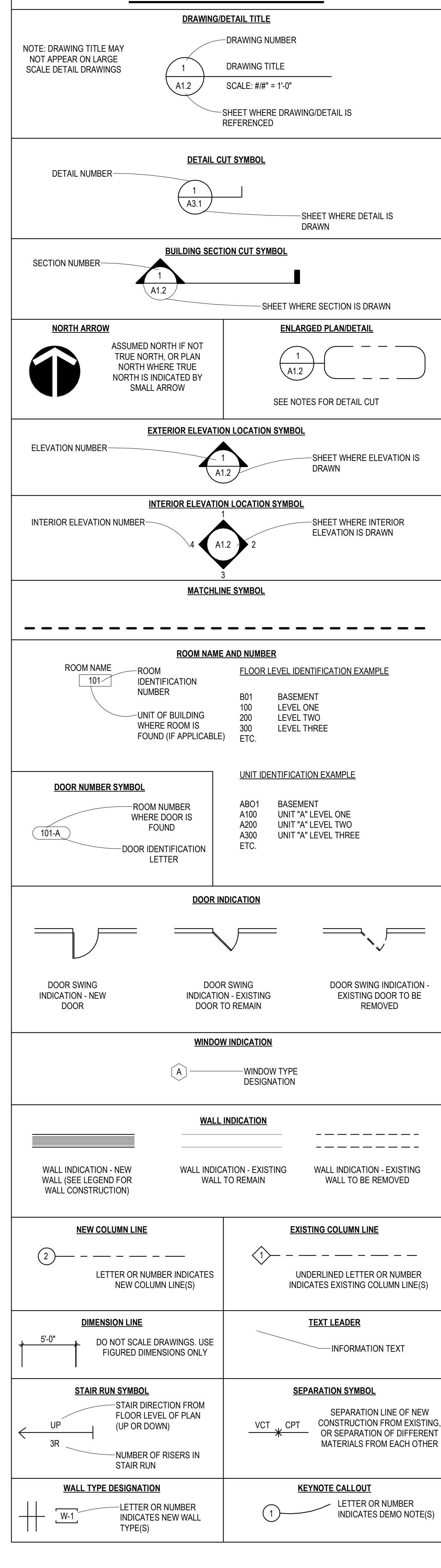
GENERAL LAYOUT INFORMATION AND TYPICAL MOUNTING HEIGHTS



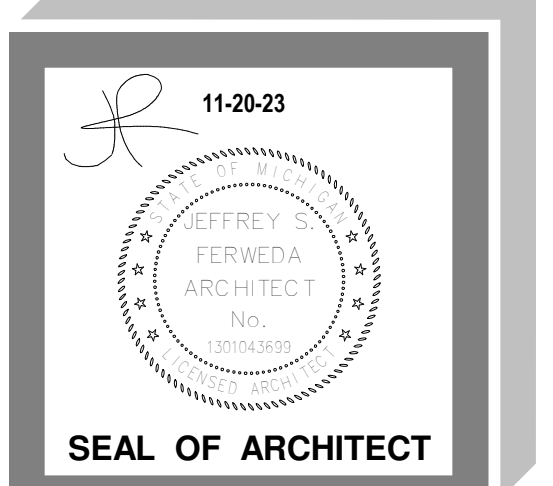
ABBREVIATIONS

A	A/C AIR CONDITIONING AV AUDIO/VIDEO AB ANCHOR BOLTS ABR ABRASIVE ABS ABSORBING AC ACOUSTIC/ACOUSTICAL AC INSUL ACOUSTIC INSULATION ACB AIR CIRCUIT BREAKER ACC ACCESS ACP ACOUSTIC CEILING PANEL ACT ACOUSTICAL CEILING TILE ACU AIR CONDITIONING UNIT AD ADDITION ADA AMERICANS WITH DISABILITIES ACT ADDN ADDITION ADDNL ADDITIONAL ADJ ADJUSTABLE AFF ABOVE FINISH FLOOR AFSS AUTOMATIC FIRE SUPPRESSION SYSTEM AGGR AGGREGATE AHU AIR HANDLING UNIT AL CLD ALUMINUM-CLAD AL ALUMINUM ALT ALTERNATE AM PNL ACOUSTICAL METAL PANEL AMP AMPERE AMPL AMPLIFIER AMT AMOUNT ANCH ANCHOR, ANCHORAGE ANCHS ANCHORS ANOD ANODIZED AP ACCESS PANEL APPR APPROVED APPROX APPROXIMATE APT APARTMENT ARCH ARCHITECT, ARCHITECTURAL ASB ASBESTOS ASB BD ASBESTOS BOARD ASPH ASPHALT ASPH T ASPHALT TILE ASPT AUTOMATIC SPRINKLER RISER ASST ASSISTANT ASSY ASSEMBLY AUTO AUTOMATIC AUX AUXILIARY AV VIDEO AVG AVERAGE AW ACID WASTE	C	CCL COAT CLOSET CD CEILING DIFFUSER CEM CEMENT CEMPLAS CEMENT PLASTER CER CERAMIC CFM CUBIC FEET PER MINUTE CG CORNER GUARD CHD CHECKERED CHG CHANGE CHKD PL CHECKERED PLATE CHWR CHILLED WATER RETURN CHWS CHILLED WATER SUPPLY CI CAST IRON CIF CAST IRON FRAME CIP CAST IRON PIPE CIRC CIRCUIT CIRCUM CIRCUMFERENCE CJ CONTROL OR CONSTRUCTION JOINT CL CLASS, CLOSET CLG CEILING CLGHT CEILING HEIGHT CLR CLEAR CLR GL CLEAR GLASS CLR WGL CLEAR WOOD GLASS CLRM CLASSROOM CMU CONCRETE MASONRY UNIT CNTR COUNTER CNVYR CONVERTOR CO ANCHOR, COMPANY COEF COEFFICIENT COL COLUMN COMPD COMPOSITION COMPRESS COMPRESSOR COMPRT COMPARTMENT CONC CONCRETE COND CONDUIT, CONDITION, CONDENSATE, CONDUCTOR CONF CONFERENCE CONN CONNECTION, CONNECTION CONSTR CONSTRUCTION CONT CONTINUE, CONTINUOUS CONTR CONTRACTOR CONV CONVECTOR COR CORNER CORR CORRIDOR, CORRUGATED COVER COVER COVPL COVER PLATE CP CONTROL PANEL CPT CARPET CRS COURSE, COURSES CSTG CASTING CT CERAMIC TILE CTR CENTER CTSX COUNTERSINK (SUNK) CUFT CUBIC FEET (FOOT) CUH CABINET UNIT HEATER CULV CULVERT CW COLD WATER CWR CONDENSING WATER RETURN CWS CONDENSING WATER SUPPLY CY CUBIC YARD, CYCLES CYL CYLINDER	E	EJ EAST EJ EXPANSION JOINT EA EACH ED EXHAUST DUCT EF EACH FACE, EXHAUST FAN EG EXHAUST GRILLE EH EXTRA HEAVY EHS EXTENDED INSULATION FINISH SYSTEM EL ELEVATION ELEC ELECTRIC, ELECTRICAL ELEC CAB ELECTRIC CABINET ELEC CL ELECTRIC CLOSET ELEC OPER ELECTRICALLY OPERATED ELEV ELEVATOR EMERG EMERGENCY ENCL ENCLOSURE ENGR ENGINEER ENT ENTRANCE, ENTRY EP ELECTRICAL PANEL, EPOXY EPODM ETHYLENE PROPYLENE DIENE MONOMER EQ EQUAL EQUIP EQUIPMENT EQUIV EQUIVALENT ER EXHAUST REGISTER ERU ELASTOMERIC SHEET MEMBRANE ESMW WATERPROOFING ESP EXTERNAL STATIC PRESSURE EW EACH WAY EWC ELECTRIC WATER COOLER EXC EXCAVATED EXH EXHAUST EXIST EXISTING EXP EXPANSION EXP B EXPANSION BOLT COMPRT COMPARTMENT EXPL P EXPLOSION PROOF EXTN EXTENSION EXTR EXTRUDED	F	F/F FACE TO FACE FA FIRE ALARM FACP FIRE ALARM CONTROL PANEL FC FLEXIBLE CONNECTION FDR FIRE DAMPER, FLOOR DRAIN FDN FOUNDATION FDR FEEDER FEB FIRE EXTINGUISHER AND BRACKET FEC FIRE EXTINGUISHER CABINET FFIN FINISH FGL FIBER GLASS FH FIRE HYDRANT FHC FIRE HOSE CABINET FHCL FIRE HOSE CLOSET FHMS FLAT HEAD MACHINE SCREW FHR FIRE HOSE RACK FHW S FLAT HEAD WOOD SCREW FIN FINISH, FINISHED FIXT FIXTURE FL FIRE LINE FLASH FLASHING FLDG FOLDING FLR FLOOR FLR FIN FLOOR FINISH FLUOR FLUORESCENT FMBD FORMBOARD FPM FEET PER MINUTE FRFRG FIREPROOFING FR FRAME FR/COV FRAME AND COVER FRP FIBERGLASS REINFORCED PLASTIC OR POLYESTER FRT FIRE RETARDANT TREATED FRZR FREEZER FS FAR SIDE, FULL SIZE FSUP FIRE SUPPRESSION FT FEET, FOOT FTG FOOTING FTR FINISHED TUBE RADIATION FURN FURNISH, FURNISHED FURR FURRED, FURRING FUT FUTURE	G	GT GREASE TRAP GV & B GATE VALVE AND BOX GYP GYPSUM GYP BD GYPSUM BOARD	H	HAGL HEAT ABSORBING GLASS HB HOSE BIBB HD HEAD HDWD HARDWOOD HDWE HARDWARE HEX HEXAGON HGT HEIGHT HHWR HEATING HOT WATER RETURN HHWS HEATING HOT WATER SUPPLY HID HIGH INTENSITY DISCHARGE HM HOLLOW METAL HO HUB OUTLET HORIZ HORIZONTAL, HORIZONTALLY HOSP HOSPITAL HSP HIGH POINT, HORSEPOWER HPR HIGH PRESSURE HR HANDRAIL HRU HEAT RECOVERY UNIT HS HIGH STRENGTH HSB HIGH STRENGTH BOLT HTG HEATING HTR HEATER HVC HIGH VOLTAGE HVL HOSE VALVE CABINET HW HOT WATER HWR HOT WATER RETURN HWY HIGHWAY HYD HYDRANT	I	ID INSIDE DIAMETER IE INVERT ELEVATION IN OR * INCH OR INCHES INCANDESC INCANDESCENT INCL INCLINATOR INCLD INCLUDE, INCLUDING INFO INFORMATION INSTL INSTALL, INSTALLATION INSUL INSULATE, INSULATION INT INTERIOR INTER INTERMEDIATE INV INVERT IW INDIRECT WASTE	J	JB JUNCTION BOX JC JANITOR CLOSET JT JOIST JT JOINT	K	KIP KIP (1,000 LB) K FLASH KOP KNOCK OUT PANEL KP KICKPLATE KV KILOVOLT KVA KILOVOLT AMPERE KILOWATT KILOWATT	L	L ANGLE LAB LABORATORY LAD LADDER LAM LAMINATE, LAMINATED LAV LAVATORY LBL LABEL LBS OR # POUNDS LD LINEAR DIFFUSER LDG LANDING LDY LANDING LEV LEVEL LF LINEAR FEET LG LONG LGR LARGE LGTH LENGTH LH LEFT HAND LIM MET CLG LINEAR METAL CEILING LKR LOCKER LL LIVE LOAD LHL LONG LEG HORIZONTAL LVL LONG LEG VERTICAL LO LOUVER OPENING LP LIGHTING PANEL, LOW POINT LPG LIQUID PROPANE GAS LPR LOW PRESSURE LUBG LUBRICATING PLF POUNDS PER LINEAR FOOT PLYWD PLYWOOD PNL PANEL POL POLISH, POLISHED PORC PORCELAIN PORCENAM PORCELAIN ENAMEL	M	M THOUSAND M & S MIRROR AND SHELF MACH MACHINE MACH RM MACHINE ROOM MAINT MAINTENANCE MAR MARBLE MAS MASONRY MATL MATERIAL MAU MAKE-UP AIR UNIT MAX MAXIMUM MB MACHINE BOLT MBT MARBLE THRESHOLD MC MEDICAL CABINET MDS METAL DIVIDING STRIP MECH MECHANICAL MED MEDIUM MEMB MEMBRANE MES METAL EDGE STRIP MET METAL MET WP METALLIC WATERPROOFING METCL METAL CLAD MEZZ MEZZANINE MFR MANUFACTURER MH MANHOLE MIL MISCELLANEOUS IRON MIN MINIMUM MIR MIRROR MISC MISCELLANEOUS MK MARK ML METAL LATH ML & P METAL LATH AND PLASTER MLDG MOLDING MO MASONRY OPENING MOD MOTOR OPERATED DAMPER MRR MOLDED RESIN RECEPTOR MRT MOLDED RESIN COUNTERTOP MT METAL THRESHOLD MTD MOUNTED MULL MULLION	N	N NORTH NAT NATURAL NEUT NEUTRAL NIC NOT IN CONTRACT NO OR # NO OR NUMBER NOM NOMINAL NOS NOSING NRC NOISE REDUCTION COEFFICIENT NS NEAR SIDE NTS NOT TO SCALE	O	O/O OUT TO OUT OA OUTSIDE AIR OBS OBSOLETE OBS GL OBSOBERE GLASS OC ON CENTER OD OUTSIDE DIAMETER OFF OFFICE OHD OVERHEAD OHD CLNG OVERHEAD COLING DOOR OHD DR OVERHEAD DOOR OP PRTN OPERABLE PARTITION OPG OPENING OPPOSITE OPPOSITE OPPHD OPPOSITE HAND ORN ORNAMENTAL OXY OXYGEN OZ OUNCE	P	P OR PHASE P/C PRECAST PA PUBLIC ADDRESS PARA PARAGRAPH PASS PASSAGE PB PUSH BUTTON PC PIECE, POINT OF CURVATURE PCF POUNDS PER CUBIC FOOT PCL PRECAST LINTEL PCS PIECES PERF PERFORATED PERIM PERIMETER PERM PERMANENT PERP PERPENDICULAR PG PRESSURE GAUGE PLV PLASTIC OR POLYESTER VALVE PL GL PLATE GLASS LDG LANDING PL LAM PLASTIC LAMINATE PL PLATE PLAS PLASTER PLAT PLATING PLUMB PLUMBING PLF POUNDS PER LINEAR FOOT PLYWD PLYWOOD PNL PANEL POL POLISH, POLISHED PORC PORCELAIN PORCENAM PORCELAIN ENAMEL	Q	QT QUARRY TILE QTR QUARTER QTY QUANTITY	R	RA RETURN AIR RAD RADIUS, RETURN AIR DUCT RAF RETURN AIR FAN RC ROOF CONDUCTOR RD ROOM, ROOF DRAIN REC RECESS RECPRT RECEPTACLE RECT RECTANGLE, RECTANGULAR REDUCY REDUCER REF REFLECTED, REFLECTIVE REFR REFRIGERATION, REFRIGERATOR REG REGISTER REIN REINFORCE, REINFORCING, OR REINFORCEMENT REM REMOVE, REMOVABLE REQD REQUIRED RESIL RESILIENT RET RETURN REV REVISION RFG ROOFING RH RIGHT HAND RHK ROBE HOOK RHMS ROUND HEAD MACHINE SCREW RHWS ROUND HEAD WOOD SCREW RM ROUGH RO ROUGH OPENING ROW RIGHT OF WAY RPM REVOLUTIONS PER MINUTE RR RAILROAD RSD ROOF SUMP RT RUBBER TILE RV ROOF VENTILATOR RWC RAIN WATER CONDUCTOR	S	S SOUTH S & P SHELF AND POLE SAN SANITARY SCD SEAT COVER DISPENSER SCHED SCHEDULE SD SOAP DISPENSER, SPLITTER DAMPER, SUPPLY DIFFUSER, SUPPLY DUCT SECT SECTION SER SERVICE SF SQUARE FEET (FOOT), SUPPLY FAN, SWAY FRAME SG SUPPLY GRILLE SGFT STRUCTURAL GLAZED FACING TILE SGL SINGLE SHT SHEET SHT MET SHEET METAL SHWR SHOWER SIM SIMILAR SK SINK SND SEMI-RECESSED SANITARY NAPKIN DISPENSER SNR SANITARY NAPKIN RECEPTACLE SP SPACE(S), STATIC PRESSURE SPEC SPECIFICATIONS SPKLR SPRINKLER SPKR SPEAKER SQR SQUARE FEET (FOOT) SQ FT. SQUARE FEET SR SUPPLY REGISTER SS SERVICE SINK SSTL STRUCTURAL STEEL ST STREET ST STL STAINLESS STEEL STA STATION STAG STAGGERED	T	T THERMOSTAT, TREAD, TOILET TOP AND BOTTOM T & B TONGUE AND GROOVE T/C TOP OF COVER/CURB T/R TOP OF RAIL T/S TOP OF STEEL TBD TRENCH DRAIN TD TOP ELEVATION TE TECHNICAL TEL TELEPHONE TELCAB TELEPHONE CABINET TEMP TEMPERATURE TEMPGL TEMPERED GLASS TERR TERRAZZO THD THREAD, THREADED THK THICK, THICKNESS THRES THRESHOLD THRU THROUGH TOIL TOILET TOIP TOILET PAPER DISPENSER TRAN TRANOM TT TWIN TEE TU TERMINAL UNIT TV TELEVISION, TURNING VANE TVM TELEVISION MONITOR TYP TYPICAL	U	U UNDERGROUND UH UNIT HEATER UL UNDERWRITERS' LABORATORIES, INC. ULT ULTIMATE UNFIN UNFINISHED UNF UNLESS OTHERWISE NOTED UR URINAL, URETHANE USUB UNIT SUBSTATION UV UNIT VENTILATOR	V	V VENT, VOLT(S) V PLAS VENEER PLASTER VAC VACUUM VAT VINYL ASBESTOS TILE VB VALVE BOX VC VERTICAL CURVE VCO VACUUM CLEANER OUTLET VCP VITRIFIED CLAY PIPE VCT VINYL COMPOSITION TILE VD VOLUME DAMPER VEH VEHICLE VENT VENTILATE, VENTILATED, VENTILATION VERT VERTICAL, VERTICALLY VEST VESTIBULE VIN VINYL VIN FAB VINYL FABRIC VIT VITREOUS VITSURF VITREOUS SURFACING VOL VOLUME VTR VAPOR TIGHT VTR THROUGH ROOF VVC VINYL WALL COVERING VVF VERIFY IN FIELD	W	WH WALL HYDRANT WHSE WAREHOUSE WIR WROUGHT IRON WL WORKING LINE WM WIRE MESH WO WINDOW OPENING WP WATERPROOFING, WEATHERPROOFING WPT WORKING POINT WPT WEIGHT WV WALL VENT WWF WELDED WIRE FABRIC XFER TRANSFER XFMR TRANSFORMER Y YARD YD YIELD POINT YR YEAR YS YIELD STRENGTH Z ZINC-COATED
----------	--	----------	--	----------	--	----------	---	----------	--	----------	---	----------	--	----------	--	----------	---	----------	--	----------	---	----------	---	----------	--	----------	--	----------	---	----------	---	----------	---	----------	---	----------	---	----------	---	----------	---

DRAWING SYMBOLS



11/20/2023 1:59:16 PM



SEAL OF ARCHITECT
ARCHITECT
SEDGEWICK + FERWEDA ARCHITECTS
 410 East Court Street Flint, MI 48503
 810-238-9647 | www.sfarch.us
 Good clients + Good design + Great architecture



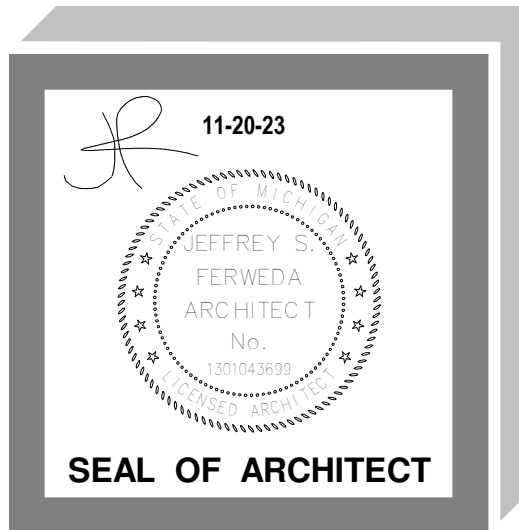
REVISIONS	
11/20/23	REVISION
10/24/23	SITE PLAN RESUBMISSION
9/5/23	SITE PLAN RESUBMISSION
8/8/23	SITE PLAN RESUBMISSION
Date	Description

Project
Long John Silver's
5481 W Main St, Kalamazoo, MI 49009
Drawing
General Notes

DRAWN BY: LJK	CHECKED BY: JSF
PROJECT NUMBER: 23-017	DATE: 07/17/2023
SCALE: 1/2" = 1'-0"	SHEET NUMBER: T-2

NO.	NOTE TEXT
3.01	NEW CONCRETE IN DRIVE THRU LANE, SEE CIVIL PLANS
3.03	SIGN FOOTINGS BY GC, SEE DETAILS ON SHEET AS-2
5.02	BOLLARDS AS INDICATED IN PLAN. BOLLARDS SHALL RECEIVE BOLLARD COVER; SEE DETAIL SHEET AS-2
8.01	DRIVE THRU WINDOW AS SCHEDULED
10.10	DRIVE THRU MENU BOARD AND MAGNET LOOP PROVIDED BY SIGN VENDOR AND INSTALLED BY G.C. SEE SITE AMENITIES DETAILS AND ELECTRICAL PLANS FOR GC PROVIDED ELECTRICAL AND ROUGH-INS
10.11	DRIVE THRU HEIGHT LIMITATION BAR PROVIDED BY SIGN VENDOR AND INSTALLED BY G.C. SEE DETAIL SHEET AS-2
10.12	DRIVE THRU DIRECTIONAL SIGNS, PROVIDED BY SIGN VENDOR AND INSTALLED BY G.C. SEE DETAILS ON AS-2 FOR NEW FOUNDATION IF NECESSARY
10.14	PYLON SIGN BY SIGN VENDOR, SEE DETAIL ON SHEET AS-2
22.04	GREASE INTERCEPTOR, SEE CIVIL PLANS
26.60	EXISTING SITE LIGHTING TO REMAIN
32.07	MASONRY DUMPSTER ENCLOSURE AND SEALED CONCRETE PAD, SEE SHEET AS-3

- GENERAL ARCHITECTURAL SITE PLAN NOTES:**
1. THE ARCHITECTURAL SITE PLAN IS FOR GRAPHIC REPRESENTATION OF THE SITE ONLY AND DOES NOT CONSTITUTE AN ACCURATE SURVEYED PLAN
 2. THE ARCHITECTURAL SITE PLAN HAS BEEN PROVIDED TO DESCRIBE THE SCOPE OF WORK FOR BIDDING PURPOSES ONLY. THE ARCHITECT MAKES NO GUARANTEE THAT THE SITE ELEMENTS, GRADES, UTILITIES, ETC AS DEPICTED ARE ACCURATE OR TRUE.
 3. THE ARCHITECTURAL SITE PLAN IS NOT AN ENGINEERED DRAWING AND IS INTENDED FOR CONSTRUCTION / INSTALLATION OF ARCHITECTURAL SITE AMENITIES. IN THE EVENT GRADES, SWM, OR OTHER SITE ITEMS ARE REQUIRED TO BE MODIFIED, THE GC SHALL BE SOLELY RESPONSIBLE FOR ALL SAID MODIFICATIONS.
 4. THE BUILDING UTILITIES ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY. THE GC SHALL BE RESPONSIBLE FOR THE COORDINATION WITH THE MUNICIPALITY AND THE UTILITY COMPANIES AS WELL AS ANY EXISTING SITE CONDITIONS. THE GC SHALL DETERMINE THE ACTUAL EXTENT OF EXCAVATION AND CONSTRUCTION REQUIRED TO BRING THE NEW SERVICES INTO THE BUILDING.
 5. THE GC SHALL FULLY EVALUATE THE EXISTING FIELD CONDITIONS AND ACTUAL LOCATIONS OF EXISTING UTILITIES TO ENSURE A PROPER CONNECTION FROM NEW TO EXISTING.
 6. EXISTING PAVING, SIDEWALKS, AND/OR LANDSCAPING AREAS DISTURBED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED WHETHER SPECIFICALLY INDICATED IN THE CONTRACT DOCUMENTS OR NOT.
 7. ALL AREAS REPAIRED OR REPLACED SHALL BE DONE IN SUCH A MANNER TO AS TO ENSURE A SMOOTH, CONTINUOUS TRANSITION FROM EXISTING TO NEW. TRANSITIONS FROM EXISTING TO NEW SHALL NOT BE APPARENT WHEN COMPLETE.
 8. WHERE CONCRETE SIDEWALKS AND CURBS ARE DEMOLISHED AND REMOVED TO FACILITATE NEW CONSTRUCTION, CONCRETE SHALL BE REMOVED TO THE NEAREST CONSTRUCTION JOINT.
 9. DAMAGED PORTIONS OF EXISTING SIDEWALK AROUND THE PERIMETER OF THE BUILDING SHALL BE REMOVED AND REPLACED IN KIND.
 10. PROVIDE POSITIVE DRAINAGE FOR ALL WALKWAYS AND PADS PER THE CIVIL PLANS. IF CIVIL PLANS ARE NOT AVAILABLE, THE GC SHALL BE RESPONSIBLE FOR PROVIDING THE REQUIRED SLOPE AT ALL WALKWAYS AND PADS TO ENSURE POSITIVE DRAINAGE.



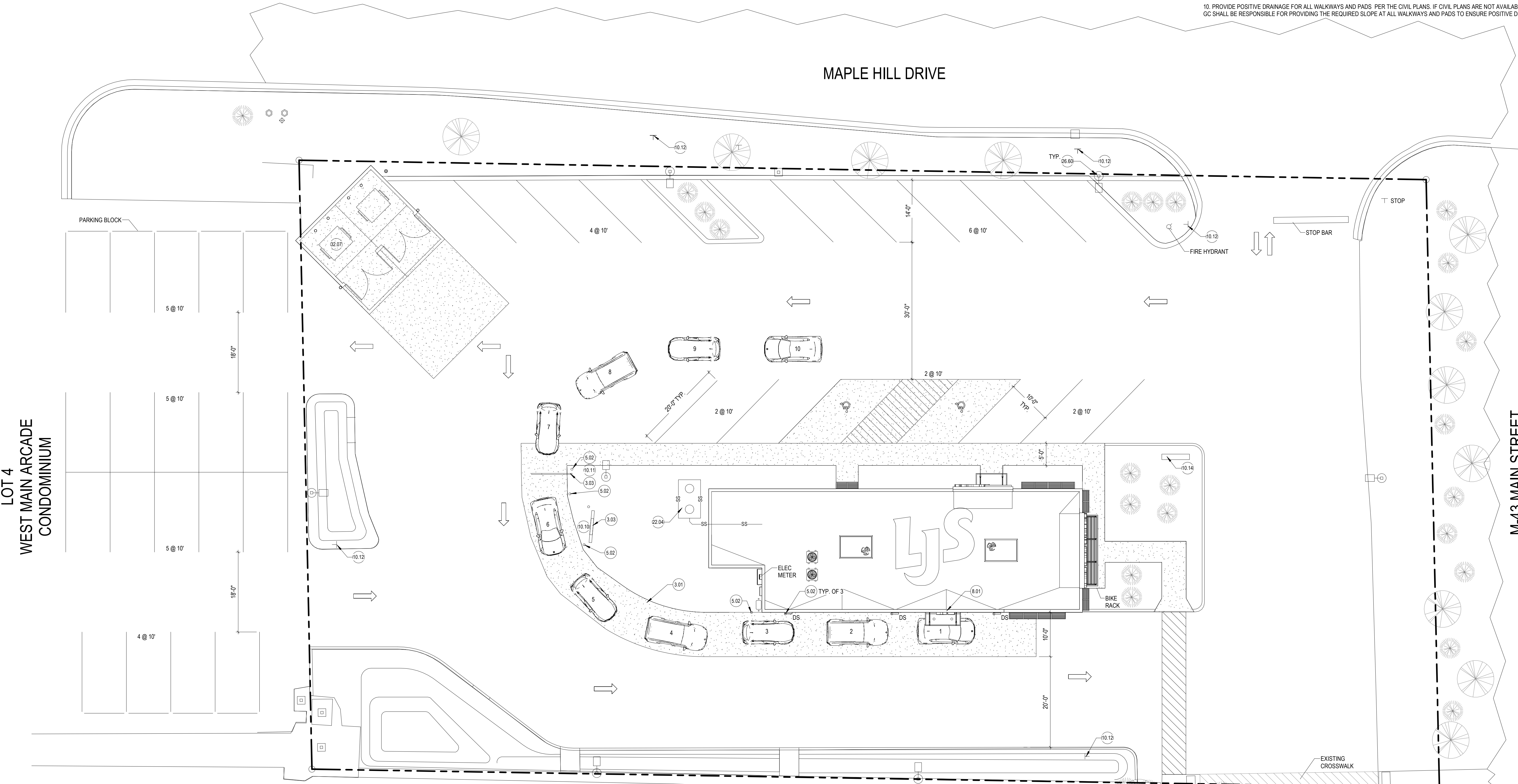
ARCHITECT
SEDGEWICK + FERWEDA ARCHITECTS
 410 East Court Street Flint, MI 48503
 810-238-9647 | www.sfarch.us
 Good clients + Good design + Great architecture



REVISIONS	
11/20/23	REVISION
10/24/23	SITE PLAN RESUBMISSION
9/5/23	SITE PLAN RESUBMISSION
8/8/23	SITE PLAN RESUBMISSION
Date	Description

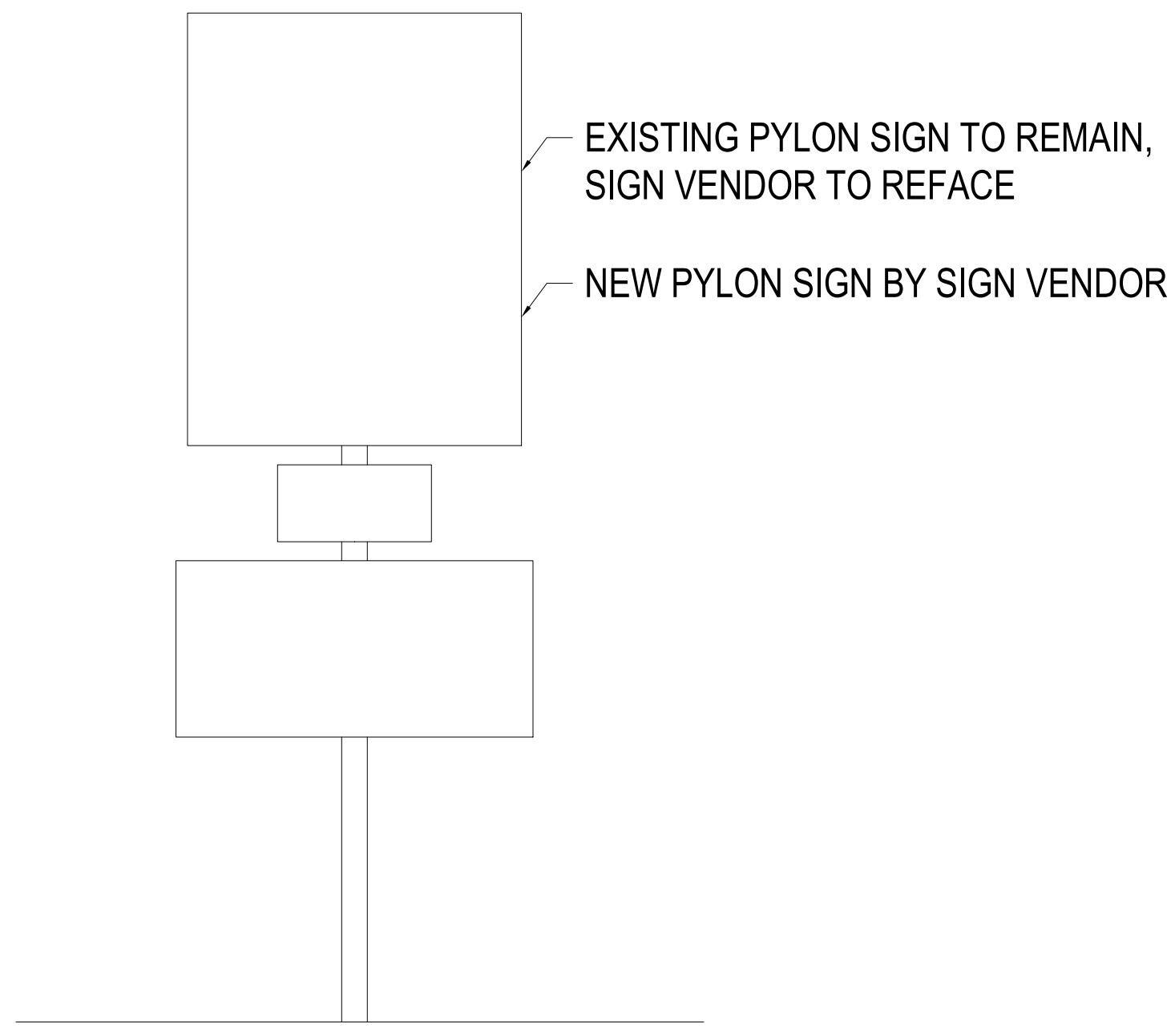
Project
 Long John Silver's
 5481 W Main St, Kalamazoo, MI 49009
Drawing
 Site Amenities Plan

DRAWN BY:	LJK
CHECKED BY:	JSF
PROJECT NUMBER	23-017
DATE	07/17/2023
SCALE	3/32" = 1'-0"
SHEET NUMBER	AS-1

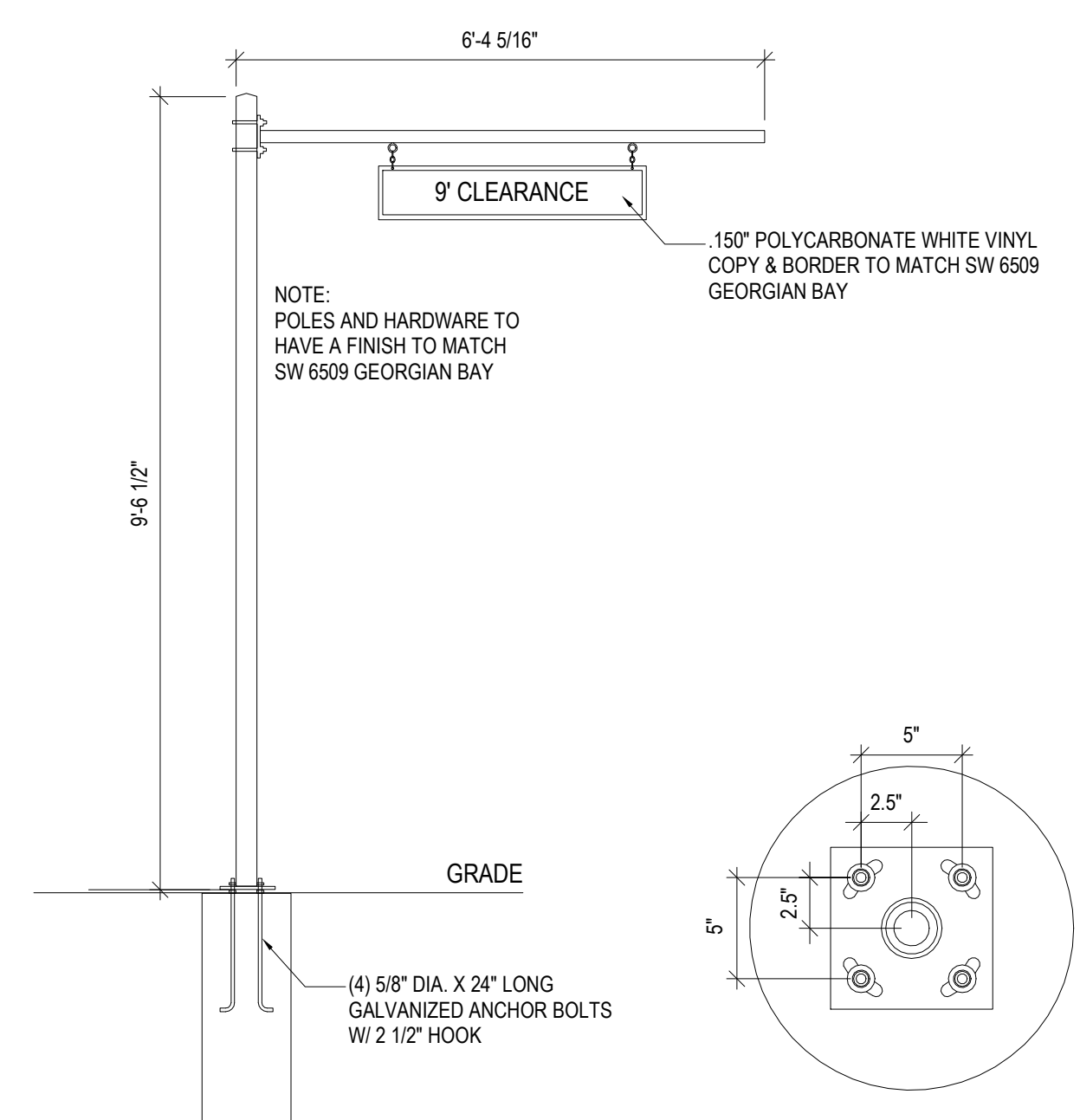


11/20/2023 2:00:12 PM
 LOT 4
 WEST MAIN ARCADE
 CONDOMINIUM
 LOT 2
 ELKS PLAT NO. 05-13-401-020
 1 SUBDIVISION

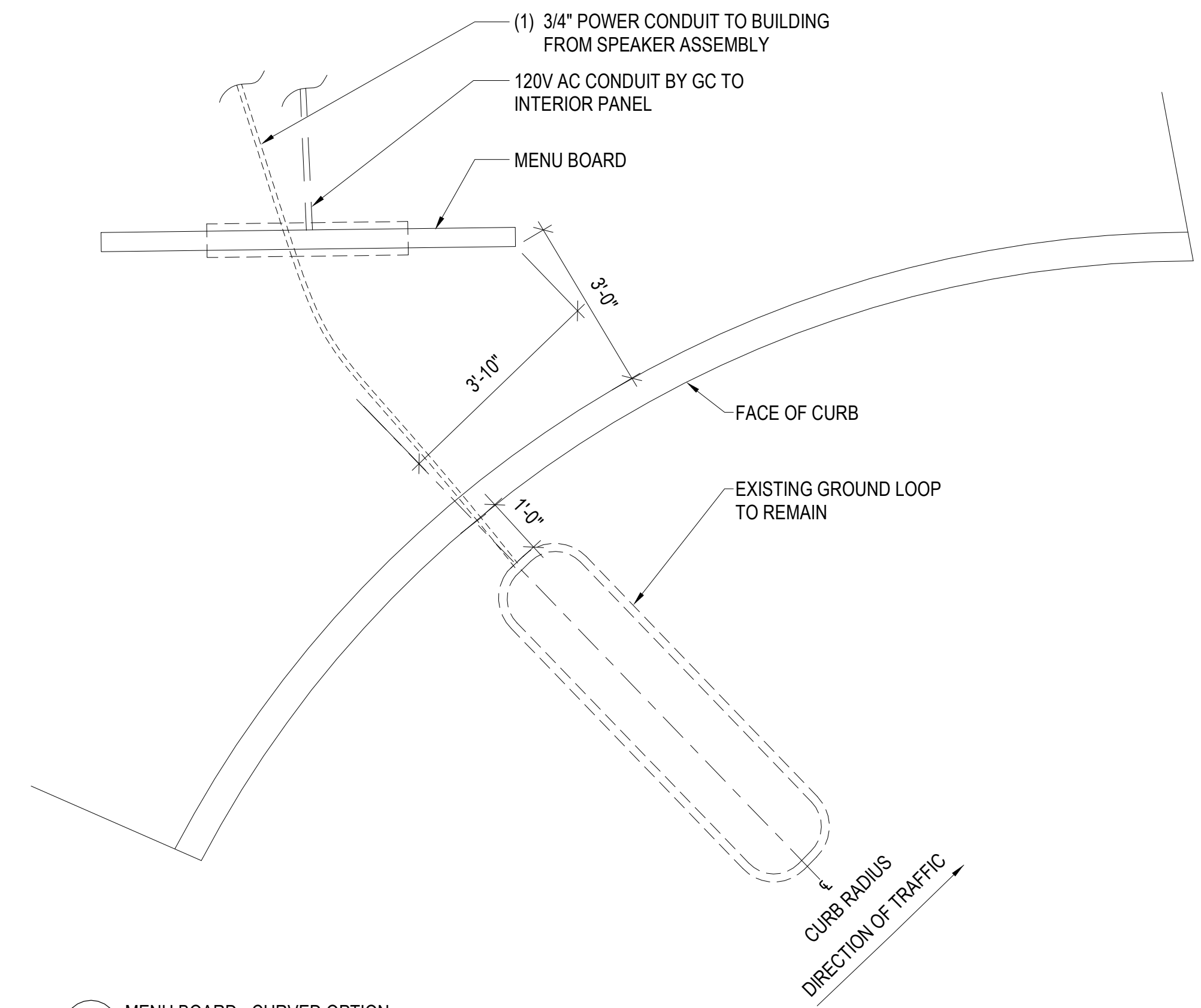




B9 PYLON SIGN DETAIL SCALE: 1/4" = 1'-0"

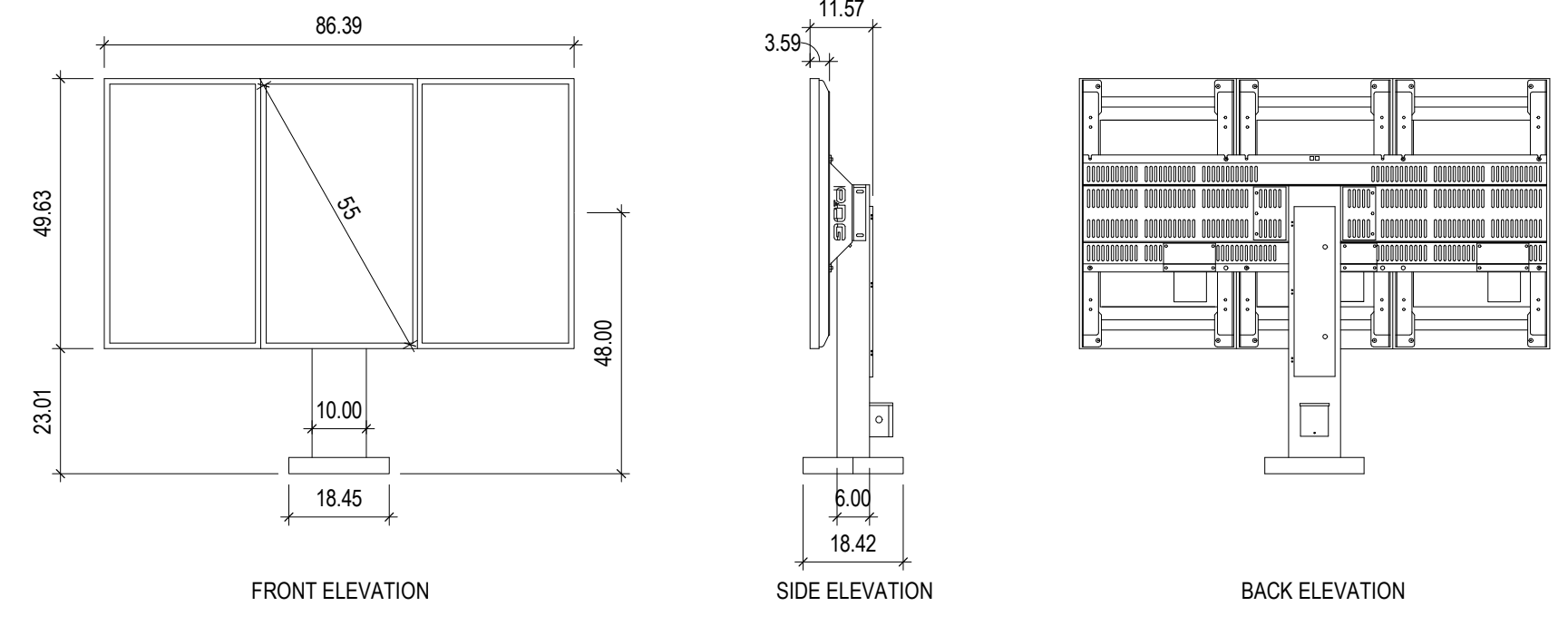


H9 CLEARANCE BAR - FOOTING DETAIL SCALE: 1/2" = 1'-0"

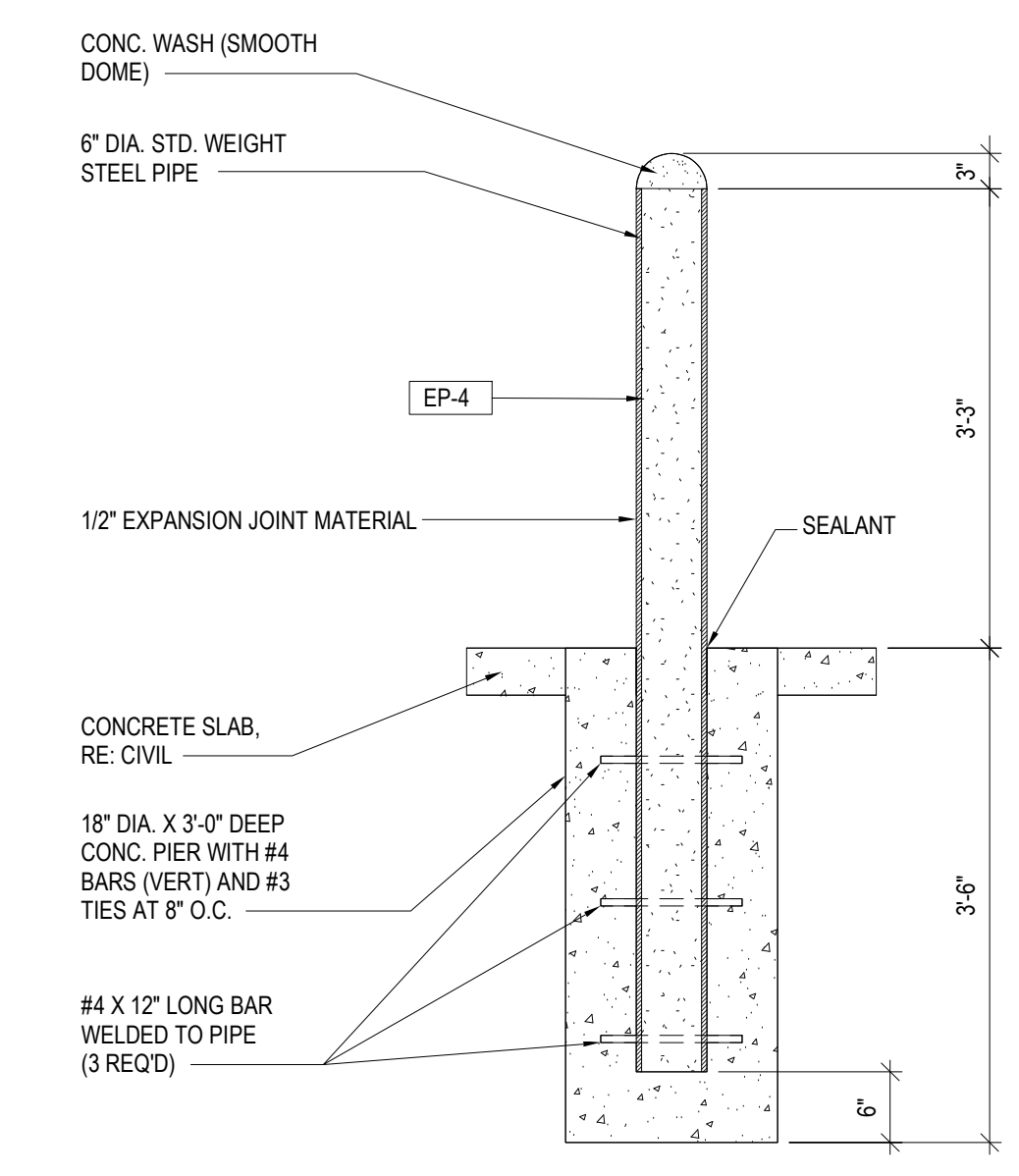


M4 MENU BOARD - CURVED OPTION SCALE: 3/8" = 1'-0"

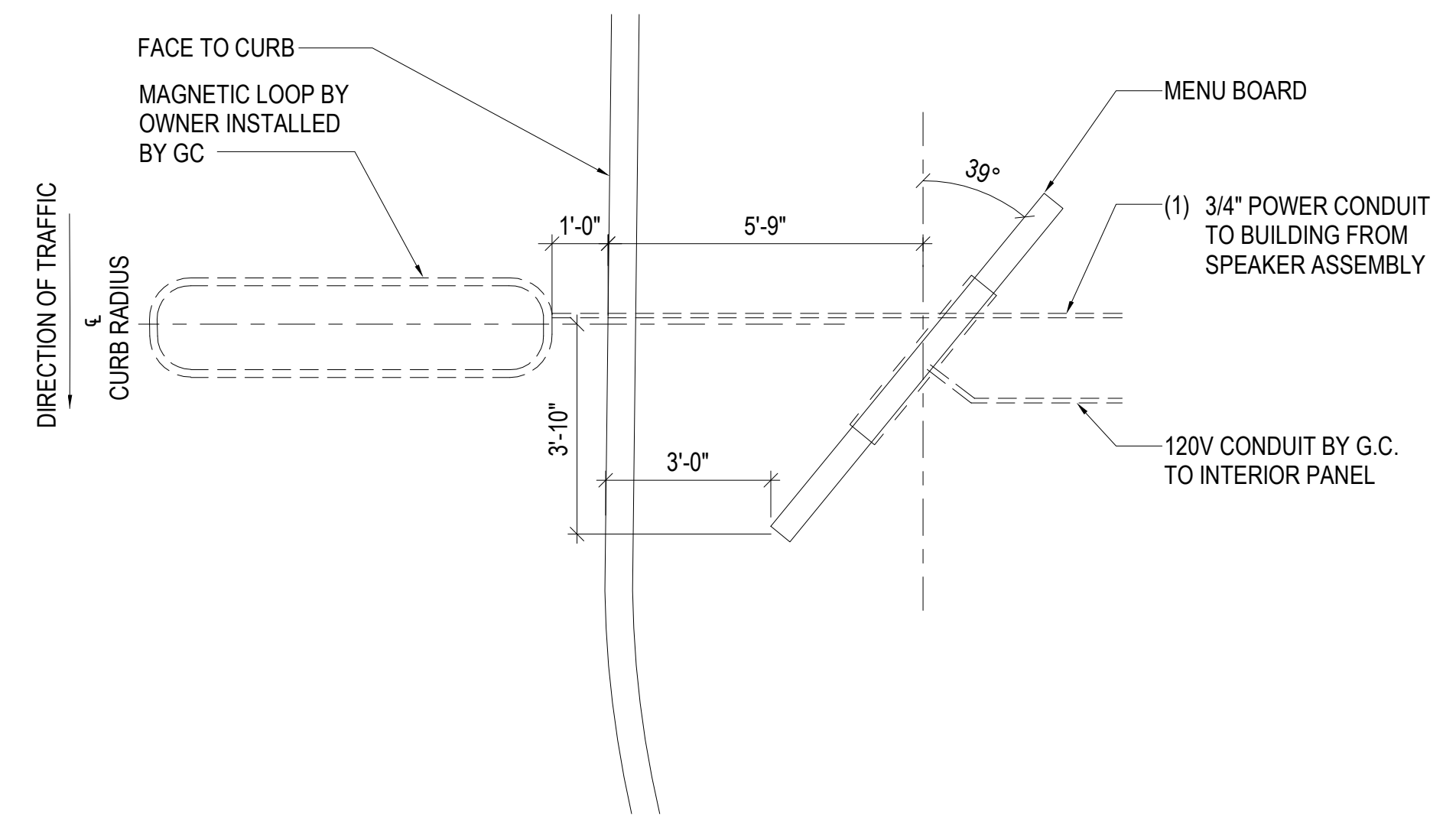
4



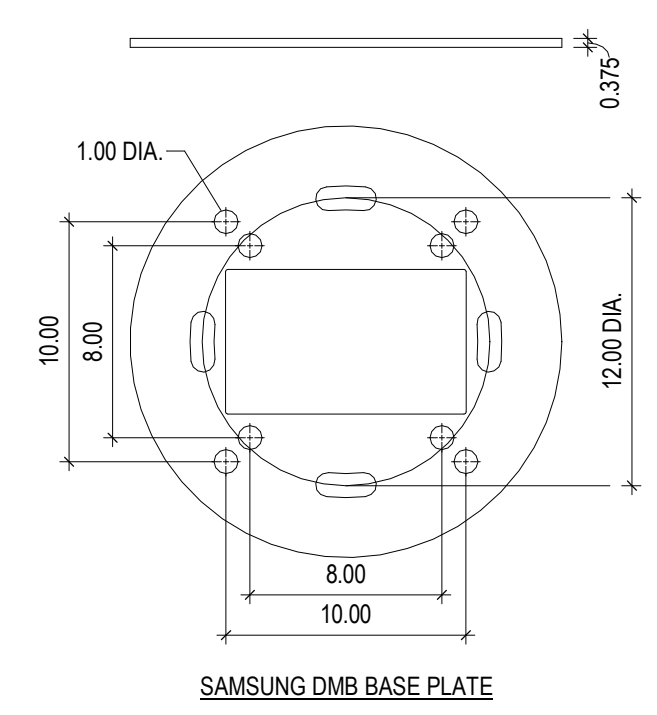
B4 MENU BOARD SCALE: 3/8" = 1'-0"



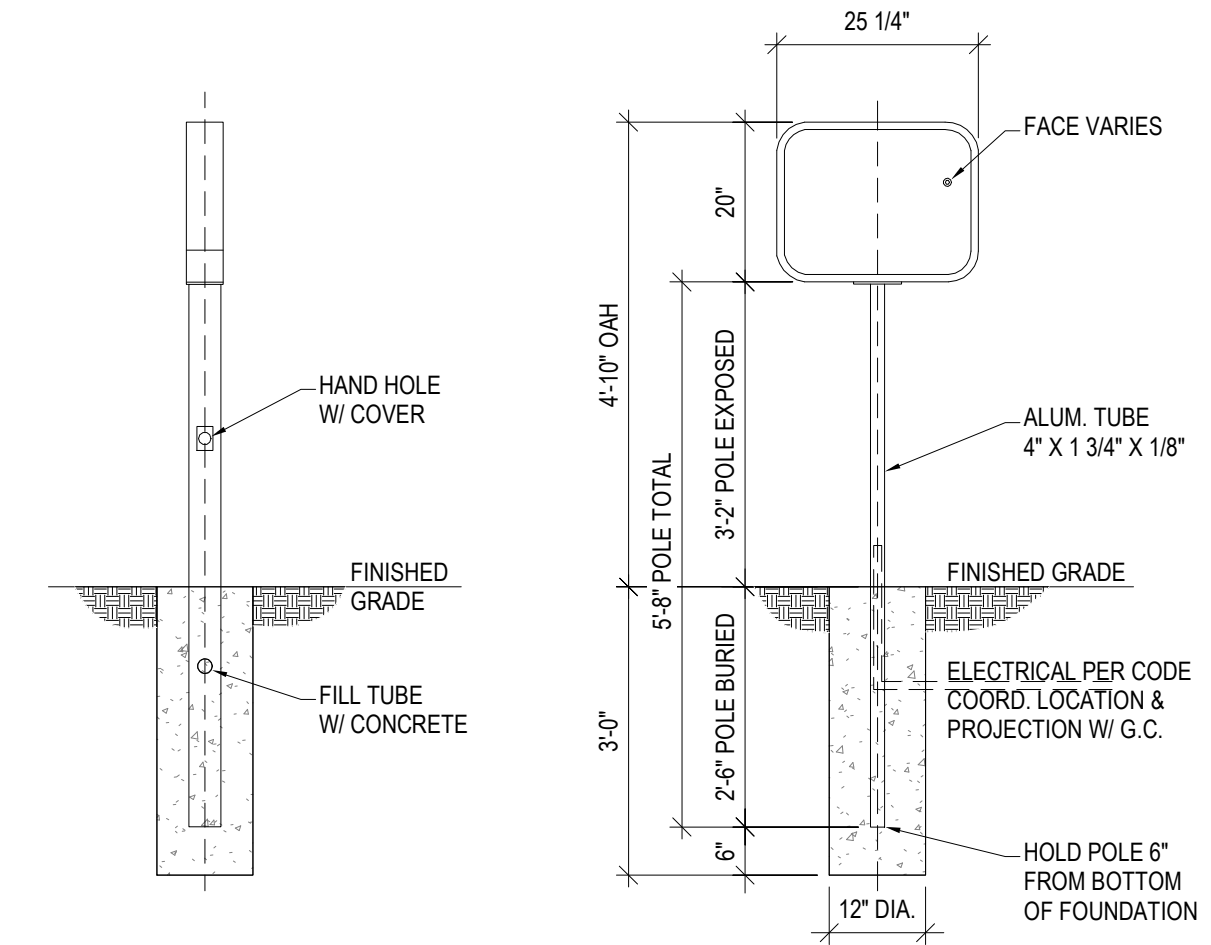
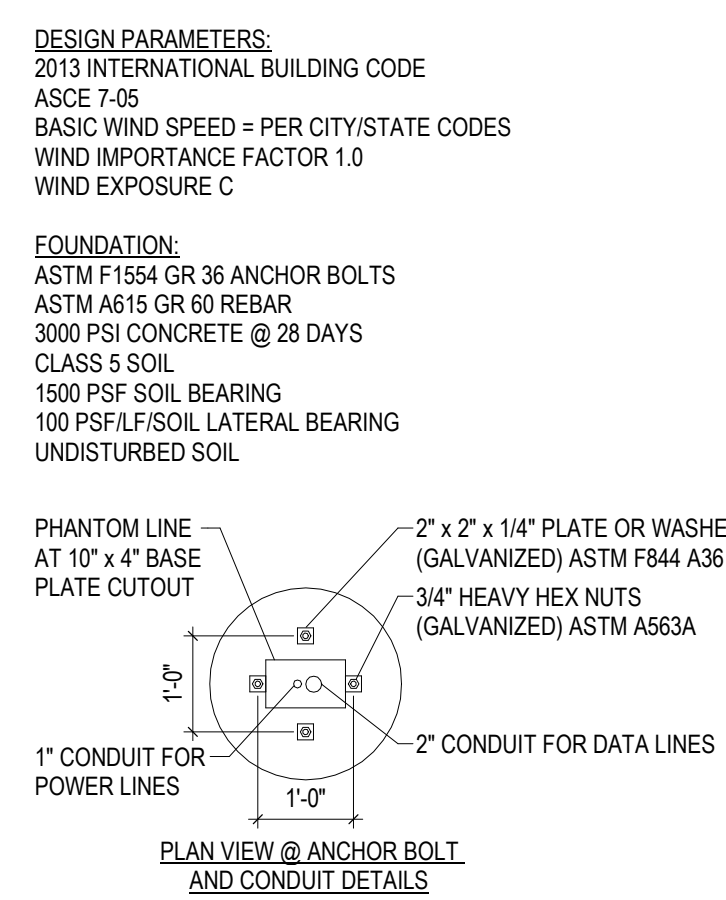
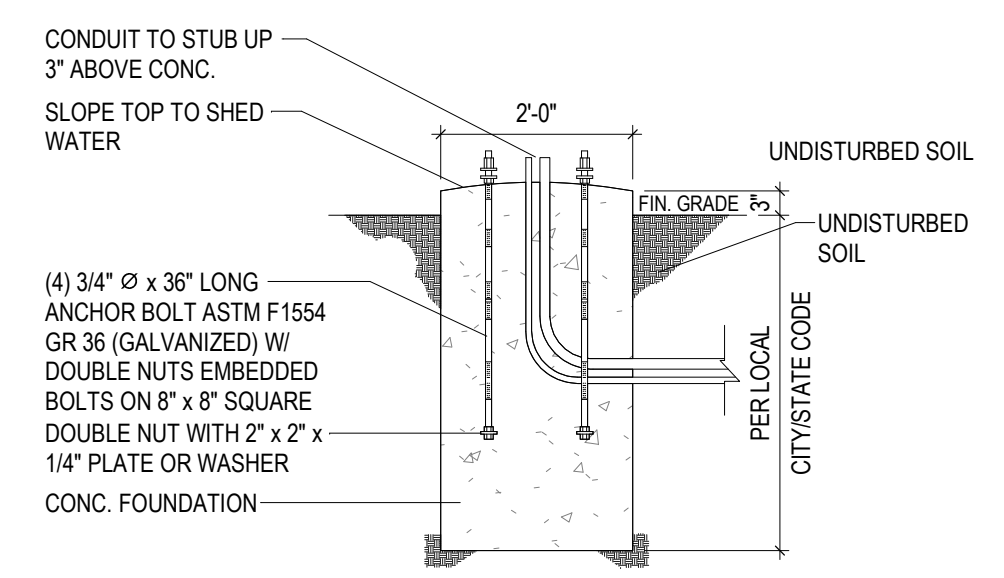
H4 CONC BOLLARD SCALE: 3/4" = 1'-0"



L4 MENU BOARD PLAN - PARALLEL OPTION SCALE: 3/8" = 1'-0"

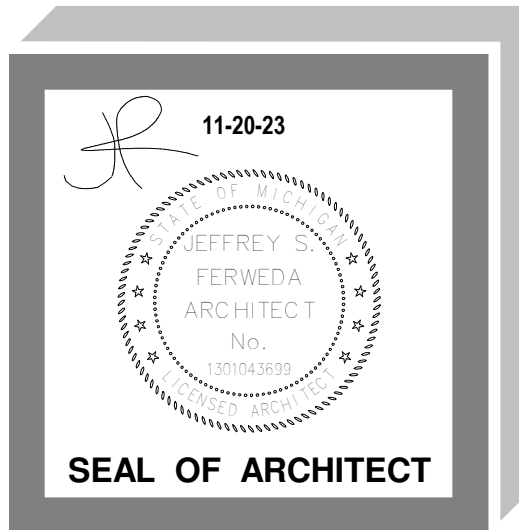


B1 MENU BOARD FOUNDATION SCALE: 1 1/2" = 1'-0"



M1 DIRECTIONAL SIGN FACE SCALE: NTS

11/20/2023 2:00:13 PM



ARCHITECT

SEDGEWICK + FERWEDA ARCHITECTS

410 East Court Street Flint, MI 48503
810-238-9647 | www.sfarch.us

Good clients + Good design + Great architecture



REVISIONS	
11/20/23	REVISION
10/24/23	SITE PLAN RESUBMISSION
9/5/23	SITE PLAN RESUBMISSION
8/8/23	SITE PLAN RESUBMISSION
Date	Description

Project

Long John Silver's
5481 W Main St, Kalamazoo, MI 49009

Drawing

Site Amenities Details

DRAWN BY:	LJK
CHECKED BY:	JSF
PROJECT NUMBER	23-017
DATE	07/17/2023
SCALE	As indicated
SHEET NUMBER	AS-2

11/20/2023 2:00:14 PM

NOTES:

1. LOCATION SHALL BE APPROVED BY THE PUBLIC WORKS DEPARTMENT.
2. AREAS SHALL BE ACCESSIBLE FOR DELIVERY AND COLLECTION.
3. GATES SHALL BE CORRUGATED METAL DOORS AND MUST BE APPROVED BY PUBLIC WORKS DEPARTMENT PRIOR TO INSTALLATION.
4. GATE LATCHES SHALL BE OF THE PLUNGER BAR TYPE OR EQUIVALENT AS APPROVED BY THE PUBLIC WORKS DEPARTMENT.
5. SEE ATTACHED SPECIFICATIONS, NOTES AND PROCEDURES

TRASH ENCLOSURE DETAIL MATERIAL SPECIFICATIONS:

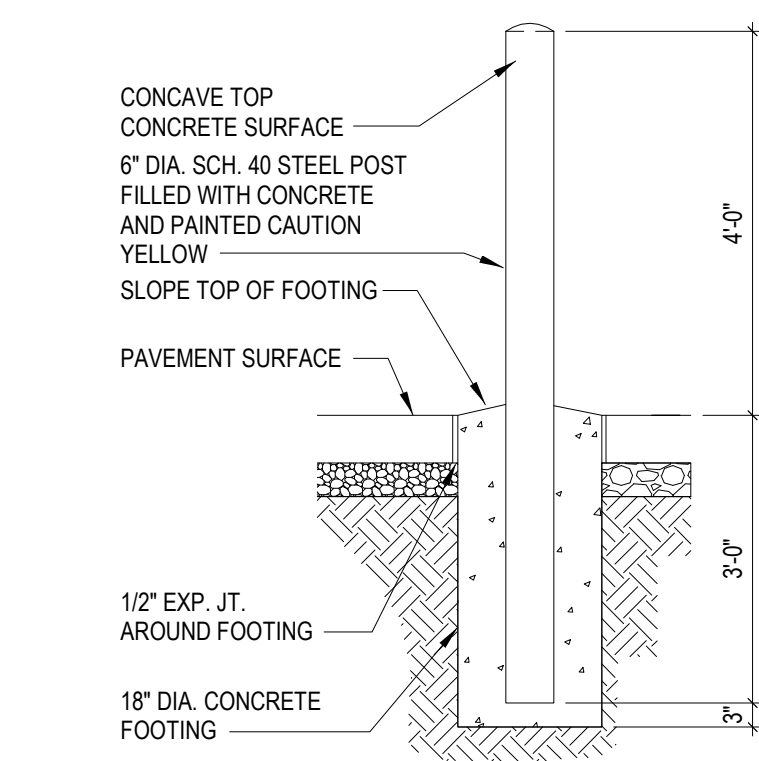
1. CONCRETE BLOCK: 6" MINIMUM IN SIZE. ASTM C90.
2. REINFORCING STEEL: ASTM 615. GRADE 40.
3. ACCEPTABLE SOIL TYPES:
 - A. MIN. ALLOWABLE BEARING: 1000 LBS/SQ. FT
 - B. MIN. LATERAL BEARING: 100 LBS/SQ. FT./FT.
 - C. MAX EXPANSION INDEX: 20
4. CONCRETE: 2000 LBS/SQ. IN. IN 28 DAYS.
5. MORTAR: 1:1/4:3 (PORTLAND CEMENT: HYDRATED LIME OR LIME PUTTY: SAND, BY VOLUME), MIXED TO PLASTIC CONSISTENCY. REFER TO UBC TABLE 24-A FOR OTHER TYPES OF CEMENT.
6. GROUT: 1:1/10:3 (PORTLAND CEMENT: HYDRATED LIME OR LIME PUTTY: SAND BY VOLUME), MIX TO FLOW WITHOUT SEGREGATION. GROUT MAY CONTAIN 2 PARTS PEA GRAVEL (3/8" MAX. SIZE), MINIMUM COMPRESSIVE STRENGTH: 2000 LBS/SQ. IN. REFER TO UBC TABLE 24-B FOR OTHER TYPES OF CEMENT.

GENERAL NOTES:

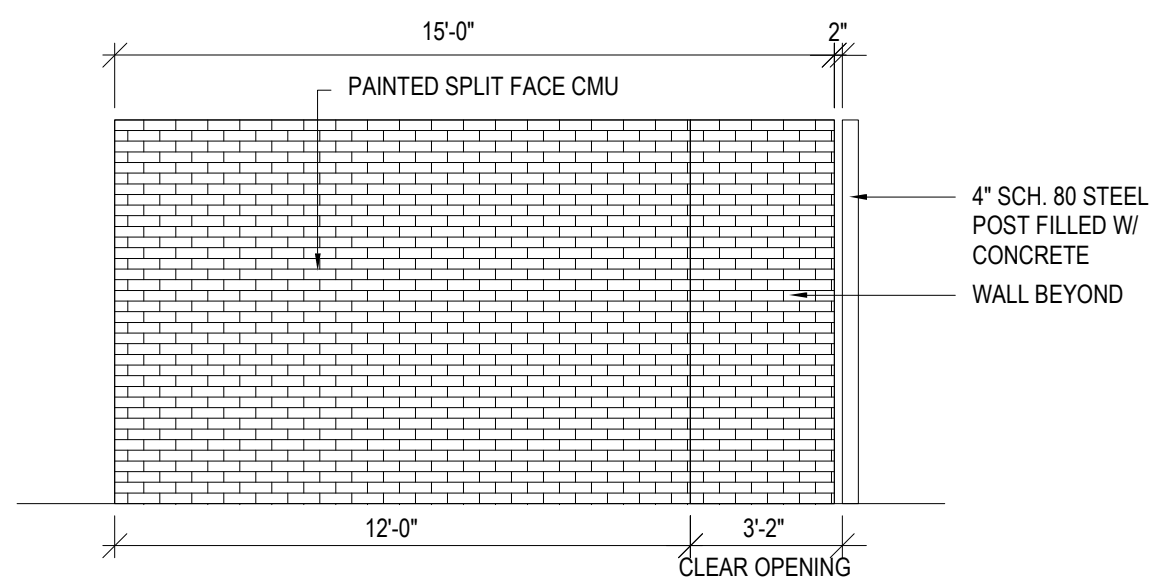
1. THIS DESIGN IS FOR AVERAGE CONDITIONS AND MAY NOT BE SUITABLE FOR ALL CASES. IT IS RECOMMENDED THAT A LICENSED CIVIL OR STRUCTURAL ENGINEER BE CONSULTED.
2. FOOTINGS SHALL BE IN NATURAL SOIL OR CERTIFIED FILL.
3. BLOCKS TO BE STAGGERED (RUNNING BOND).
4. ALL CELLS CONTAINING REINFORCING STEEL SHALL BE GROUTED.
5. APPROVED GROUT STOPS ARE REQUIRED BELOW HORIZONTAL STEEL IN PARTIALLY GROUTED WALLS. BAGS, NEWSPAPERS, ETC. ARE NOT APPROVED GROUT STOPS.
6. INITIAL BED JOINT SHALL BE 1/4" MIN. 1" MAX. SUBSEQUENT BED JOINTS SHALL BE 1/4" MIN., 5/8" MAX.
7. VERTICAL CONTINUITY OF CELLS SHALL BE UNOBSTRUCTED. MORTAR PROJECTIONS SHALL NOT EXCEED 1/2" FOR 6" AND LARGER BLOCK. MORTAR DROPPINGS OR OTHER FOREIGN MATTER ARE NOT PERMITTED IN CELLS AND MUST BE REMOVED.
8. EXPANSION JOINTS REQUIRED AT 60'-0" MAX. INTERVALS.
9. REQUIRED BAR LAPS:
 - A. VERTICAL STEEL: 30 BAR DIAMETERS.
 - B. HORIZONTAL STEEL: 40 BAR DIAMETERS.
 - C. WIRE JOINT REINFORCEMENT IN THE MORTARED BED JOINT: 75 WIRE DIAMETERS OR IN ALTERNATE BED JOINTS OF RUNNING BOND. 54 DIAMETERS PLUS TWICE THE BED JOINT SPACING.
10. WHERE HORIZONTAL WIRE JOINT REINFORCEMENT IS REQUIRED OR UTILIZED, IT SHALL BE EQUIVALENT TO TWO 3/16" DIAMETER BARS CONNECTED AT 16" INTERVALS BY NO. 9 GAUGE WELDED WIRE.
11. FOR PIPES AND CONDUIT EMBEDDED IN MASONRY, REFER TO SEC. 24-07(G), UBC.
12. 3" MIN. COVER REQUIRED FOR REINFORCEMENT IN CONCRETE WHICH IS CAST AGAINST EARTH.

INSPECTION PROCEDURES:

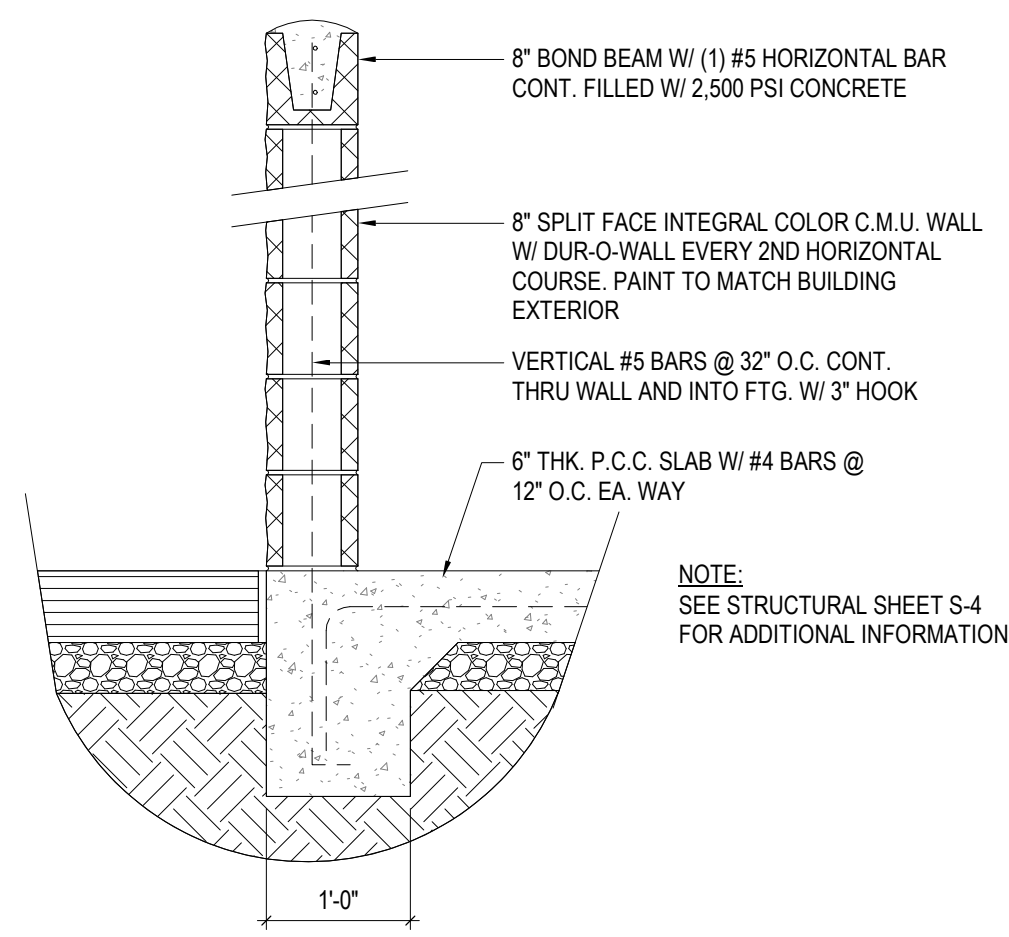
1. FOUNDATION: AFTER TRENCHES ARE DUG, STEEL IS TIED IN PLACE AND BEFORE ANY CONCRETE IS POURED.
2. PREGROUT: AFTER ALL BLOCKS (EXCEPT CAP) ARE IN PLACE, VERTICAL AND HORIZONTAL STEEL IS IN PLACE, GROUT STOPS (FOR PARTIALLY GROUTED MASONRY) ARE IN PLACE, AND PRIOR TO GROUTING.
3. FINAL: AFTER GROUT IS IN PLACE AND PRIOR TO PLACEMENT OF CAP.



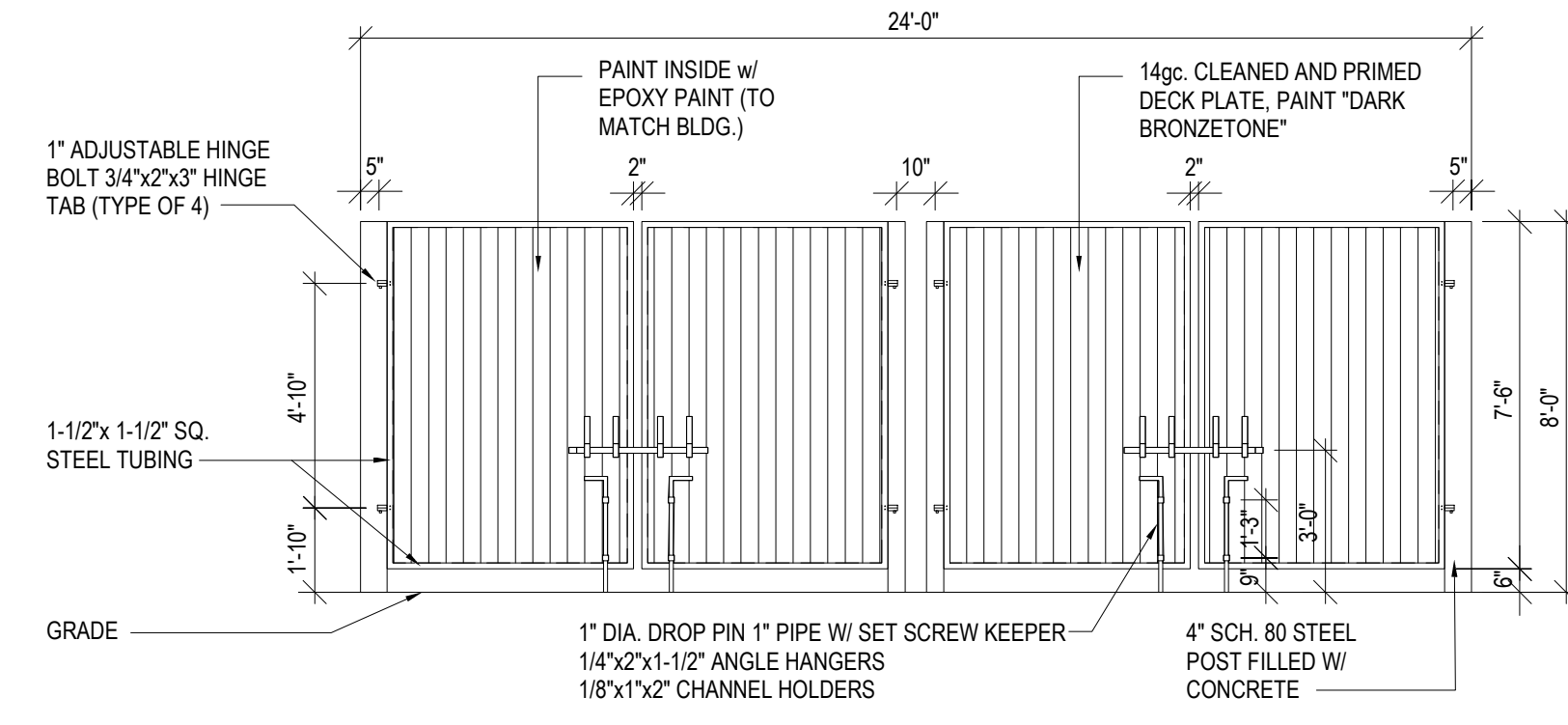
D1 BOLLARD DETAIL SCALE: NTS



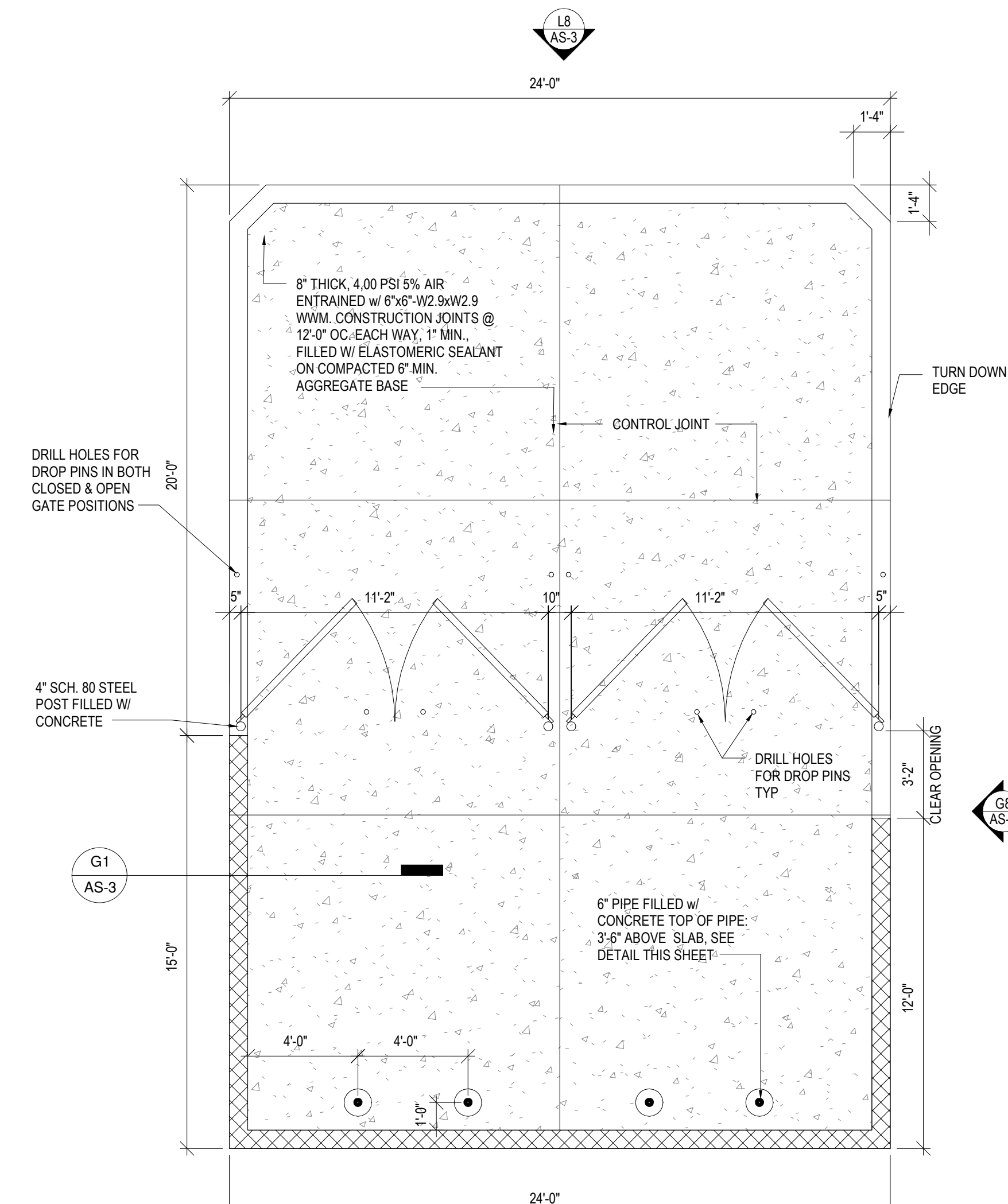
G8 SIDE ELEVATION SCALE: 1/4" = 1'-0"



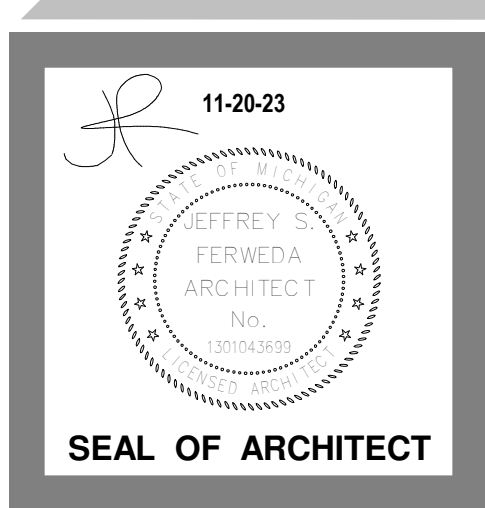
G1 WALL SECTION SCALE: 3/4" = 1'-0"



G5 TYPICAL DOOR SILL DETAIL SCALE: 3/8" = 1'-0"



L1 DUMPSTER ENCLOSURE PLAN SCALE: 1/4" = 1'-0"



ARCHITECT
SEDGEWICK + FERWEDA ARCHITECTS
 410 East Court Street Flint, MI 48503
 810-238-9647 | www.sfarch.us
 Good clients + Good design + Great architecture

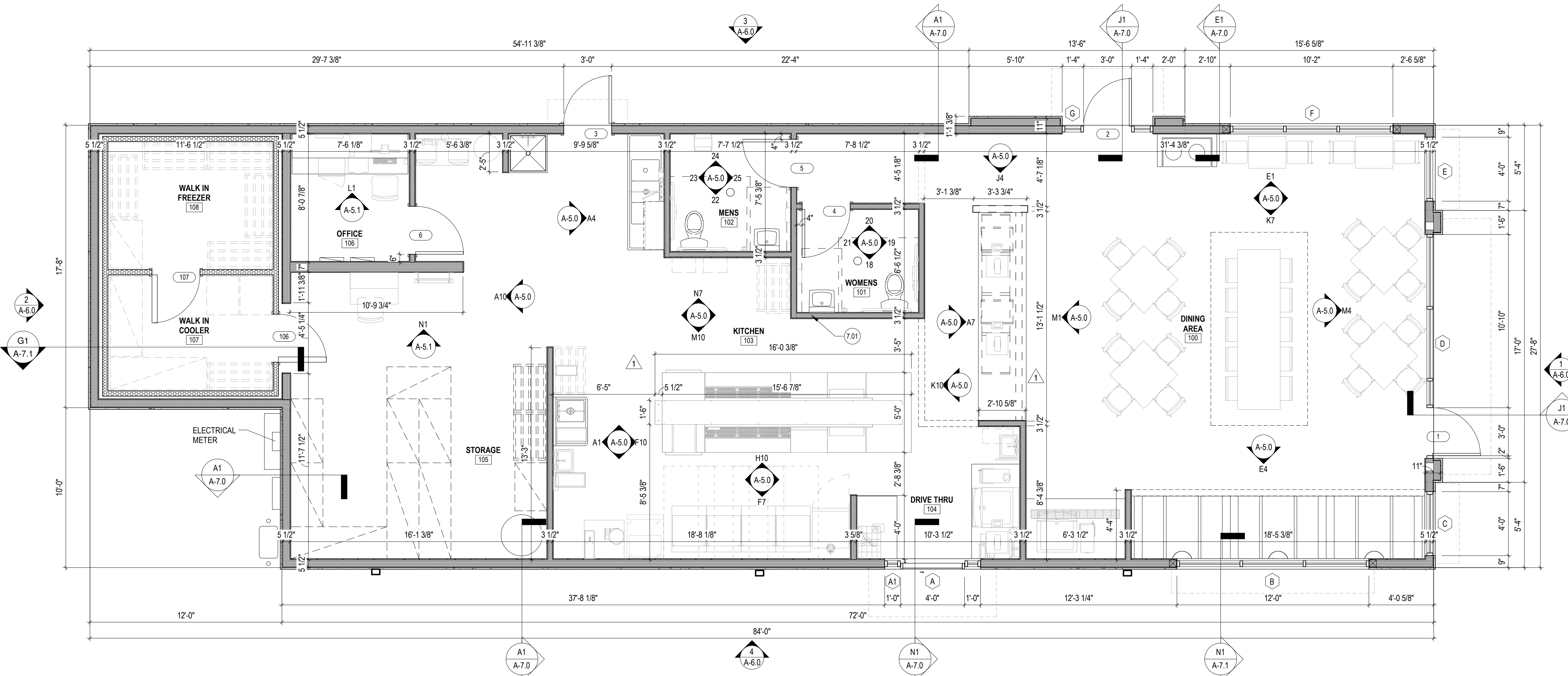


REVISIONS	
Date	Description
11/20/23	REVISION
10/24/23	SITE PLAN RESUBMISSION
9/5/23	SITE PLAN RESUBMISSION
8/8/23	SITE PLAN RESUBMISSION

Project
Long John Silver's
5481 W Main St, Kalamazoo, MI 49009
 Drawing
Dumpster Enclosure Details

DRAWN BY:	LJK
CHECKED BY:	JSF
PROJECT NUMBER	23-017
DATE	07/17/2023
SCALE	As indicated
SHEET NUMBER	AS-3

11/20/2023 2:00:15 PM



1 Proposed Floor Plan
A:10 | 1/4" = 1'-0"

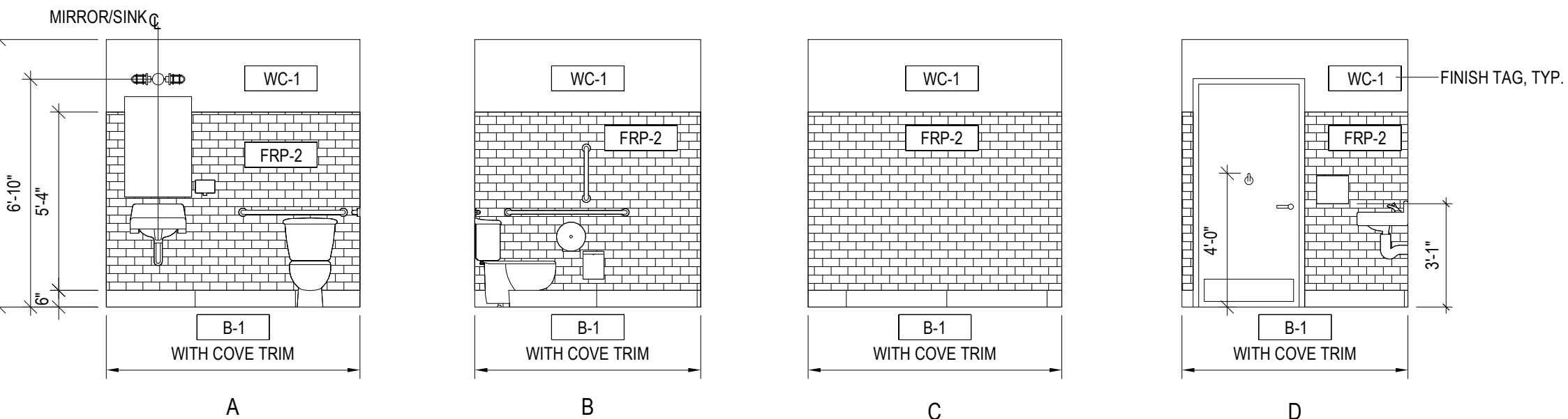
BUILDING ENVELOPE NOTES:

- A. EXTERIOR SHEATHING:**
1. FURNISH & INSTALL EXTERIOR SHEATHING MATERIAL AS INDICATED IN THE CONTRACT DOCUMENTS. INSTALL SHEATHING MATERIAL PER MANUFACTURER'S WRITTEN SPECIFICATIONS.
- B. INSULATION:**
1. FURNISH & INSTALL INSULATION AS INDICATED BELOW & ELSEWHERE IN THE CONTRACT DOCUMENTS. INSULATION SHALL BE INSTALLED PER THE MANUFACTURER'S WRITTEN SPECIFICATIONS.
2. BELOW GRADE INSULATION BOARD
2.1 FOUNDATION WALLS SHALL BE INSULATED FROM TOP OF FOOTING TO TOP OF FOUNDATION WALL WITH EXTRUDED POLYSTYRENE (XPS) RIGID FOAM INSULATION AS MANUFACTURED BY OWING'S CORNING "FOAMULAR 150 EXTRUDED POLYSTYRENE INSULATION" 2" THICK.
2.2 INTERIOR CONCRETE SLABS SHALL BE INSULATED FROM THE INTERIOR FACE OF FOUNDATION WALL TO 2'-0" INTO THE BUILDING & CONTINUOUS AROUND THE PERIMETER OF THE BUILDING WITH EXTRUDED POLYSTYRENE (XPS) RIGID FOAM INSULATION AS MANUFACTURED BY OWING'S CORNING "FOAMULAR 150 EXTRUDED POLYSTYRENE INSULATION" 2" THICK.
3. ABOVE GRADE INSULATION (WALL)
3.1 ALL EXTERIOR WALLS SHALL HAVE BATT INSULATION INSTALLED WITHIN STUD CAVITY. BATT INSULATION SHALL BE R-21, 5 1/2" UNFACED BATT INSULATION AS MANUFACTURED BY OWENS CORNING "ECOTOUCH PINK FIBERGLASS INSULATION". SUBSTRATE SHALL CONTINUE TO UNDERSIDE OF ROOF DECK AT ALL UNFACED BATT INSULATION LOCATIONS
4. ABOVE GRADE INSULATION (ROOF)
4.1 THE ROOF SHALL HAVE CONTINUOUS INSULATION INSTALLED OVER THE ENTIRE ROOF AREA AS INDICATED IN THE CONTRACT DOCUMENTS. ROOF INSULATION SHALL BE R-30 MINIMUM, EXTRUDED POLYSTYRENE ROOF BOARD INSULATION AS MANUFACTURED BY OWENS CORNING "FOAMULAR 25".
- C. AIR INFILTRATION & WEATHER BARRIER:**
1. ALL ABOVE GRADE EXTERIOR WALLS SHALL HAVE AN AIR INFILTRATION & WEATHER BARRIER INSTALLED PER MANUFACTURER'S WRITTEN SPECIFICATIONS ON THE EXTERIOR SIDE OF THE BUILDING SHEATHING. THE AIR INFILTRATION & WEATHER BARRIER SELECTED SHALL BE COMPATIBLE WITH THE EXTERIOR CLADDING SYSTEM.
- D. ROOFING MEMBRANE:**
1. THE ROOFING MEMBRANE SHALL BE A MECHANICALLY FASTENED PVC MEMBRANE INSTALLED PER THE MANUFACTURER'S WRITTEN SPECIFICATION AND AS INDICATED IN THE CONTRACT DOCUMENTS. THE PVC MEMBRANE SHALL BE AS MANUFACTURED BY DURO-LAST "DURO-LAST 50 MIL MEMBRANE".

GENERAL FLOOR PLAN NOTES:

1. TYPICAL SUBSTRATES FOR INTERIOR WALLS SHALL BE AS FOLLOWS:
KITCHEN, OFFICE AND STORAGE AREAS:
5/8" CEMENTITIOUS BOARD TO 12" A.F.F. - 5/8" PLYWOOD FROM 12" A.F.F. TO 6" ABOVE CEILING.
SALES AND SEATING AREAS:
5/8" GYPSUM BOARD
RESTROOMS:
5/8" CEMENTITIOUS BOARD TO 12" A.F.F. - 5/8" GYPSUM BOARD FROM 12" A.F.F. TO 6" ABOVE CEILING
GENERAL CONTRACTOR SHALL PROVIDE ADEQUATE BLOCKING IN BATHROOMS FOR GRAB BARS, LAVATORIES, HAND DRYERS, MIRRORS, PAPER TOWEL DISPENSERS, SOAP DISPENSERS, AND OTHER ACCESSORIES AS REQUIRED UNLESS OTHERWISE NOTED.
2. ALL NEW INTERIOR PARTITIONS SHALL BE 2X4 WOOD STUDS @ 16" O.C. U.N.O.
3. RESTROOM IDENTIFICATION SIGN W/ HANDICAP ACCESSIBLE SYMBOL AND BRAILLE SUPPLIED WITH STORE GRAPHICS PACKAGE. GC TO INSTALL (1) RESTROOM SIGN FOR WOMEN'S / 1 RESTROOM SIGN FOR MEN'S; RESTROOM SIGNAGE MUST BE MOUNTED SUCH THAT THE LOWEST BRAILLE CHARACTER IS 48" MINIMUM A.F.F. AND THE HIGHEST BRAILLE CHARACTER IS 60" MAXIMUM A.F.F. INSTALL 9" FROM STRIKE SIDE OF DOOR.
4. ALL INTERIOR DIMENSIONS SHOWN ARE FROM FACE OF SUBSTRATE TO FACE OF SUBSTRATE.
5. PROVIDE FIRE EXTINGUISHERS, TYPE ABC, WALL HUNG, TOP AT 4'-0" A.F.F. (BY G.C.)
-NUMBER AND LOCATION AS DETERMINED BY LOCAL CODE
6. EQUIPMENT/CASEWORK SUPPLIER TO PLACE ALL NEW EQUIPMENT AND CASEWORK.
7. G.C. TO MAKE FINAL MECHANICAL, ELECTRICAL & PLUMBING CONNECTIONS FOR ALL EQUIPMENT AS INDICATED ON PLANS.

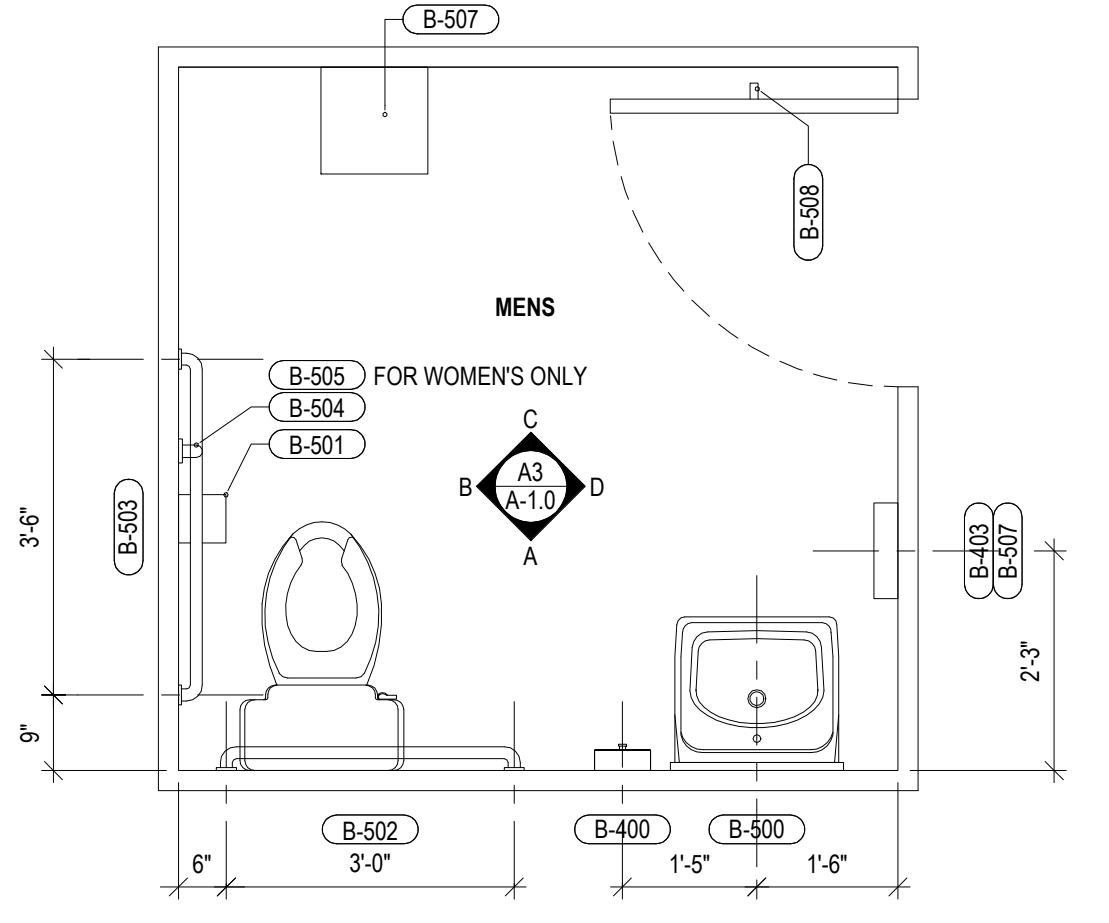
FLOOR PLAN KEY NOTES	
NO.	NOTE TEXT
7.01	PROVIDE 3-1/2" SOUND ATTENUATION BATT AT ALL RESTROOM WALLS



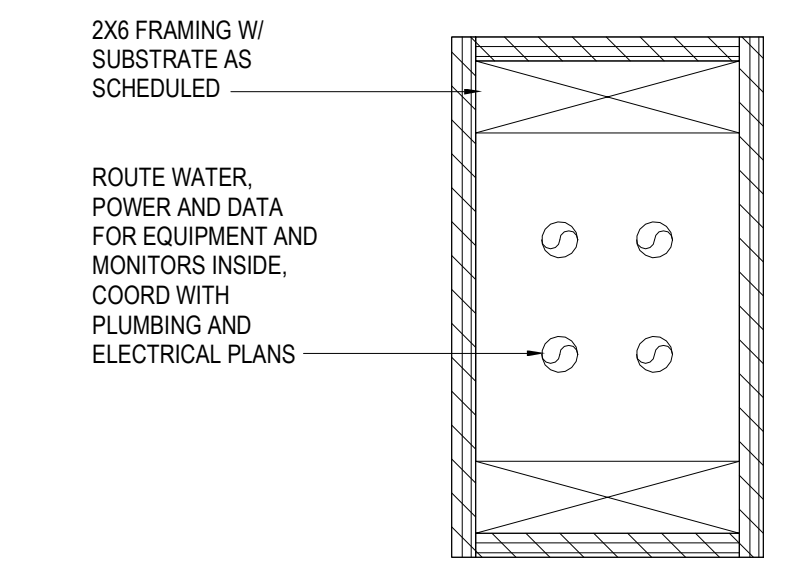
SCALE: 1/4" = 1'-0"

A3 RESTROOM ELEVATIONS

RESTROOM ACCESSORY SCHEDULE					
TAG	QTY.	TYPE	MODEL	MANUFACTURER	COMMENTS
B-400	-	SOAP DISPENSER (WALL MOUNT)	818615	BOBRICK	SURFACE MOUNTED
B-403	-	SLIMDRI HAND DRYER	L-974	WORLD DRYER	SURFACE MOUNTED
B-500	-	MIRROR 18"X36"	B-165-1832	BOBRICK	SURFACE MOUNTED
B-501	-	TOILET PAPER DISP.	B-4288	BOBRICK	SURFACE MOUNTED
B-502	-	GRAB BAR	B6806X36	BOBRICK	SURFACE MOUNTED
B-503	-	GRAB BAR	B6806X42	BOBRICK	SURFACE MOUNTED
B-504	-	GRAB BAR	B6806X18	BOBRICK	SURFACE MOUNTED
B-505	-	SANITARY NAPKIN RECEPTACLE	B-270	BOBRICK	INCLUDES ACCESSORY KIT AND MOUNTING HARDWARE
B-507	-	RR WASTE RECEPTACLE	B-2250	BOBRICK	-
B-508	-	COAT HOOK	B-212	BOBRICK	-

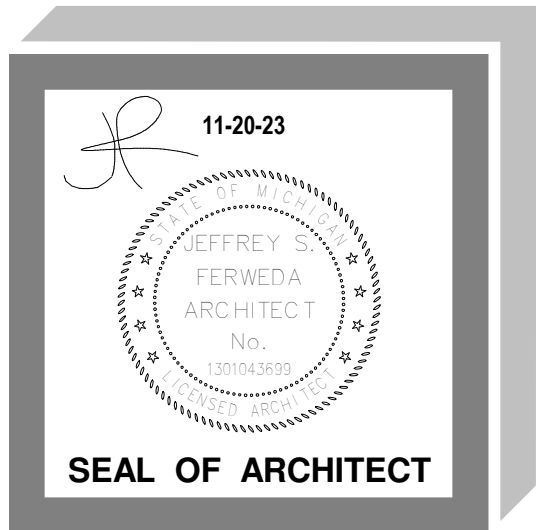


G1 ENLARGED RESTROOM ACCESSORY PLAN
SIMILAR FOR WOMEN'S ROOM
SCALE: 1/2" = 1'-0"



L1 H-LINE CHASE WALL DETAIL
SCALE: 3" = 1'-0"

GENERAL FLOOR PLAN LEGEND	
	PROPOSED WALL TO 6' ABOVE CEILING
	PROPOSED 1-HR RATED ASSEMBLY
	PROPOSED PARTIAL HT. WALL
	WALL TYPE TAG NOTE: ALL WALLS IN INDIVIDUAL ROOMS SAME TYPE U.N.O.
	SEMI-RECESSED FIRE EXTINGUISHER CABINET



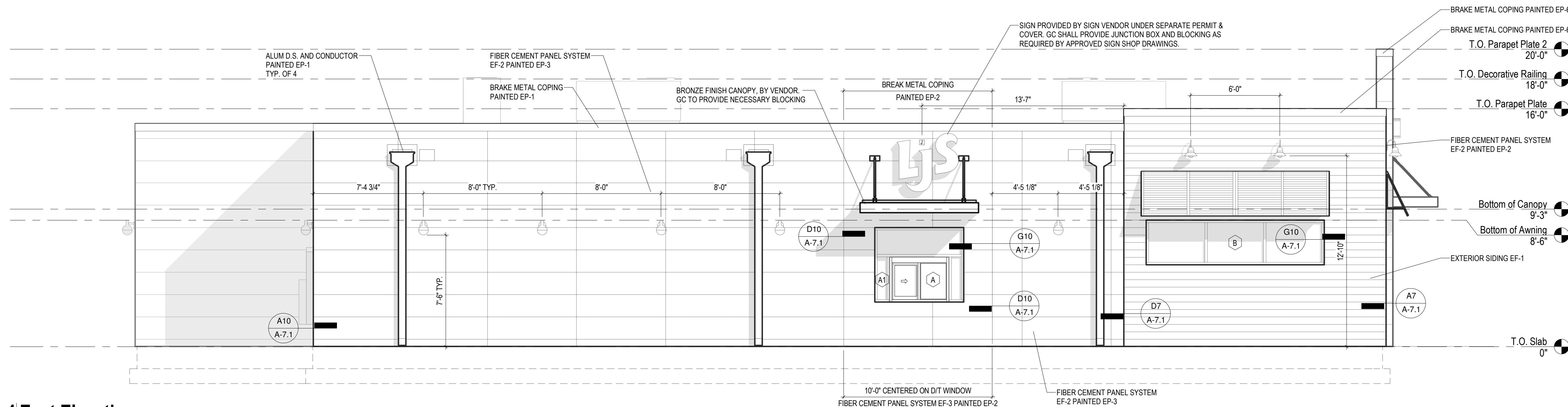
ARCHITECT
SEDGEWICK + FERWEDA ARCHITECTS
410 East Court Street Flint, MI 48503
810-238-9647 | www.sfarch.us
Good clients + Good design + Great architecture



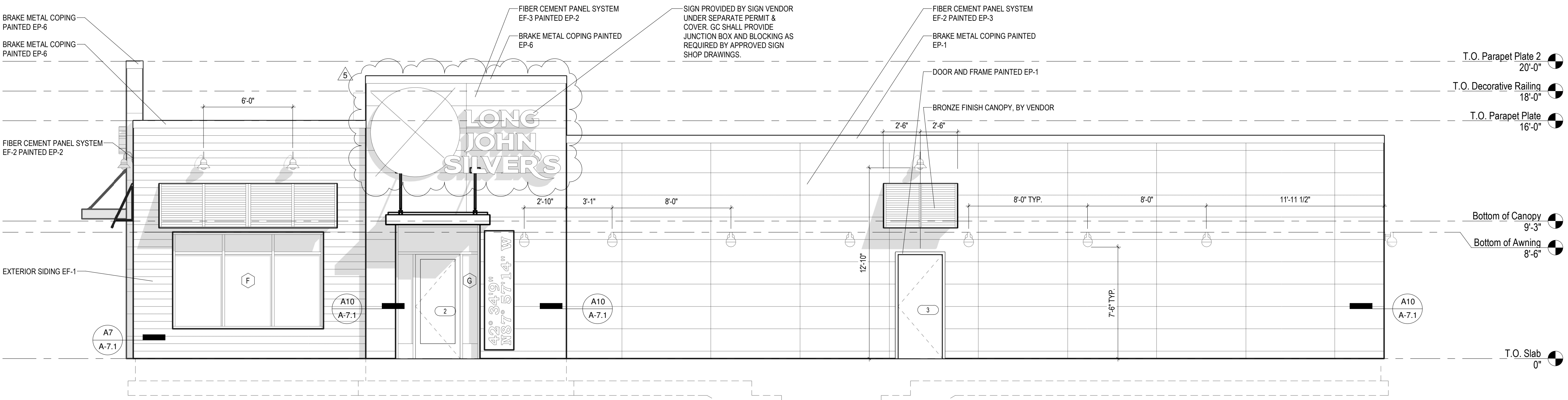
REVISIONS	
11/20/23	REVISION
10/24/23	SITE PLAN RESUBMISSION
9/5/23	SITE PLAN RESUBMISSION
8/8/23	SITE PLAN RESUBMISSION
Date	Description

Project
Long John Silver's
5481 W Main St, Kalamazoo, MI 49009
Drawing
Proposed Floor Plan

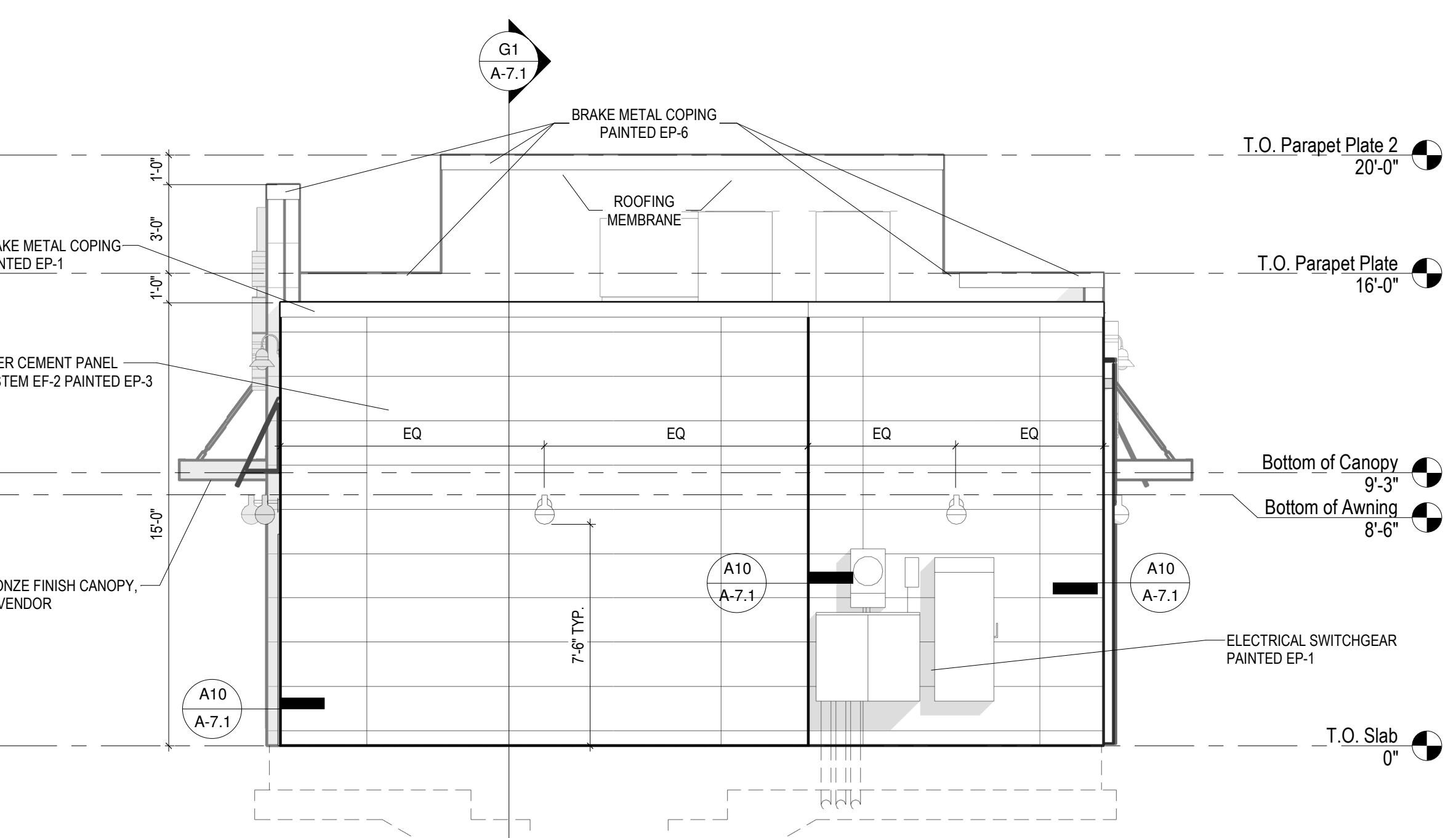
DRAWN BY:	LJK
CHECKED BY:	JSF
PROJECT NUMBER	23-017
DATE	07/17/2023
SCALE	As indicated
SHEET NUMBER	A-1.0



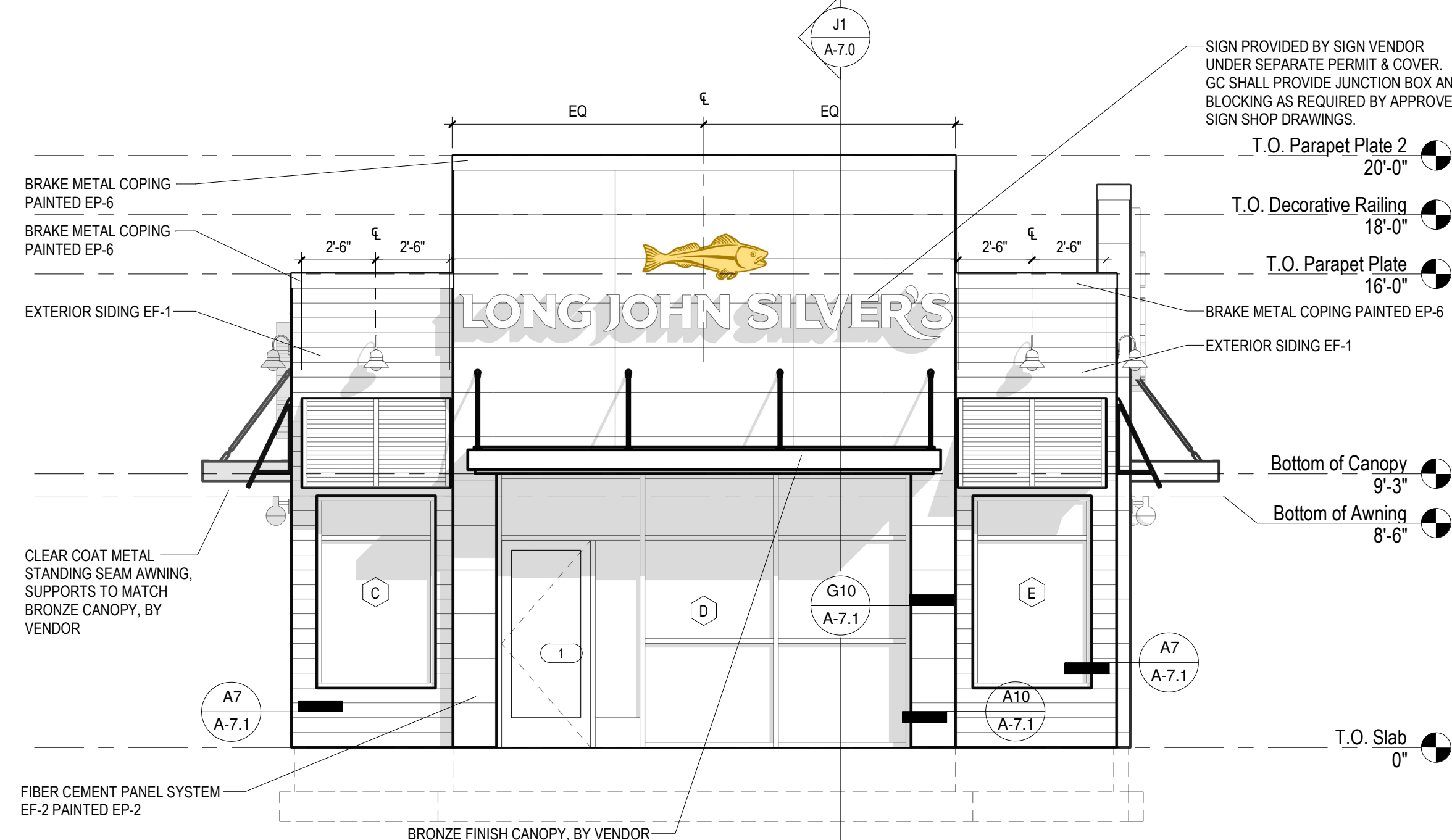
4 East Elevation
A:6.0 | 1/4" = 1'-0"



3 West Elevation
A:6.0 | 1/4" = 1'-0"

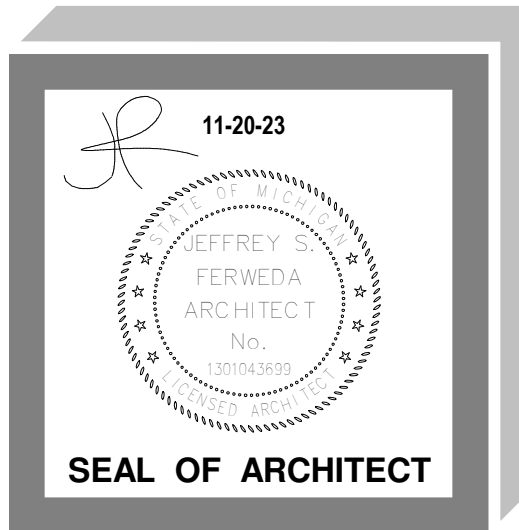


2 South Elevation
A:6.0 | 1/4" = 1'-0"



1 North Elevation
A:6.0 | 1/4" = 1'-0"

- GENERAL EXTERIOR ELEVATION NOTES:**
- CONSIDERATION WILL NOT BE GRANTED FOR ANY ALLEGED MISUNDERSTANDINGS OF THE AMOUNT OF WORK TO BE PERFORMED. TENDER OF PROPOSAL SHALL CONVEY FULL AGREEMENT TO THE ITEMS AND CONDITIONS INDICATED ON THE DRAWINGS. SHOULD THE CONTRACTOR FIND DISCREPANCIES OR OMISSIONS IN THE CONTRACT DOCUMENT OR BE IN DOUBT AS TO THE INTENT THEREOF, THE CONTRACTOR SHALL OBTAIN CLARIFICATION FROM THE ARCHITECT AND/OR THE OWNER'S REPRESENTATIVE PRIOR TO SUBMITTING HIS PROPOSAL FOR WORK.
 - ALL DIMENSIONS ARE SHOWN TO FACE OF FINISH UNLESS NOTES OTHERWISE.
 - WALLS TO STRUCTURAL DECK MUST BE THOROUGHLY SEALED AROUND PENETRATIONS.



ARCHITECT

SEDGEWICK + FERWEDA ARCHITECTS

410 East Court Street Flint, MI 48503
810-238-9647 | www.sfarch.us

Good clients + Good design + Great architecture



REVISIONS	
11/20/23	REVISION
10/24/23	SITE PLAN RESUBMISSION
9/5/23	SITE PLAN RESUBMISSION
8/8/23	SITE PLAN RESUBMISSION
Date	Description

Project **Long John Silver's**
5481 W Main St, Kalamazoo, MI 49009

Drawing **Exterior Elevations**

DRAWN BY: LJK
CHECKED BY: JSF

PROJECT NUMBER 23-017
DATE 07/17/2023
SCALE 1/4" = 1'-0"
SHEET NUMBER

A-6.0

11/20/2023 2:00:29 PM

LONG JOHN SILVER'S

OSHTEMO TOWNSHIP
 5481 W. MAIN ST KALAMAZOO, MI 49009
 STORE #: 70150



LEGAL DESCRIPTION:

THE NORTH 250 FEET OF LOT 3, ELKS PLAT NO. 1, ACCORDING TO THE PLAT THEREOF AS RECORDED IN LIBER 31 OF PLATS OF PAGE 48, KALAMAZOO COUNTRY RECORDS.

BEING THE SAME PROPERTY CONVEYED BY WARRANTY DEED FROM CONCORD CAPITAL MANAGEMENT, L.L.C., A MICHIGAN LIMITED LIABILITY COMPANY TO BR ASSOCIATES, INC., AN INDIANA CORPORATION DATED DECEMBER 29, 2003 AND RECORDED JANUARY 14, 2004 IN INSTRUMENT NO. 2004-002150.

EXISTING ZONING:

C - LOCAL BUSINESS

ADJACENT ZONING:

ZONING NORTH, EAST, WEST, AND SOUTH, C - LOCAL BUSINESS

PARKING REQUIREMENT:

1 PARKING SPOT PER 70 S.F. FLOOR AREA PLUS 1 PER EACH EMPLOYEE ON THE LARGEST SHIFT.

PARKING PROPOSED:

CALCULATIONS -

TOTAL SQ. FT. - HALLWAY/UTILITY SQ. FT. = DESIGN SQ. FT.

2,288 SQ. FT. - 245 SQ. FT. = 1,983 SQ. FT., LESS 10% = 1,785 SQ. FT.

1,785 SQ. FT. / 70 S.F. = 26 SPACES

5 WORKERS ON LARGEST SHIFTS = 5 ADDITIONAL SPACES

REQUIRED PARKING SPACES = 31 SPACES

14 SPACES

2 ADA SPACES

19 CROSS PARKING AGREEMENT SPACES

35 TOTAL SPACES

SHEET INDEX:

C-100 - COVER SHEET

C-300 - CIVIL SITE PLAN - EXISTING CONDITIONS

C-301 - CIVIL SITE PLAN - DEMOLITION

C-400 - CIVIL SITE PLAN - GRADING AND STORM SEWER

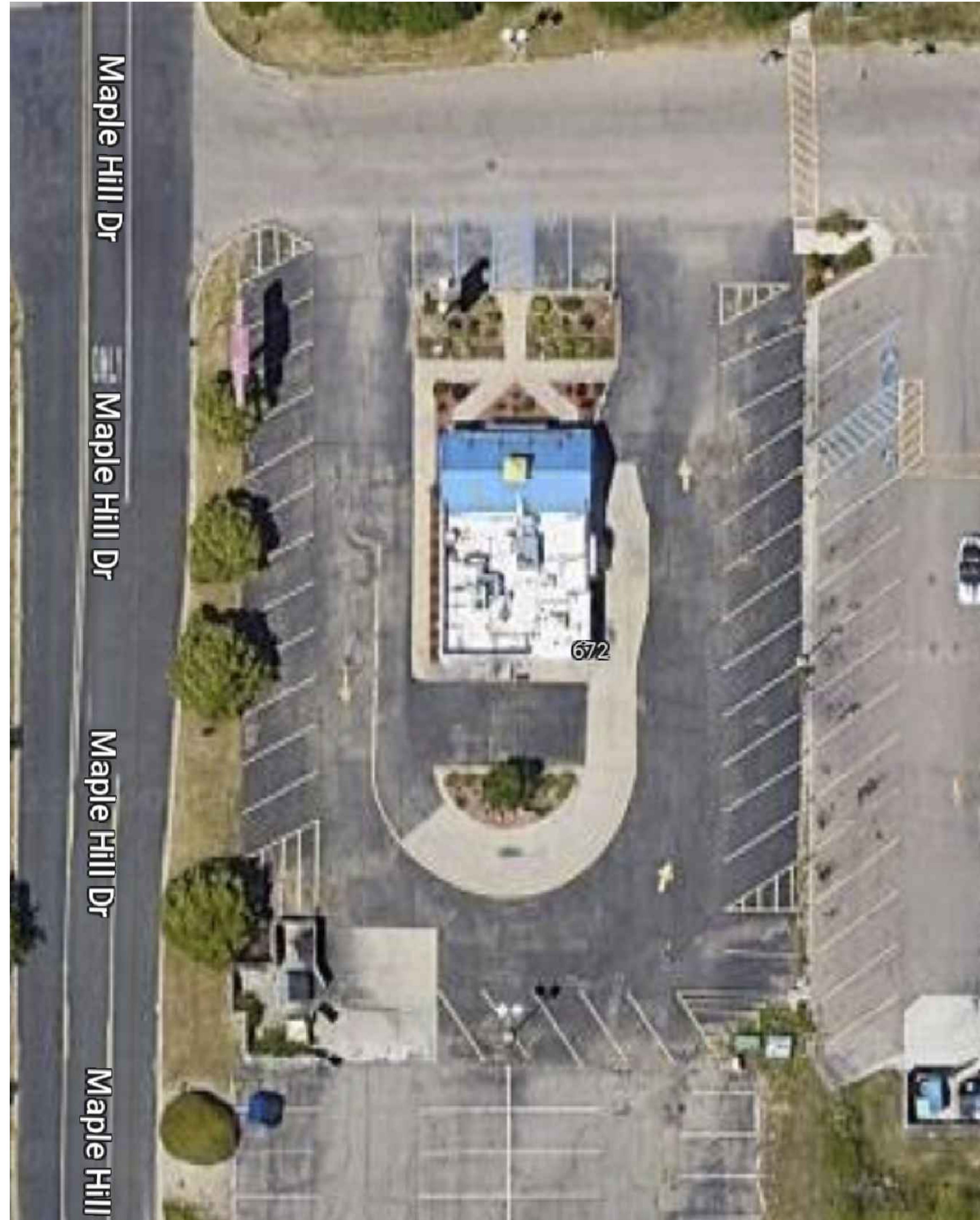
C-401 - CIVIL SITE PLAN - UTILITY PLAN

C-402 - GEOMETRIC DIMENSIONS AND STRIPING PLAN

C-403 - LANDSCAPE PLAN

C-800 - CIVIL DETAILS

C-801 - DUMPSTER DETAIL



 AERIAL SITE PHOTO

SCOTT BRADLEY
 CHARTER FOODS INC.
 TALBOTT, TN 37877
 (615)-310-6589

JEFFREY S. FERWEDA, AIA
 SEDGEWICK + FERWEDA ARCHITECTS
 FLINT, MI 48503
 (810)-397-3711

CRAIG TRIERWEILER, P.E.
 MATRIX CONSULTING ENGINEERS INC.
 LANSING MI, 48906
 (517)-487-2511

JOE SOVIS, P.E.
 MATRIX CONSULTING ENGINEERS INC.
 LANSING MI, 48906
 (517)-487-2511

GREG MINSHALL, P.E.
 MATRIX CONSULTING ENGINEERS INC.
 LANSING MI, 48906
 (517)-487-2511

10/23/2023	ISSUED FOR BIDDING
11/20/2023	REVISION

DRAWN	BTT
APPROVED	GLM

MATRIX
 CONSULTING ENGINEERS, INC.
 601 E. CECILIA CHAVEZ AVE.
 LANSING, MI 48906
 PHONE: (517) 487-2511
 FAX: (517) 487-2544
 administrator@matrixinc.com
 Main Project No. 23134.00

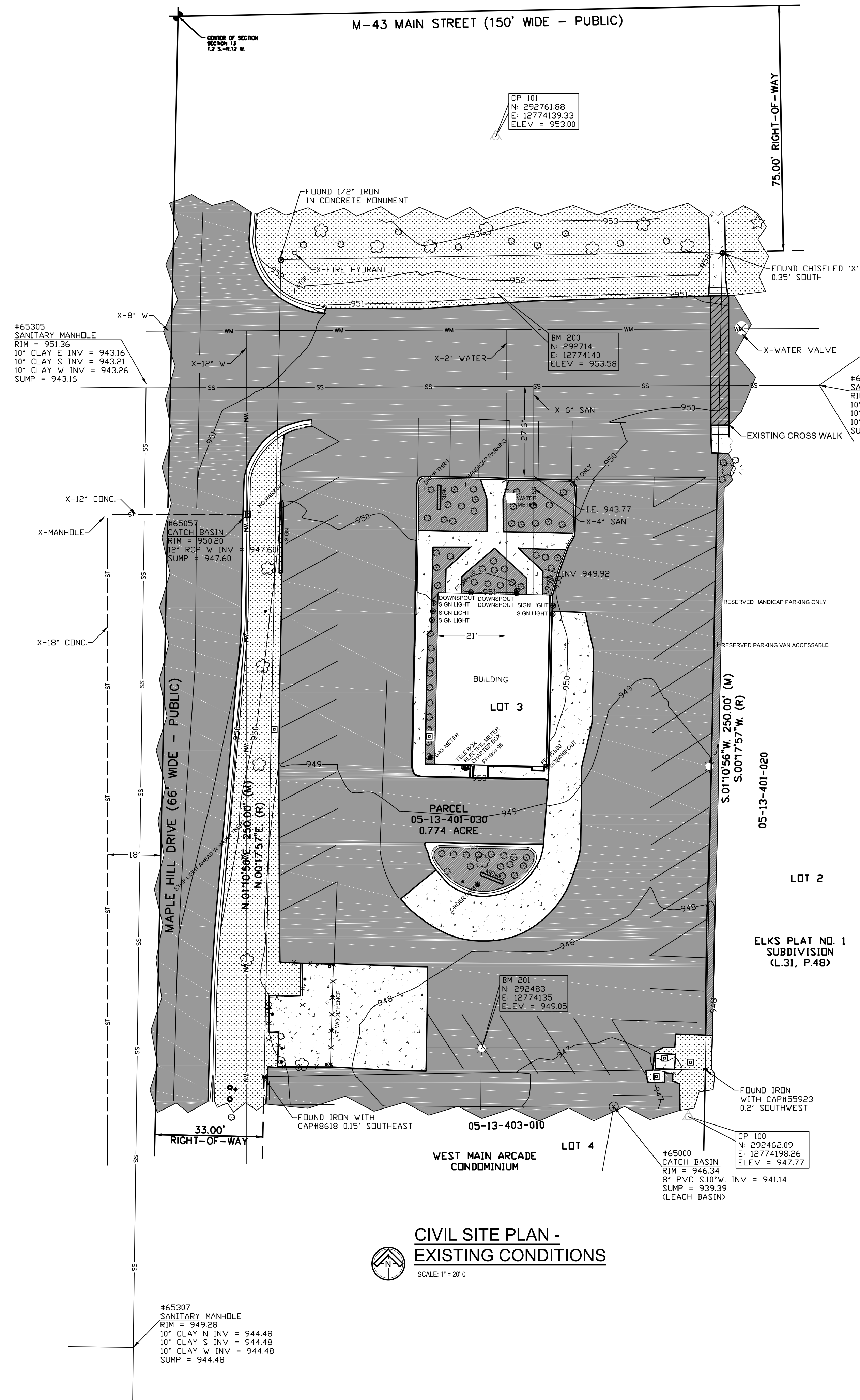
PROJECT
 LONG JOHN SILVER'S
 5481 W. MAIN ST.
 KALAMAZOO, MI
 49009

SHEET DESCRIPTION
COVER SHEET

DATE
07.21.23

PROJECT NUMBER
23134.00

SHEET NUMBER
C-100



CIVIL SITE PLAN - EXISTING CONDITIONS
SCALE: 1" = 20'-0"

LEGEND

■	CATCHBASIN	—	CONCRETE
+	UTILITY MARKER	—	ASPHALT
•	PEDESTAL	—	GRAVEL
○	TELEPHONE MANHOLE	—	GUTTER LINE
○	MONITORING WELL	—	BACK OF CURB
○	GAS VALVE	—	SIDEWALK
○	ROCK	—	PAINT MARKING
○	ELECTRIC HANDHOLE	—	MISC STRUCTURE
○	LIGHT POLE	—	LANDSCAPING
○	STORM MANHOLE	—	SIGN OUTLINE
○	ELECTRIC MANHOLE	—	BREAKLINE
○	POWER POLE	—	BUILDING
○	CAPPED IRON	—	BRUSHLINE
○	IRON PIN	—	FENCELINE
○	GUY ANCHOR	—	OVERHEAD ELECTRIC
○	MAILBOX	—	DETECTIBLE WARNING PAD
○	SANITARY MANHOLE	—	CENTERLINE
○	BUSH	—	WALL TOP
○	2 POST SIGN	—	UNDERGROUND TELEPHONE
○	SINGLE POST SIGN	—	UNDERGROUND GAS
○	TRANSFORMER	—	UNDERGROUND WATERMAIN
○	DECIDUOUS TREE	—	SANITARY SEWER LINE
○	STUMP	—	STORM SEWER LINE
○	CONIFEROUS TREE		
○	HYDRANT		
○	WATER VALVE		
○	SPRINKLER HEAD		
○	WATER MANHOLE		
○	BENCHMARK		
○	BOLLARD		

LEGAL DESCRIPTION
(Per Government Deed, Recorded as Document #2018-035603, Kalamazoo County Records)

Lot 3, Elks Plat No. 1, according to the Plat thereof as recorded in Liber 31 of Plats on Page 48, Kalamazoo County Records.

SURVEYOR NOTES:

- COORDINATES AND BEARINGS ARE BASED ON MICHIGAN STATE PLANE COORDINATE SYSTEM, SOUTH ZONE (213), INTERNATIONAL FEET. ELEVATIONS ARE BASED ON NAVD83, DERIVED FROM RTK GPS OBSERVATION.
- TOPOGRAPHIC DATA WAS COLLECTED BY SURVEYING SOLUTIONS INC., IN MAY OF 2023.
- ABOVE GROUND UTILITIES ARE SHOWN PER FIELD OBSERVATION. NO MISS DIG DESIGN TICKET REQUEST WAS SUBMITTED FOR THE PROJECT. NO PIN FLAGS WERE LOCATED IN THE FIELD AND UTILITIES WHICH WERE NOT VISUALLY CONFIRMED HAVE NOT BEEN SHOWN HEREON.
- PROPERTY LINES SHOWN HEREON WERE RETRACED BASED UPON RECORDED DEEDS, SUBDIVISION PLAT, AND WAYNE COUNTY TAX PARCEL INFORMATION. NO TITLE REPORT FOR THE SUBJECT PARCEL HAS BEEN PROVIDED. EASEMENTS AND OTHER ENCUMBRANCES WHICH MAY BURDEN THE SUBJECT PARCEL HAVE NOT BEEN SHOWN HEREON.

CONTROL POINTS:

- CP100 - SET 1/2"x18" IRON WITH SSI TRAVERSE CAP +/-10' SOUTH OF THE SOUTHEAST PROPERTY CORNER OF SUBJECT PARCEL.
- CP101 - SET 1/2"x18" IRON WITH SSI TRAVERSE CAP ON THE SOUTH SIDE OF MAIN STREET +/-100' EAST OF MAPLE HILL DRIVE.

BENCHMARKS:

- BM200 - SET PK NAIL ON THE SOUTH SIDE OF A CONCRETE FOUNDATION FOR LIGHT POLE ON THE NORTH SIDE OF PARKING LOT.
- BM201 - SET PK NAIL ON THE NORTH SIDE OF A CONCRETE FOUNDATION FOR LIGHT POLE ON THE SOUTH SIDE OF PARKING LOT.

REVISIONS

10/23/2023	ISSUED FOR BIDDING
11/20/2023	REVISION

DRAWN: **BTT**
APPROVED: **GLM**

1601 E. CESAR CHAVEZ AVE
LANSING, MI 48906
PHONE (617) 487-2511
FAX (617) 487-2544
admin@matrix-engineers.com
Matrix Project No. 23134.00

MATRIX
CONSULTING ENGINEERS, INC.

PROJECT

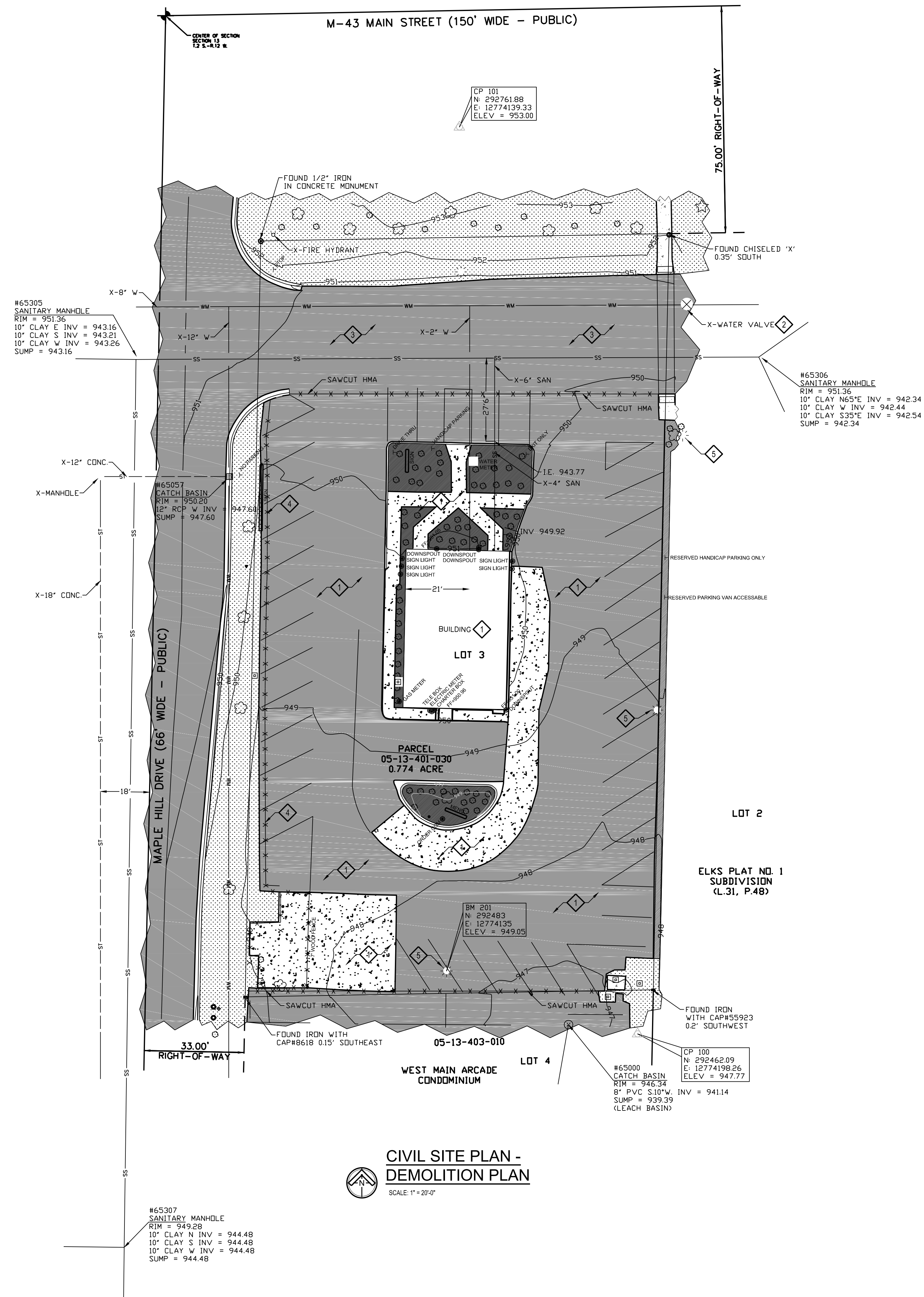
LONG JOHN SILVER'S
5481 W. MAIN ST.
KALAMAZOO, MI 49009

SHEET DESCRIPTION
CIVIL SITE PLAN - EXISTING CONDITIONS

DATE
07.21.23

PROJECT NUMBER
23134.00

SHEET NUMBER
C-300



**CIVIL SITE PLAN -
DEMOLITION PLAN**
SCALE: 1" = 20'-0"

DEMOLITION NOTES

- 1 REMOVE EXISTING HOT MIX ASPHALT, CONCRETE, LANDSCAPING, AND STRUCTURES. BACKFILL WITH MDOT CII SAND COMPACTED TO 95% MAXIMUM DENSITY.
- 2 EXISTING WATER PIPING AND WATER VALVE TO REMAIN.
- 3 PROTECT EXISTING HOT MIX ASPHALT IN LOCATION SHOWN.
- 4 INSTALL TREE PROTECTION FENCING OUTSIDE DRIP EDGE OF TREES.
- 5 REMOVE EXISTING LIGHT POLES TO LIMITS SHOWN. DISCONNECT AND TEMPORARILY CAP.

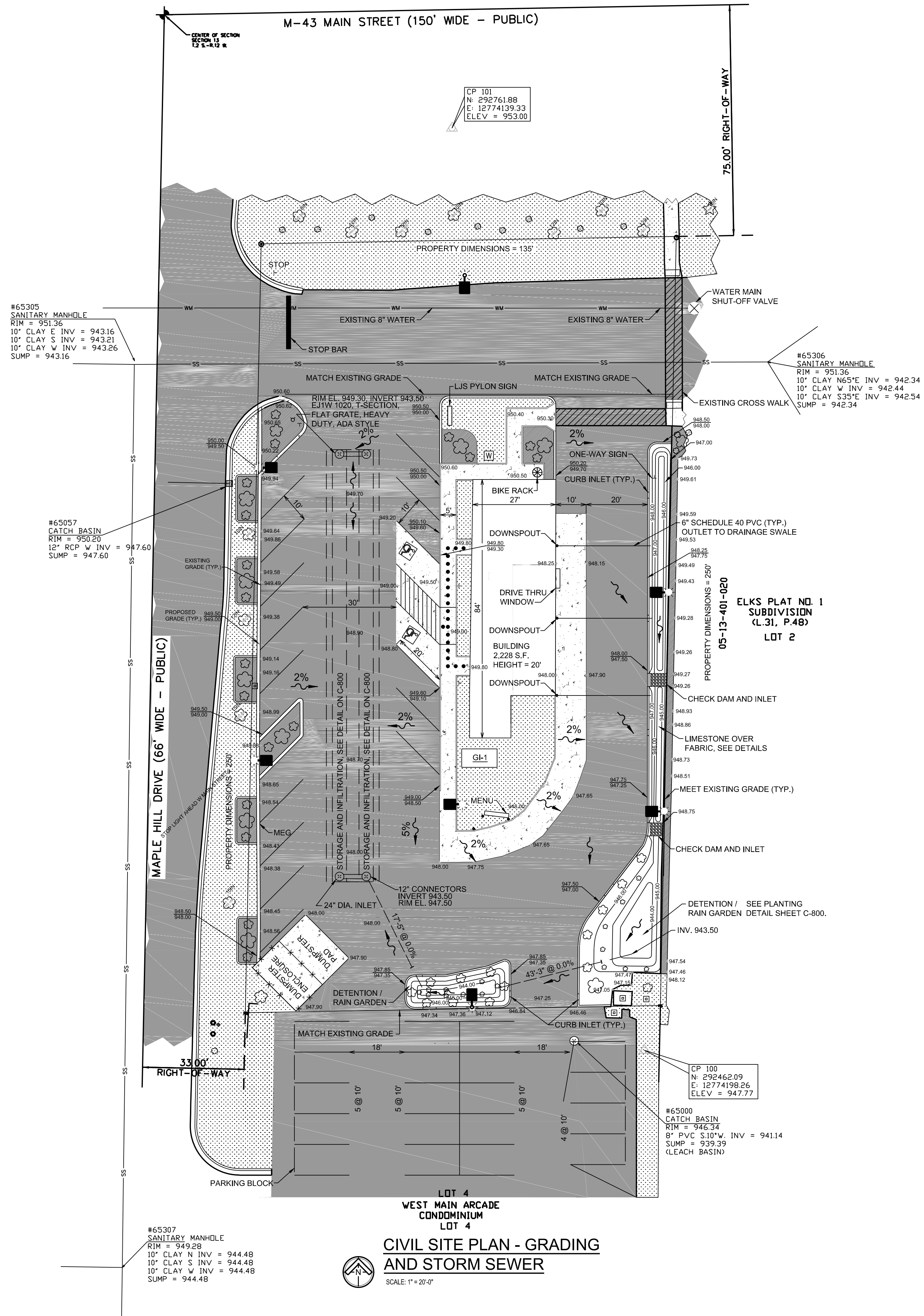
REVISIONS	
10/23/2023	ISSUED FOR BIDDING
11/20/2023	REVISION
DRAWN BTT	
APPROVED GLM	

1601 E. CEASAR CHAVEZ AVE
LANSING, MI 48906
PHONE (617) 467-2511
FAX (617) 467-2544
admin@matrixeng.com
Matrix Project No. 23134.00

MATRIX
CONSULTING ENGINEERS, INC.

PROJECT
LONG JOHN SILVER'S
5481 W. MAIN ST.
KALAMAZOO, MI
49009

SHEET DESCRIPTION	
CIVIL SITE PLAN - DEMOLITION PLAN	
DATE	
07.21.23	
PROJECT NUMBER	
23134.00	
SHEET NUMBER	
C-301	



GENERAL NOTES

- 94X.XX TOP OF CURB OR THICKENED EDGE SIDEWALK
94X.XX BOTTOM OF CURB OR THICKENED EDGE SIDEWALK
- SIDEWALK CROSS SLOPES TO BE 1% TO 2% MAX.
- CONTRACTOR SHALL INSTALL MAN HOLE COVER AND UNDERGROUND STORAGE PER MANUFACTURERS WRITTEN INSTRUCTIONS.

LONG JOHN SILVERS OSHTEMO TOWNSHIP;
PROPERTY SIZE - 250 FT x 135 FT = 33,750 S.F.
CONTRIBUTING AREA = 33,750 S.F. = 0.88 ACRE
ALLOWABLE RELEASE RATE = N/A

DETAIL/INFILTRATE 25 YEAR STORM:
NOAA ATLAS 14 VOLUME 8, 25-YEAR, 60 MINUTE RAINFALL = 2.31 IN.

REQUIRED STORAGE
(2.31 IN. / 12") * 33,750 S.F. * 0.88 = 5,717 C.F. - 312 C.F. (1 HR INFILTRATION) = 5,405 C.F.
TIME TO EMPTY (INFILTRATE)
130 FT * 8 FT = 1,040 S.F.

INFILTRATION RATE (FOR DOUBLE 30-INCH TRENCH NOT INCLUDING RAIN GARDEN)
4.5 IN/HR * 0.8 FACTOR OF SAFETY = 3.6 IN/HR
3.6/12 * 1,040 = 312 C.F./HR
5,717 / 12 = 18.3 HRS

HIGH WATER ELEVATION = 946.52

STORAGE/INFILTRATION SURFACE:
DRAINAGE SWALE = 3,037 C.F.
DETENTION / RAIN GARDENS = 1,261 C.F.
PIPES (SUBSURFACE) = 1,375 C.F.
TOTAL = 5,673 C.F.

LEGEND

■	CATCHBASIN	—	CONCRETE
⊥	UTILITY MARKER	—	ASPHALT
⊕	PEDESTAL	—	GRAVEL
⊙	TELEPHONE MANHOLE	—	GUTTER LINE
⊖	MONITORING WELL	—	BACK OF CURB
⊗	GAS VALVE	—	SIDEWALK
⊘	ROCK	—	PAINT MARKING
⊙	ELECTRIC HANDHOLE	—	MISC. STRUCTURE
⊕	LIGHT POLE	—	LANDSCAPING
⊖	STORM MANHOLE	—	SIGN OUTLINE
⊙	ELECTRIC MANHOLE	—	BREAKLINE
⊖	POWDER POLE	—	BUILDING
⊙	CAPPED IRON	—	BRUSHLINE
⊕	IRON PIN	—	FENCELINE
⊕	GUY ANCHOR	—	OVERHEAD ELECTRIC
⊕	MAILBOX	—	DETECTIBLE WARNING PAD
⊕	SANITARY MANHOLE	—	CENTERLINE
⊕	JUNIPERUS SQUATAMA	—	WALL TOP
⊕	2 POST SIGN	—	UNDERGROUND TELEPHONE
⊕	SINGLE POST SIGN	—	UNDERGROUND GAS
⊕	TRANSFORMER	—	UNDERGROUND WATERMAIN
⊕	RED MAPLE	—	SANITARY SEWER LINE
⊕	STUMP	—	STORM SEWER LINE
⊕	CONIFEROUS TREE	—	GRASS, TOPSOIL
⊕	HYDRANT	—	ASPHALT
⊕	WATER VALVE	—	GRAVEL
⊕	SPRINKLER HEAD	—	CONCRETE
⊕	WATER MANHOLE	—	BARK/MULCH
⊕	BENCHMARK		
⊕	BELLARD		

10/23/2023	ISSUED FOR BIDDING
11/20/2023	REVISION

BTT	DRAWN
GLM	APPROVED

MATRIX
CONSULTING ENGINEERS, INC.
1601 E. CASAR CHAVEZ AVE
LANSING, MI 48906
PHONE (313) 487-2511
FAX (313) 487-2544
admin@matrixinc.com
Main Project No. 23134.00

LONG JOHN SILVER'S
5481 W. MAIN ST.
KALAMAZOO, MI
49009

CIVIL SITE PLAN - GRADING AND STORM SEWER

DATE	07.21.23
------	----------

PROJECT NUMBER	23134.00
----------------	----------

SHEET NUMBER	C-400
--------------	-------

GENERAL NOTES

- INSTALL GREASE INTERCEPTOR BELOW GRADE AT LOCATION SHOWN. GREASE INTERCEPTOR WAS SIZED BASED OFF NUMBERS OF MEALS SERVED PER DAY WHICH WAS DETERMINED TO BE 600 MEALS. INSTALL A SCHIER GREASE INTERCEPTOR (GB-1500, 200 GPM)
- INSTALL NEW FIRE HYDRANT AT LOCATION SHOWN. FIRE HYDRANT SHALL BE COORDINATED WITH THE LOCAL FIRE MARSHALL PRIOR TO INSTALLING. HYDRANT AND VALVES SHALL MEET OSHTEMO TOWNSHIP STANDARDS.
- INSTALL NEW 6" SANITARY SEWER CONNECTION AT LOCATION SHOWN. ALL NEW SANITARY SEWER PIPING IS TO BE SCHEDULE 80 PVC. NEW SANITARY SEWER CONNECTION SHALL BE COORDINATED WITH THE CITY PRIOR TO PERFORMING THE WORK. SEE CLEANOUT DETAIL ON C-800 FOR MORE INFORMATION.

REVISIONS
10/23/2023 ISSUED FOR BIDDING
11/20/2023 REVISION

DRAWN
BTT

APPROVED
GLM

601 E. CASAR CHAVEZ AVE
LANSING, MI 48206
PHONE (313) 877-2511
FAX (313) 877-2544
administrator@matrixinc.com
Main Project No. 23134.00

MATRIX
CONSULTING ENGINEERS, INC.

PROJECT

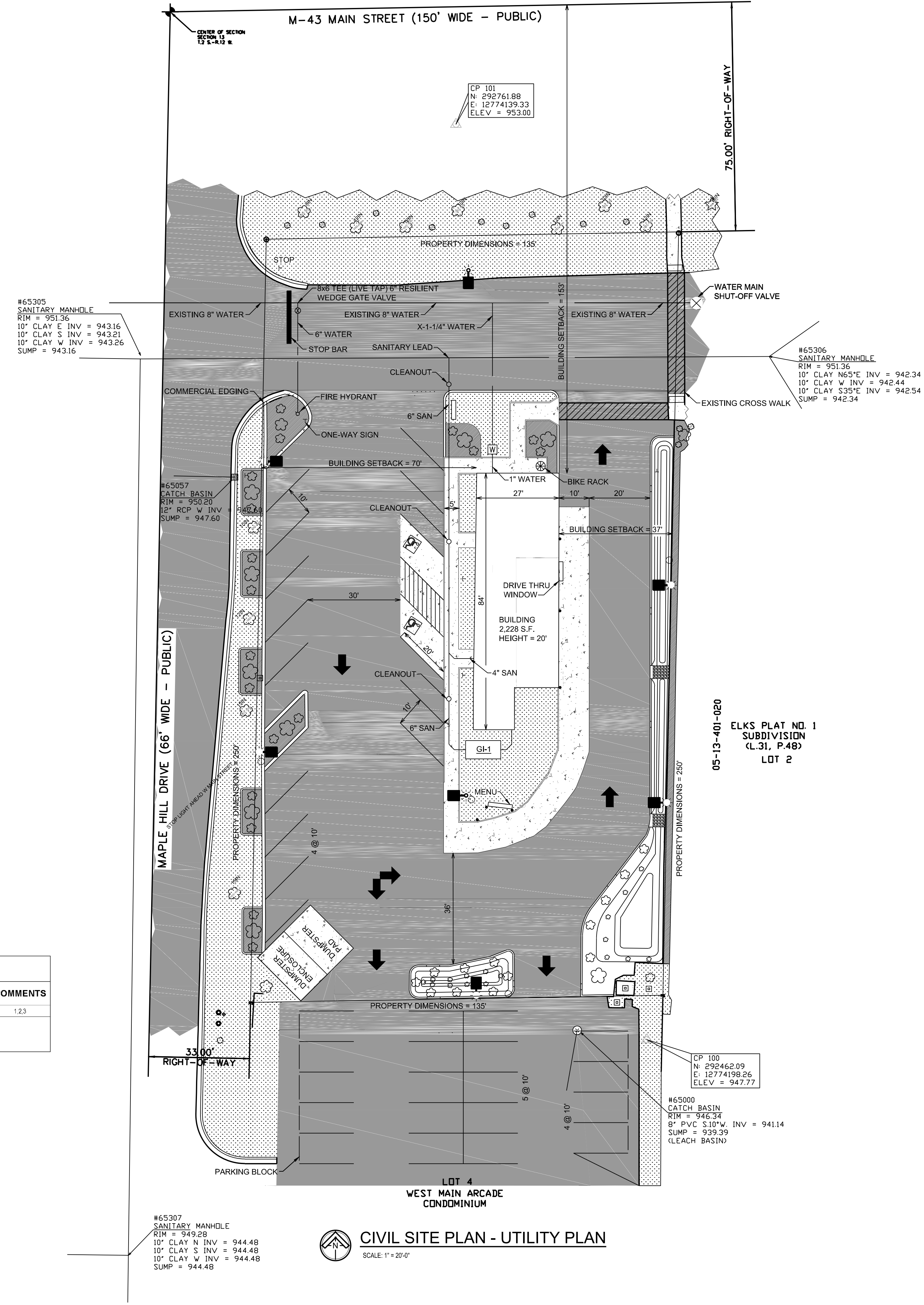
LONG JOHN SILVER'S
5481 W. MAIN ST.
KALAMAZOO, MI
49009

SHEET DESCRIPTION
CIVIL SITE PLAN - UTILITY PLAN

DATE
07.21.23

PROJECT NUMBER
23134.00

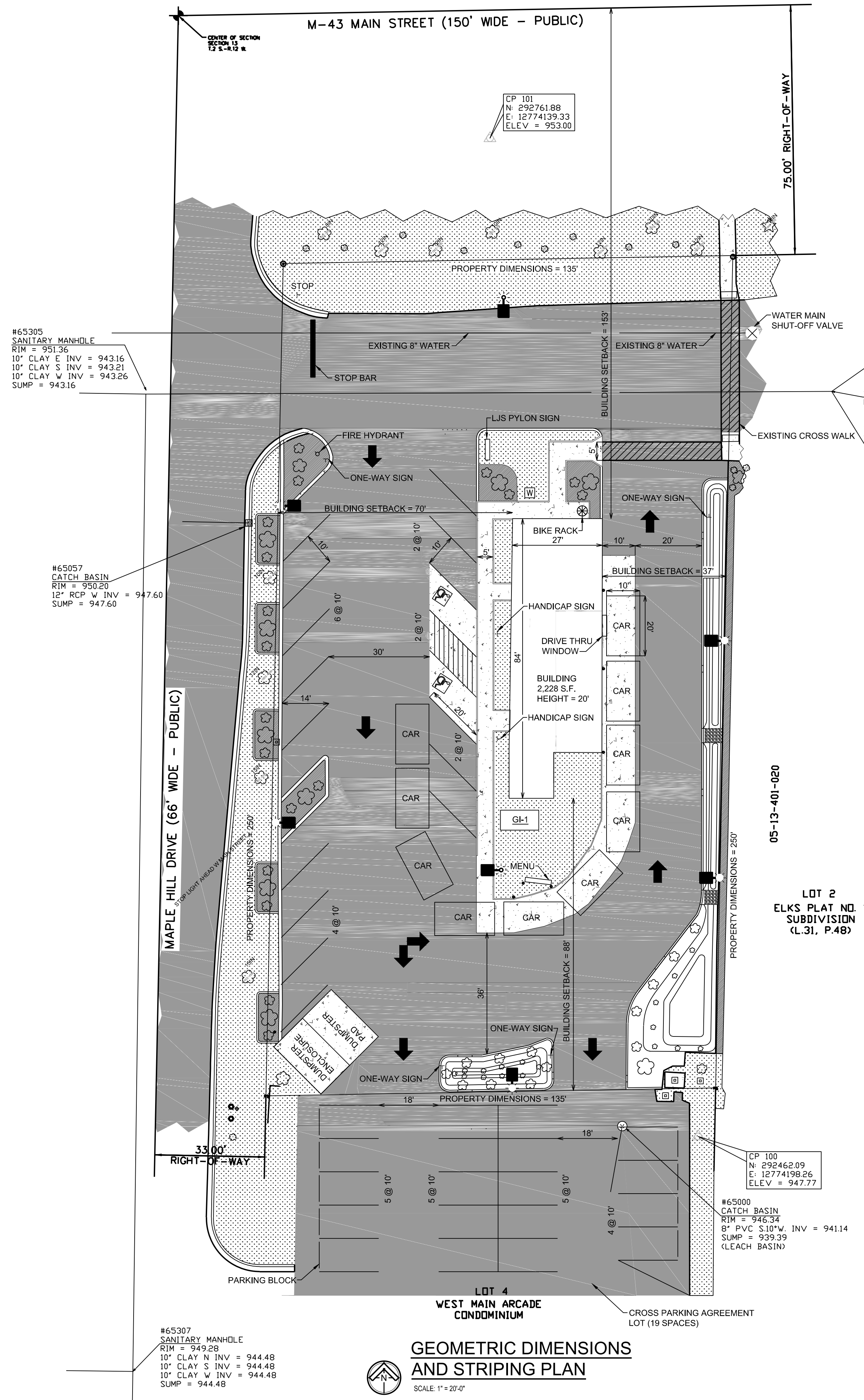
SHEET NUMBER
C-401



GREASE INTERCEPTOR SCHEDULE									
TAG	MODEL	LOCATION	GREASE HOLDING CAPACITY (LBS)	GPM	INLET/OUTLET	DIMENSIONS			COMMENTS
						WIDTH	DEPTH	HEIGHT	
GI-1	GB-1500	SOUTH OF BUILDING	1500	200	4"	137"	73"	72"	1,2,3

NOTES:
1. BASED ON SCHIER
2. SOLID HEAVY DUTY DIAMOND PLATE COVER
3. SIZED BASED ON FACILITY TYPE AND NUMBER OF SEATS

CIVIL SITE PLAN - UTILITY PLAN
SCALE: 1" = 20'-0"



GENERAL NOTES

1. PARKING SPOTS = 14 STANDARD, 2 ACCESSIBLE, 19 STANDARD FROM CROSS PARKING AGREEMENT. TOTAL = 35 PARKING SPOTS.
2. PROVIDE (2) MICHIGAN ACCESSIBLE PARKING SIGNS MICHIGAN MUTCD R7-8 WITH VAN ACCESSIBLE SIGNS R7-8A. NOTE NEW "ACTIVE" WHEEL CHAIR SYMBOL. GROUT SIGN POSTS IN STEEL SLEEVES.
3. CONNECT NEW MANHOLES TO EXISTING PIPE WITH 15-INCH SDR 26 PVC AND RESILIENT CONNECTOR AND STAINLESS STEEL STRAPS.
4. CONCRETE MDOT P1.
5. MINIMUM BUILDING SETBACK OFF ADJACENT PROPERTY LINES IS 20' UNLESS INDICATED OTHERWISE ON THE DRAWINGS.

LEGEND

■	CATCHBASIN	—	CONCRETE
+	UTILITY MARKER	—	ASPHALT
⊕	PEDESTAL	—	GRAVEL
⊙	TELEPHONE MANHOLE	—	GUTTER LINE
⊗	MONITORING WELL	—	BACK OF CURB
⊖	GAS VALVE	—	SIDEWALK
⊘	ROCK	—	PAINT MARKING
⊙	ELECTRIC HANDHOLE	—	MISC. STRUCTURE
⊙	LIGHT POLE	—	LANDSCAPING
⊙	STORM MANHOLE	—	SIGN OUTLINE
⊙	ELECTRIC MANHOLE	—	BREAKLINE
⊙	POWER POLE	—	BUILDING
⊙	CAPPED IRDN	—	BRUSHLINE
⊙	IRON PIN	—	FENCELINE
⊙	GUY ANCHOR	—	DIVERHEAD ELECTRIC
⊙	MAILBOX	—	DETECTIBLE WARNING PAD
⊙	SANITARY MANHOLE	—	CENTERLINE
⊙	JUNIPERUS SQUATAMA	—	UNDERGROUND TELEPHONE
⊙	2 POST SIGN	—	UNDERGROUND GAS
⊙	SINGLE POST SIGN	—	UNDERGROUND WATERMAIN
⊙	TRANSFORMER	—	SANITARY SEWER LINE
⊙	RED MAPLE	—	STORM SEWER LINE
⊙	STUMP	—	GRASS, TOPSOIL
⊙	CONIFEROUS TREE	—	ASPHALT
⊙	HYDRANT	—	GRAVEL
⊙	WATER VALVE	—	CONCRETE
⊙	SPRINKLER HEAD	—	BARK/MULCH
⊙	WATER MANHOLE		
⊙	BENCHMARK		
⊙	BOLLARD		

10/29/2023	ISSUED FOR BIDDING
11/20/2023	REVISION

DRAWN
BTT

APPROVED
GLM

MATRIX
CONSULTING ENGINEERS, INC.
601 E. CASAR CHAVEZ AVE.
LANSING, MI 48206
PHONE: (313) 487-2511
FAX: (313) 487-2544
administrator@matrixce.com
Matrix Project No. 23134.00

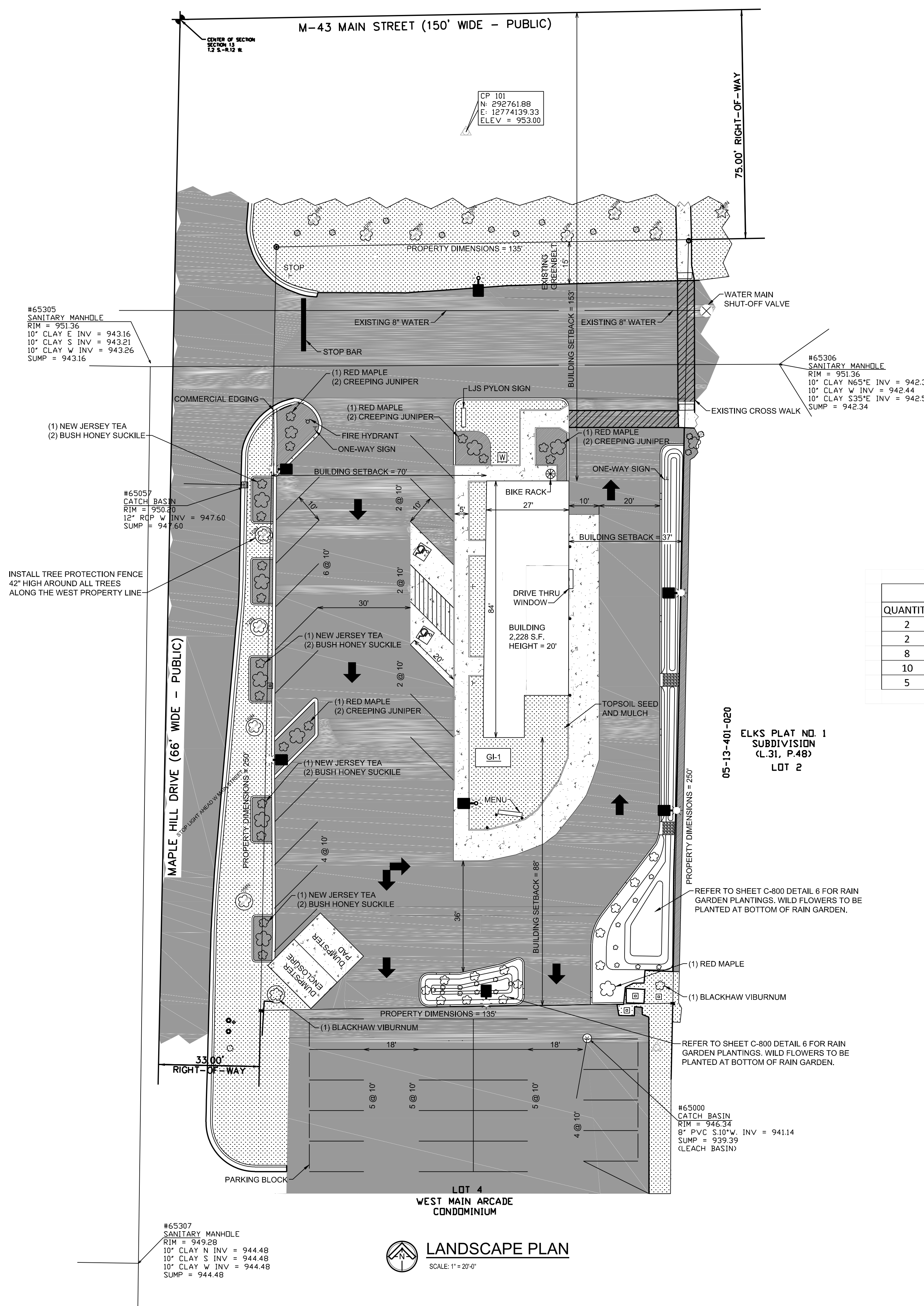
PROJECT
LONG JOHN SILVER'S
5481 W. MAIN ST.
KALAMAZOO, MI
49009

SHEET DESCRIPTION
GEOMETRIC DIMENSIONS AND STRIPING PLAN

DATE
07.21.23

PROJECT NUMBER
23134.00

SHEET NUMBER
C-402



REQUIRED GREEN SPACE = 26 SPACES * 25 S.F./SPACE = 650 S.F. ADDITIONAL GREEN SPACE

FOUR ISLANDS GREEN SPACE = 254 S.F. + 227 S.F. + 144 S.F. + 230 S.F. = 855 S.F. ADDITIONAL GREEN SPACE.

ORDINANCE 53.5 PARKING LOT
 • 25 S.F. / SPACE
 • 16 SPACES * 25 S.F. = 400
 • (2) ISLANDS 400 S.F. TOTAL

PARKING LOT SHRUBS
 1.5 PER SPACE
 10 SPACES ADJACENT MAPLE HILL
 15 SHRUBS REQUIRED

ORDINANCE 53.6 STREET RIGHT-OF-WAY GREENBELT
 • NORTH SIDE IS 15 FEET AND WILL BE PROTECTED
 WEST SIDE WEST OF THE RIGHT-OF-WAY IS 10 TO 15 FEET AND WILL BE PROTECTED.
 • PER 53.150 C WE ARE REQUESTING THAT THE GREENBELT REQUIREMENT BE REDUCED AS IT IS CONSTRAINED BY THE SITE.

ORDINANCE 53.70 INTERIOR SITE LANDSCAPING
 • 3,375 S.F. REQUIRED 3,100 PROVIDED
 • 2,050 ADJACENT BUILDING, 1,050 RAIN GARDENS
 • (3) CANOPY TREES REQUIRED - (3) PROVIDED
 • (2) UNDERSTORM TREES REQUIRED - (2) PROVIDED

LEGEND

■	CATCHBASIN	—	CONCRETE
⊕	UTILITY MARKER	—	ASPHALT
⊙	PEDESTAL	—	GRAVEL
⊗	TELEPHONE MANHOLE	—	GUTTER LINE
⊖	MONITORING WELL	—	BACK OF CURB
⊕	GAS VALVE	—	SIDEWALK
⊕	ROCK	—	PAINT MARKING
⊕	ELECTRIC HANDHOLE	—	MISC. STRUCTURE
⊕	LIGHT POLE	—	LANDSCAPING
⊕	STORM MANHOLE	—	SIGN OUTLINE
⊕	ELECTRIC MANHOLE	—	BREAKLINE
⊕	POWDR POLE	—	BUILDING
⊕	CAPPED IRON	—	BRUSHLINE
⊕	IRON PIN	—	FENCELINE
⊕	GUY ANCHOR	—	OVERHEAD ELECTRIC
⊕	MAILBOX	—	DETECTABLE WARNING PAD
⊕	SANITARY MANHOLE	—	CENTERLINE
⊕	JUNIPERUS SQUATAMA	—	WALL TOP
⊕	2 POST SIGN	—	UNDERGROUND TELEPHONE
⊕	SINGLE POST SIGN	—	UNDERGROUND GAS
⊕	TRANSFORMER	—	UNDERGROUND WATERMAIN
⊕	RED MAPLE	—	SANITARY SEWER LINE
⊕	STUMP	—	STORM SEWER LINE
⊕	CONIFEROUS TREE	—	GRASS, TOPSOIL
⊕	HYDRANT	—	ASPHALT
⊕	WATER VALVE	—	GRAVEL
⊕	SPRINKLER HEAD	—	CONCRETE
⊕	WATER MANHOLE	—	BARK/MULCH
⊕	BENCHMARK		
⊕	BELLARD		

LANDSCAPE SCHEDULE

QUANTITY	SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	NOTES
2	AF	ACER FREEMANII 'AUTUMN BLAZE'	AUTUMN BLAZE RED MAPLE	2" CALIPER	BALLED AND BURLAP, BARK/MULCH
2	BV	VIBURNUM PRUNIFOLIUM	BLACKHAW VIBURNUM	3 GALLON	BALLED AND BURLAP, BARK/MULCH
8	JH	JUNIPERUS HORIZONTALIS 'WILTONII'	CREeping JUNIPER	1 GALLON	BARK/MULCH
10	BH	DIERVILLA IONICERA	BUSHY HONEY SUCKLE	3 GALLON	BARK/MULCH
5	NJ	CEANOTHUS AMERICANUS	NEW JERSEY TEA	3 GALLON	BARK/MULCH

LANDSCAPE PLAN
 SCALE: 1" = 20'-0"

REVISIONS

10/23/2023	ISSUED FOR BIDDING
11/20/2023	REVISION

DRAWN
BTT

APPROVED
GLM

MATRIX
 CONSULTING ENGINEERS, INC.
 401 E. CASAR CHAVEZ AVE
 LANSING, MI 48206
 PHONE (517) 887-2511
 FAX (517) 887-2544
 administrator@matrixeng.com
 Matrix Project No. 23134.00

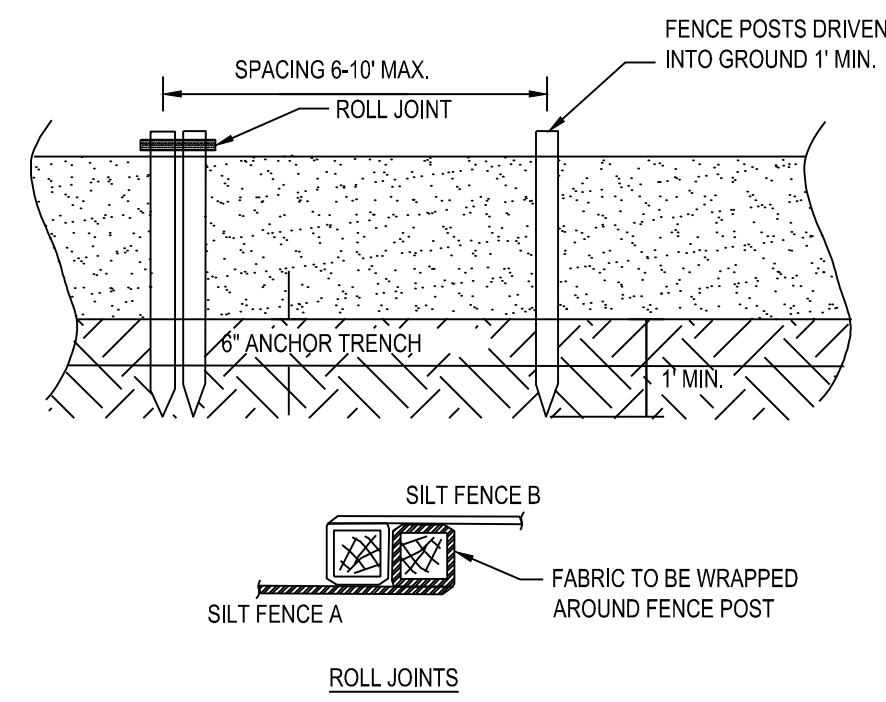
PROJECT
LONG JOHN SILVER'S
 5481 W. MAIN ST.
 KALAMAZOO, MI
 49009

SHEET DESCRIPTION
LANDSCAPE PLAN

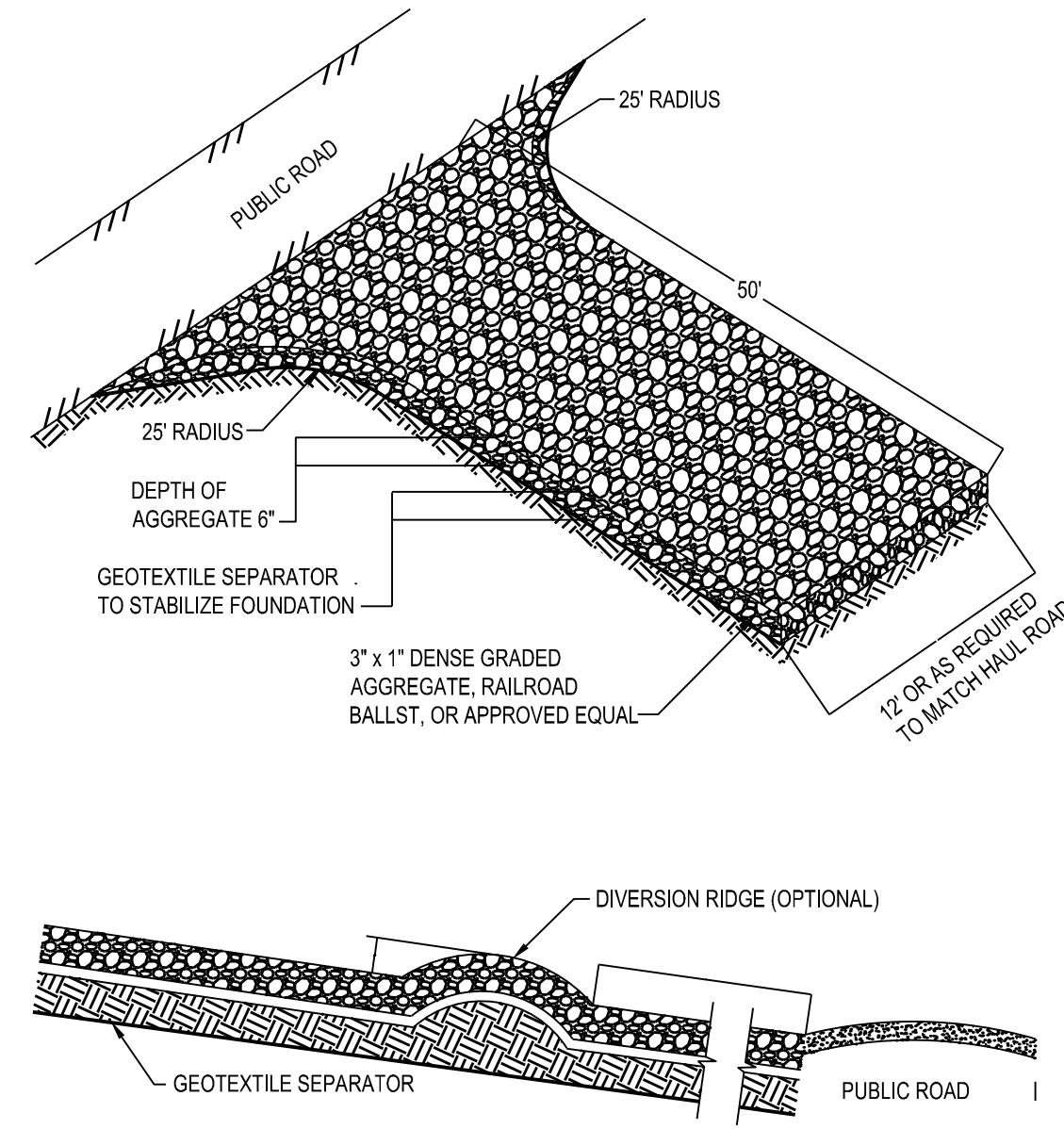
DATE
07.21.23

PROJECT NUMBER
23134.00

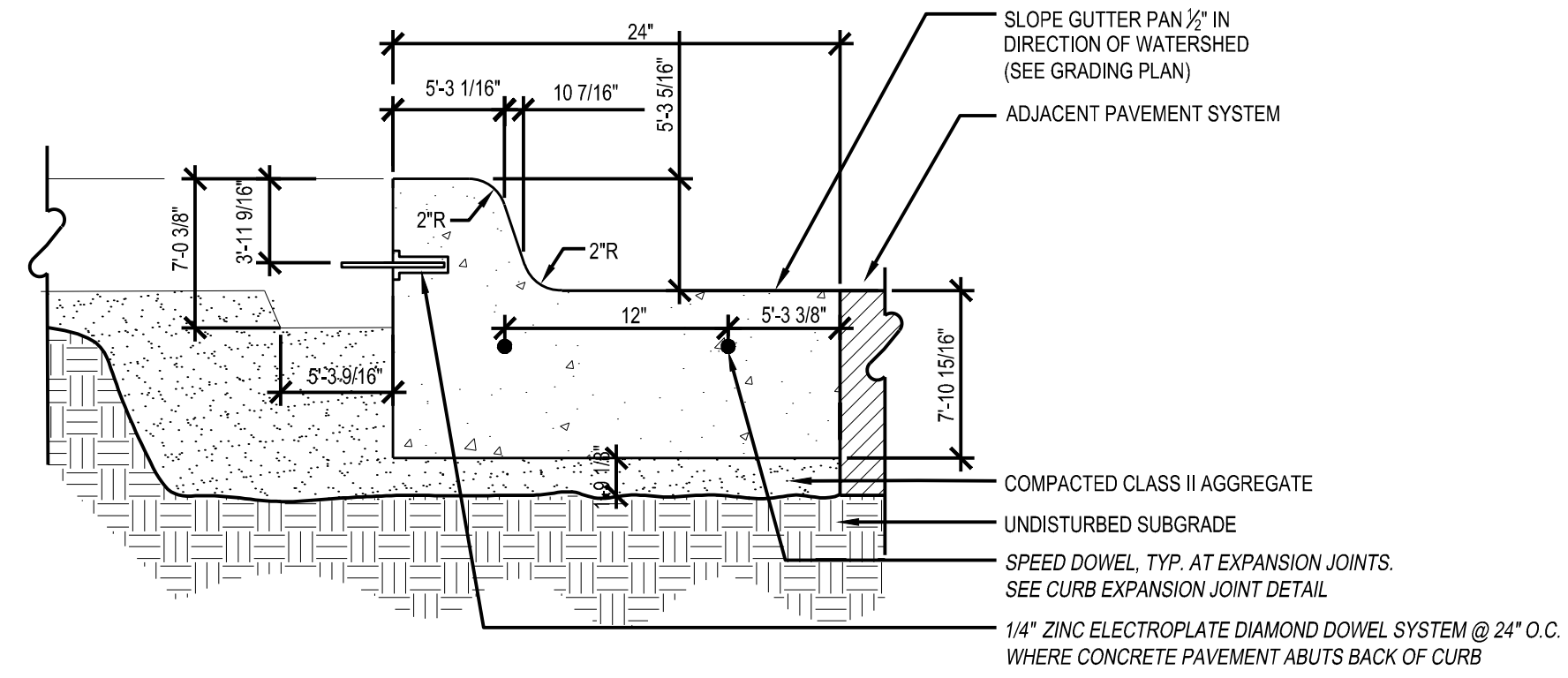
SHEET NUMBER
C-403



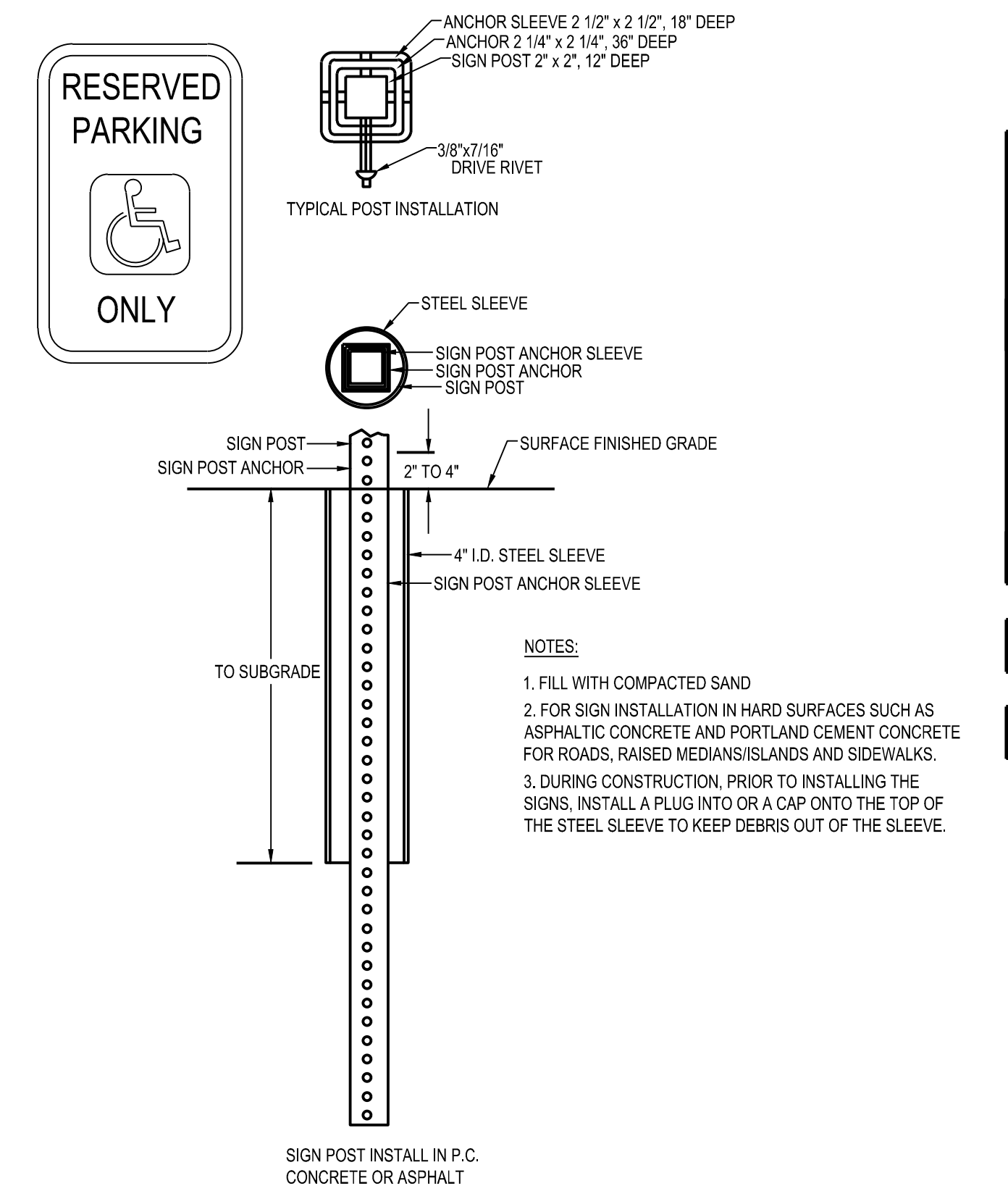
1 SILT FENCE DETAIL
SCALE: NTS



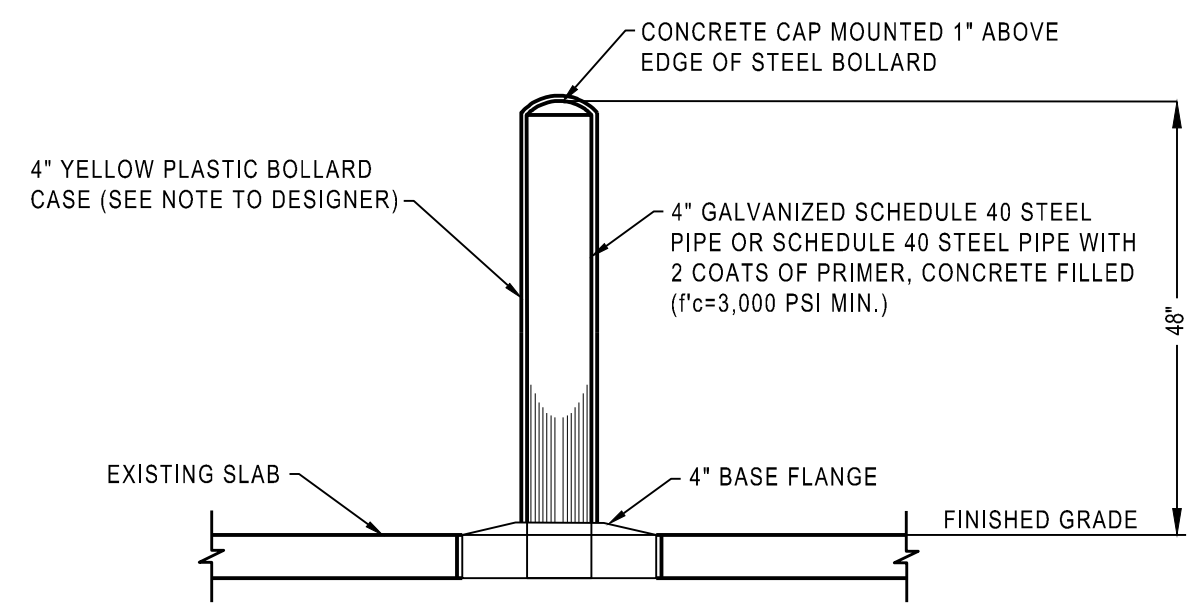
2 AGGREGATE ACCESS DRIVE DETAIL
SCALE: NTS



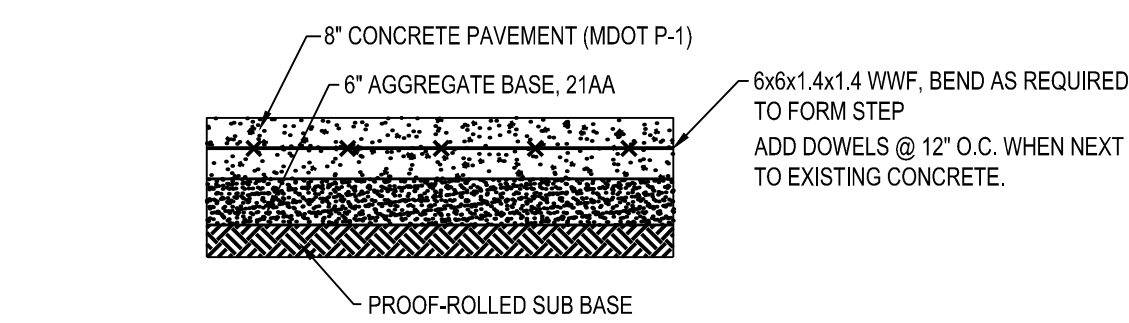
3 CURB AND GUTTER DETAIL
SCALE: NTS



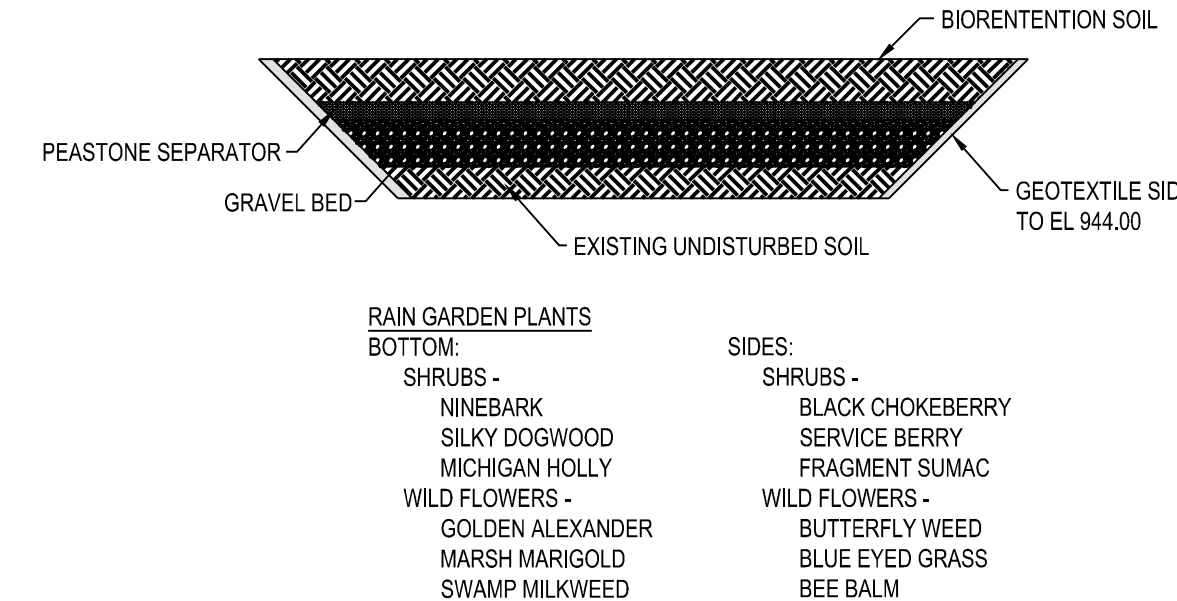
12 HANDICAP SIGN DETAIL
SCALE: NTS



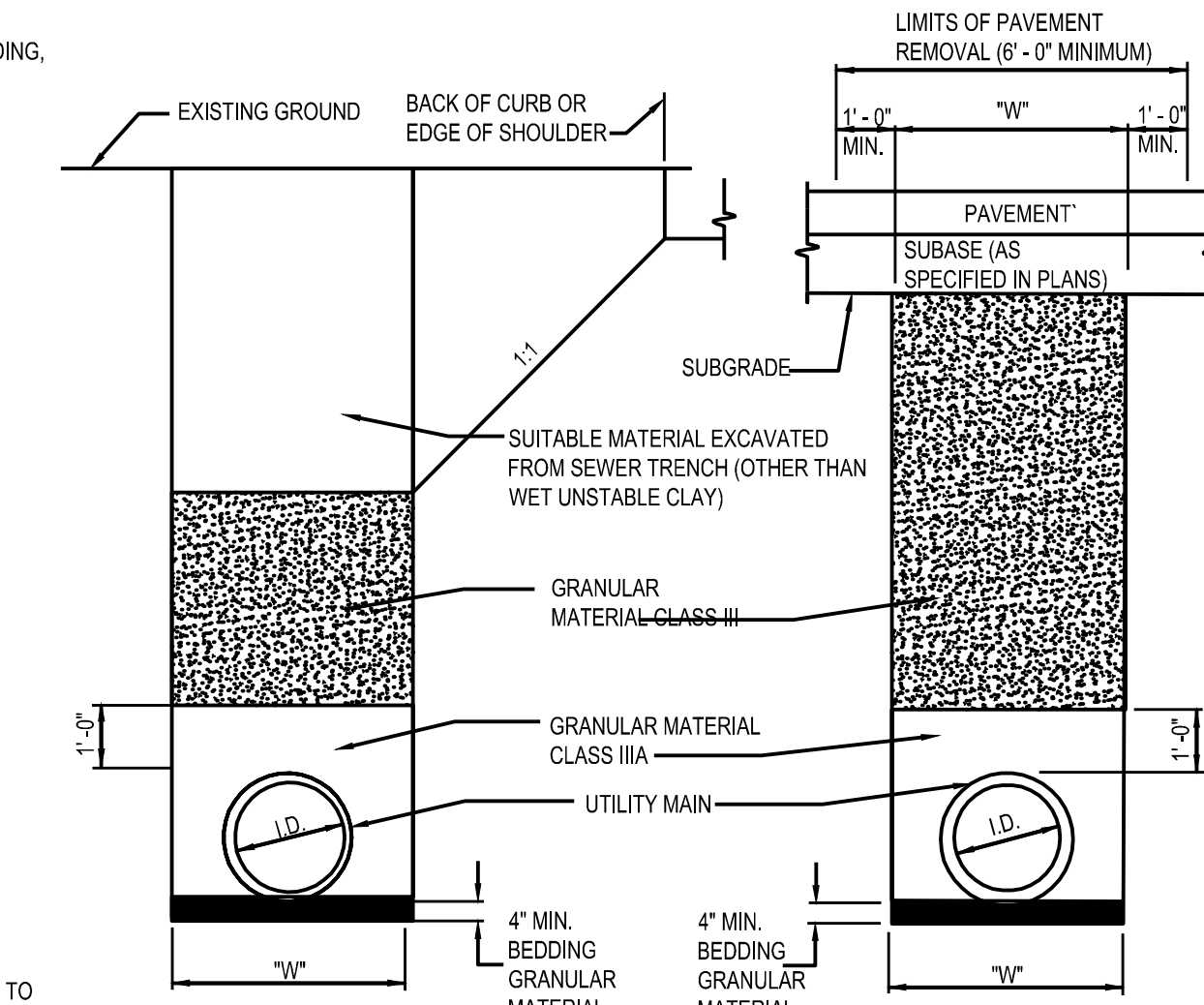
4 BOLLARD DETAIL
SCALE: NTS



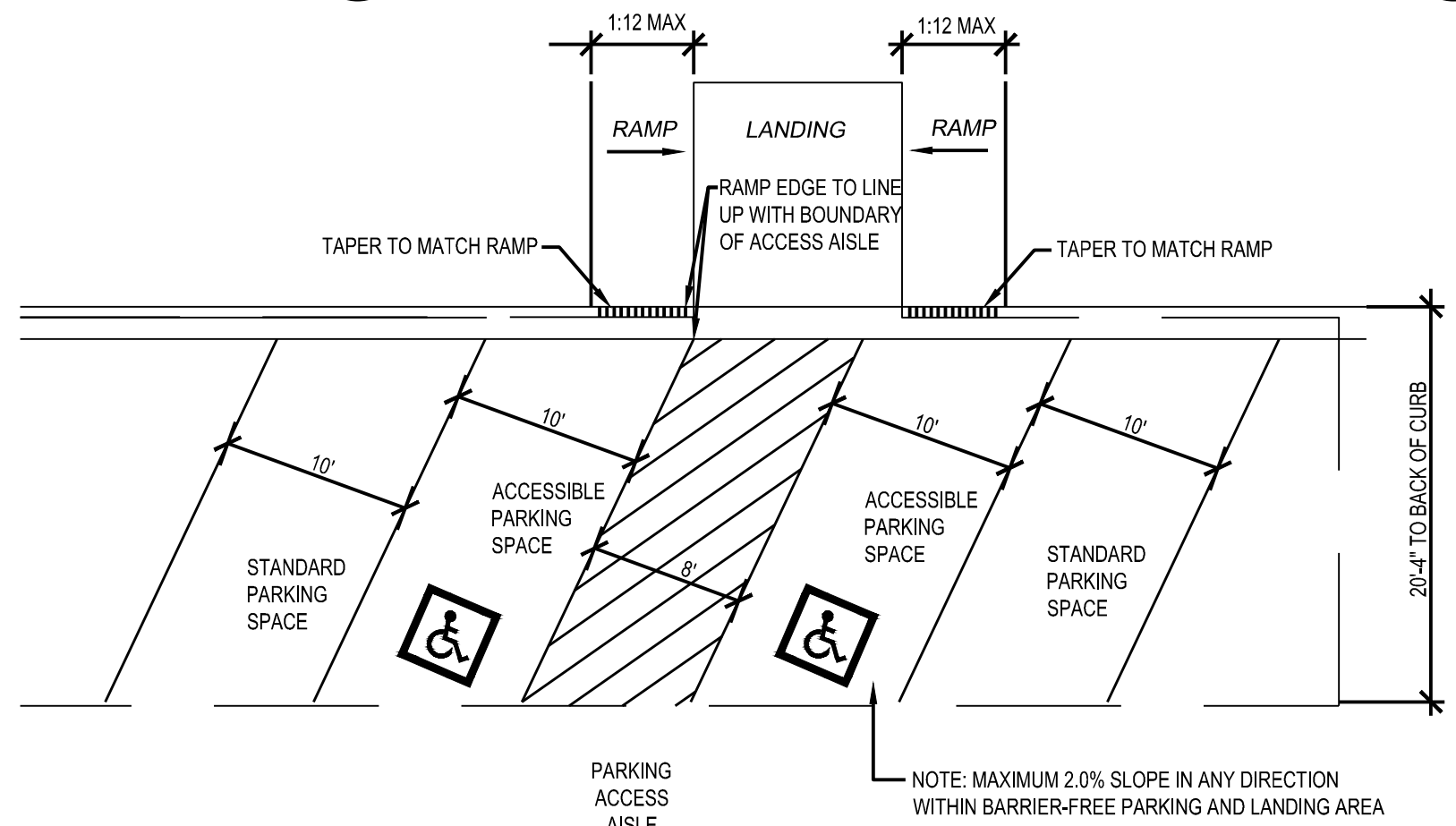
5 CONCRETE PAVEMENT DETAIL
SCALE: NTS



6 RAIN GARDEN DETAIL
SCALE: NTS

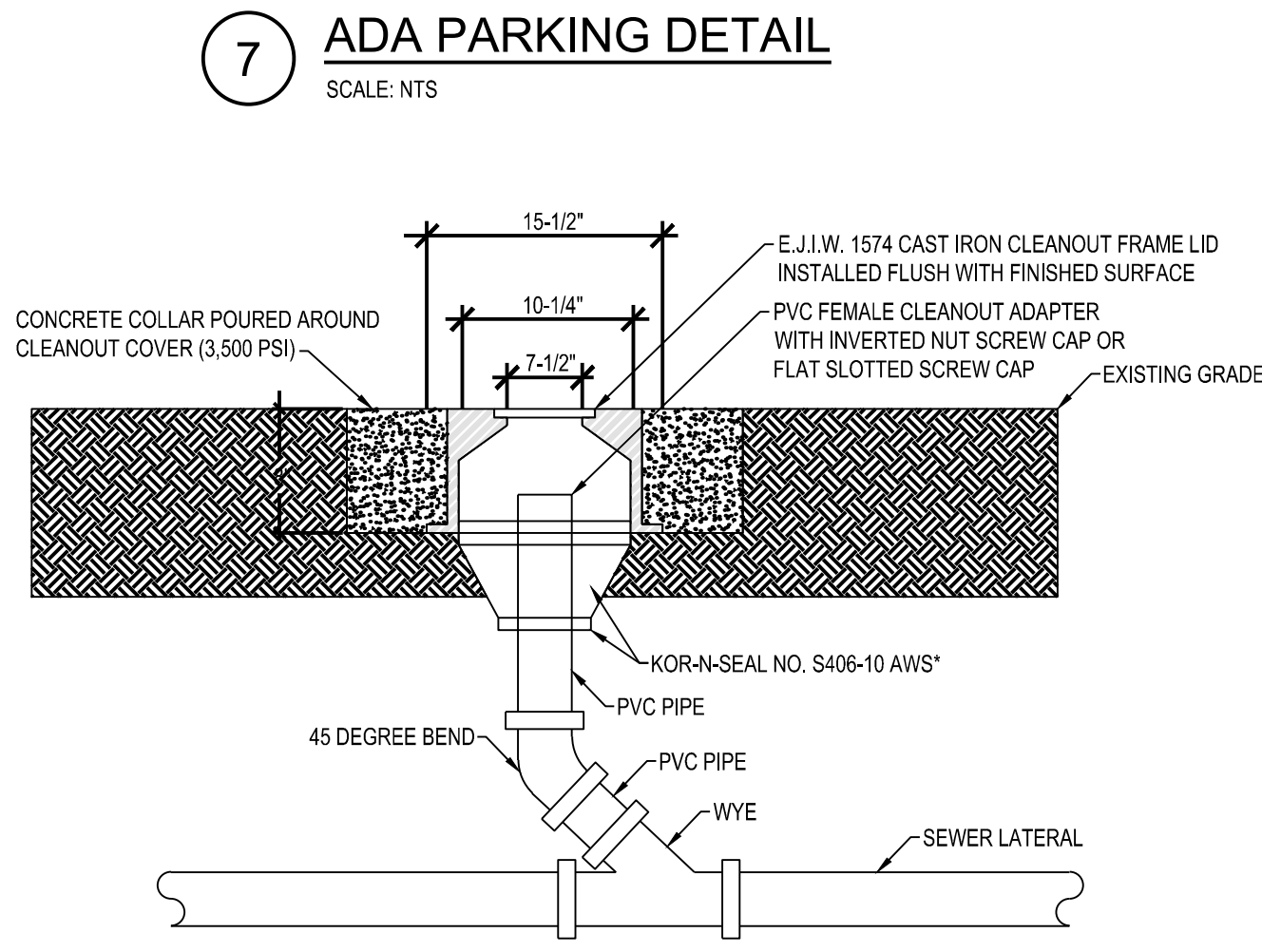
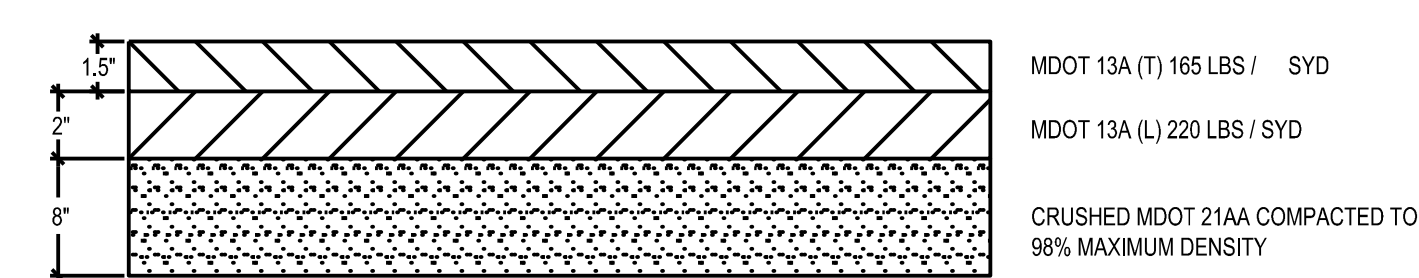


9 UTILITY TRENCH DETAIL
SCALE: NTS

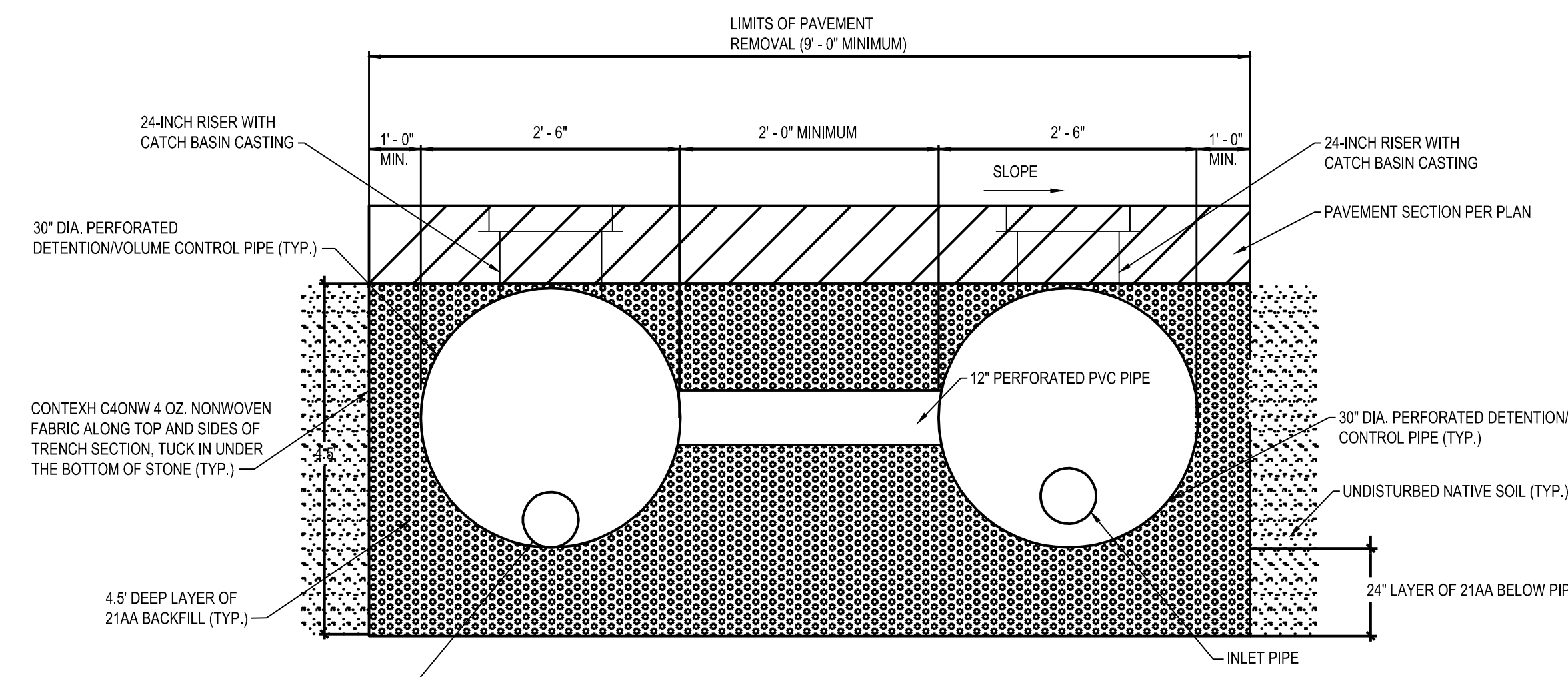
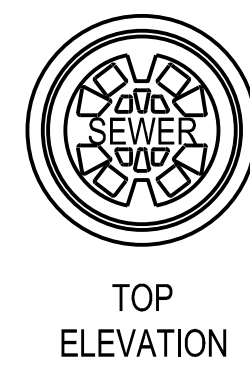


7 ADA PARKING DETAIL
SCALE: NTS

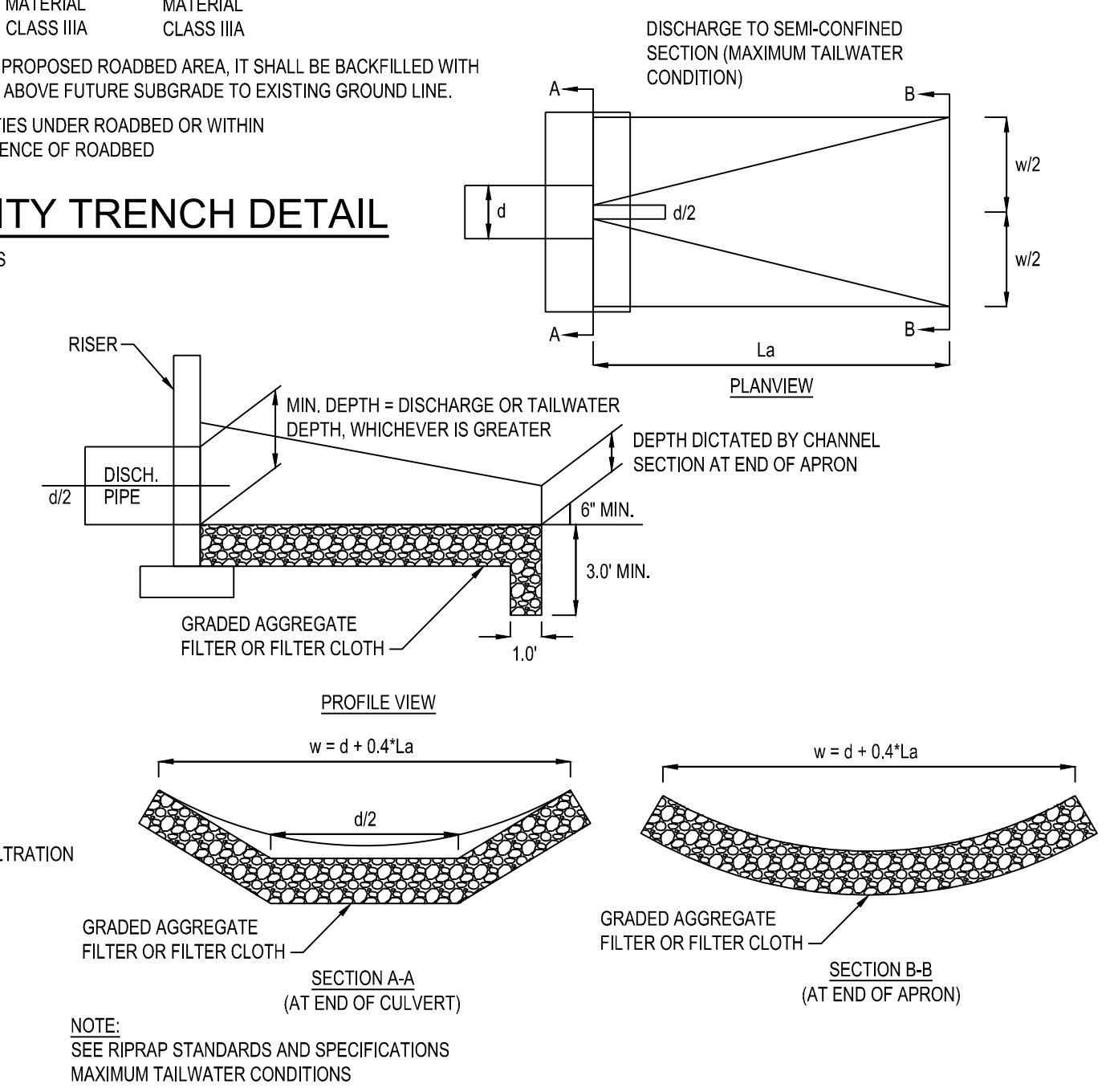
8 TYPICAL HMA CROSS SECTION
SCALE: NTS



10 SANITARY CLEANOUT DETAIL
SCALE: NTS



11 TYPICAL DETENTION/VOLUME CONTROL AND INFILTRATION TRENCH SECTION
SCALE: NTS



13 RIP RAP OUTLET DETAIL
SCALE: NTS

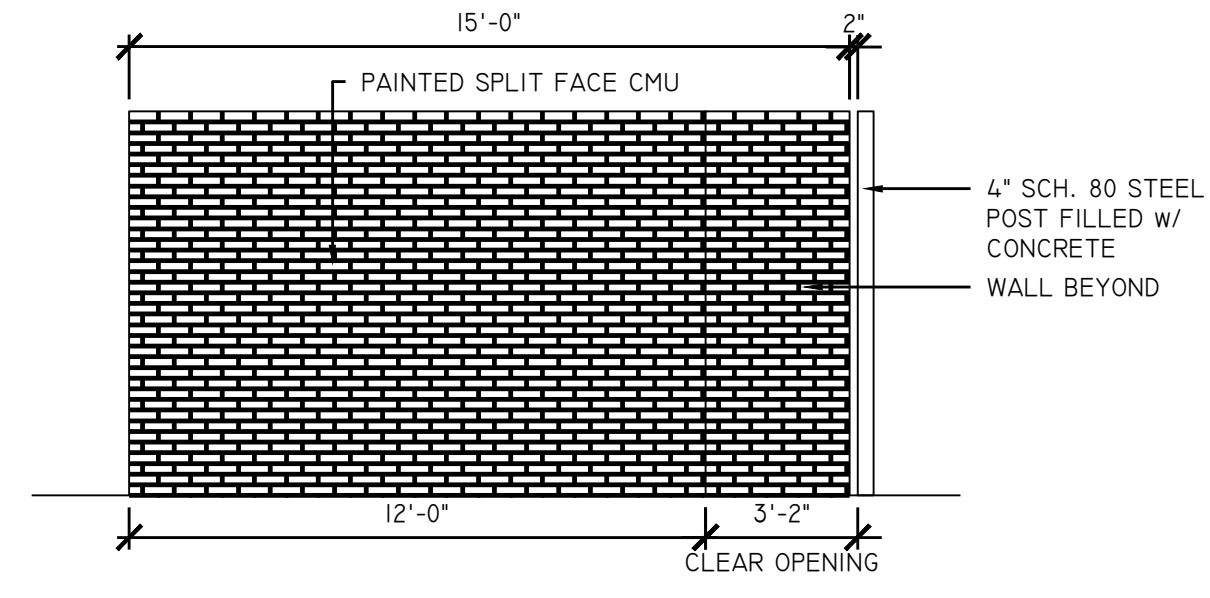
REVISIONS
ISSUED FOR BIDDING 11/20/2023
REVISION

DRAWN: BTT
APPROVED: GLM

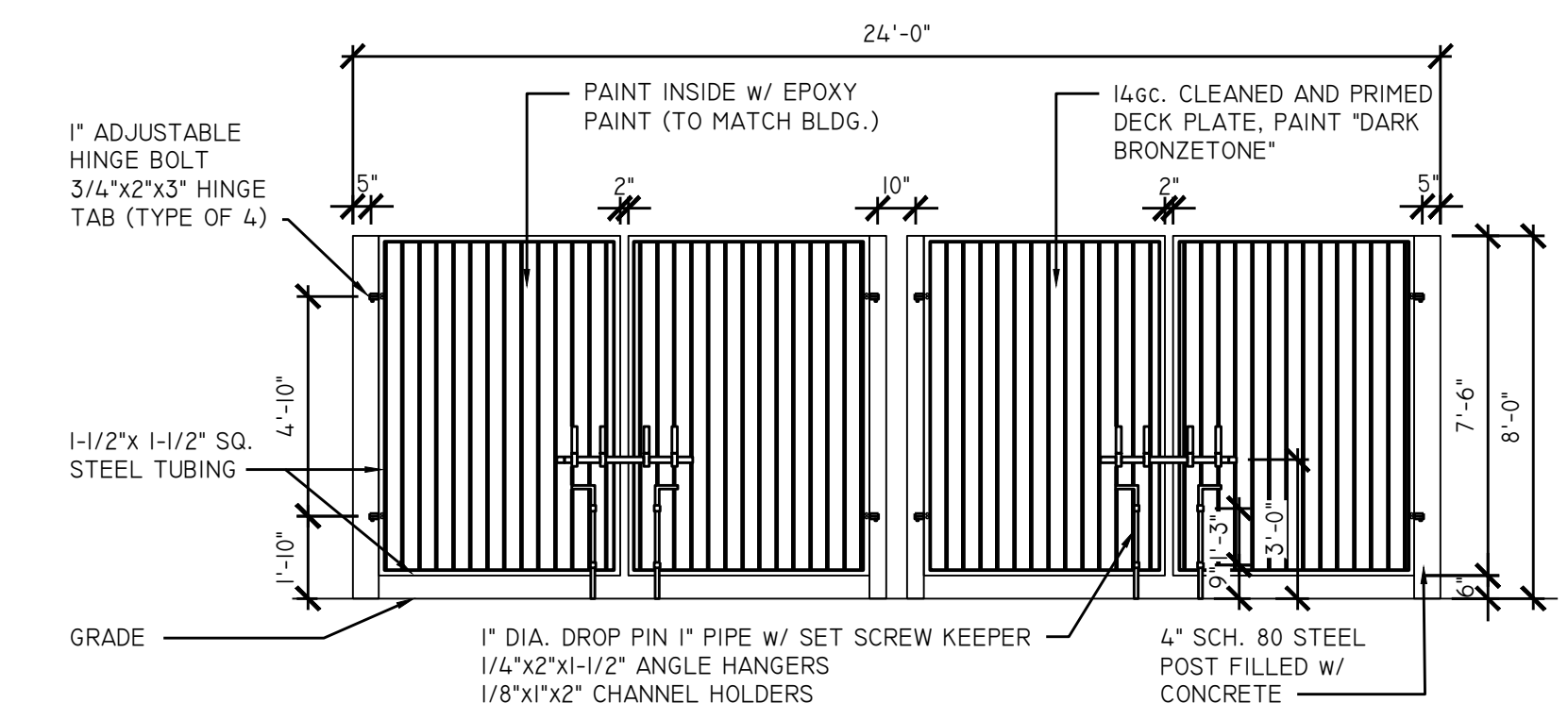
MATRIX CONSULTING ENGINEERS, INC.
460 E. CASAR CHAVEZ AVE
LANSING, MI 48906
PHONE (517) 487-2311
FAX (517) 487-2344
administrator@matrixinc.com
Matrix Project No. 23134.00

PROJECT
LONG JOHN SILVER'S
5481 W. MAIN ST.
KALAMAZOO, MI 49009

SHEET DESCRIPTION: CIVIL DETAILS
DATE: 07.21.23
PROJECT NUMBER: 23134.00
SHEET NUMBER: C-800
51



14 SIDE ELEVATION
NTS



15 FRONT ELEVATION
NTS

NOTES:

1. LOCATION SHALL BE APPROVED BY THE PUBLIC WORKS DEPARTMENT.
2. AREAS SHALL BE ACCESSIBLE FOR DELIVERY AND COLLECTION.
3. GATES SHALL BE CORRUGATED METAL DOORS AND MUST BE APPROVED BY PUBLIC WORKS DEPARTMENT PRIOR TO INSTALLATION.
4. GATE LATCHES SHALL BE OF THE PLUNGER BAR TYPE OR EQUIVALENT AS APPROVED BY THE PUBLIC WORKS DEPARTMENT.
5. SEE ATTACHED SPECIFICATIONS, NOTES AND PROCEDURES

TRASH ENCLOSURE DETAIL MATERIAL SPECIFICATIONS:

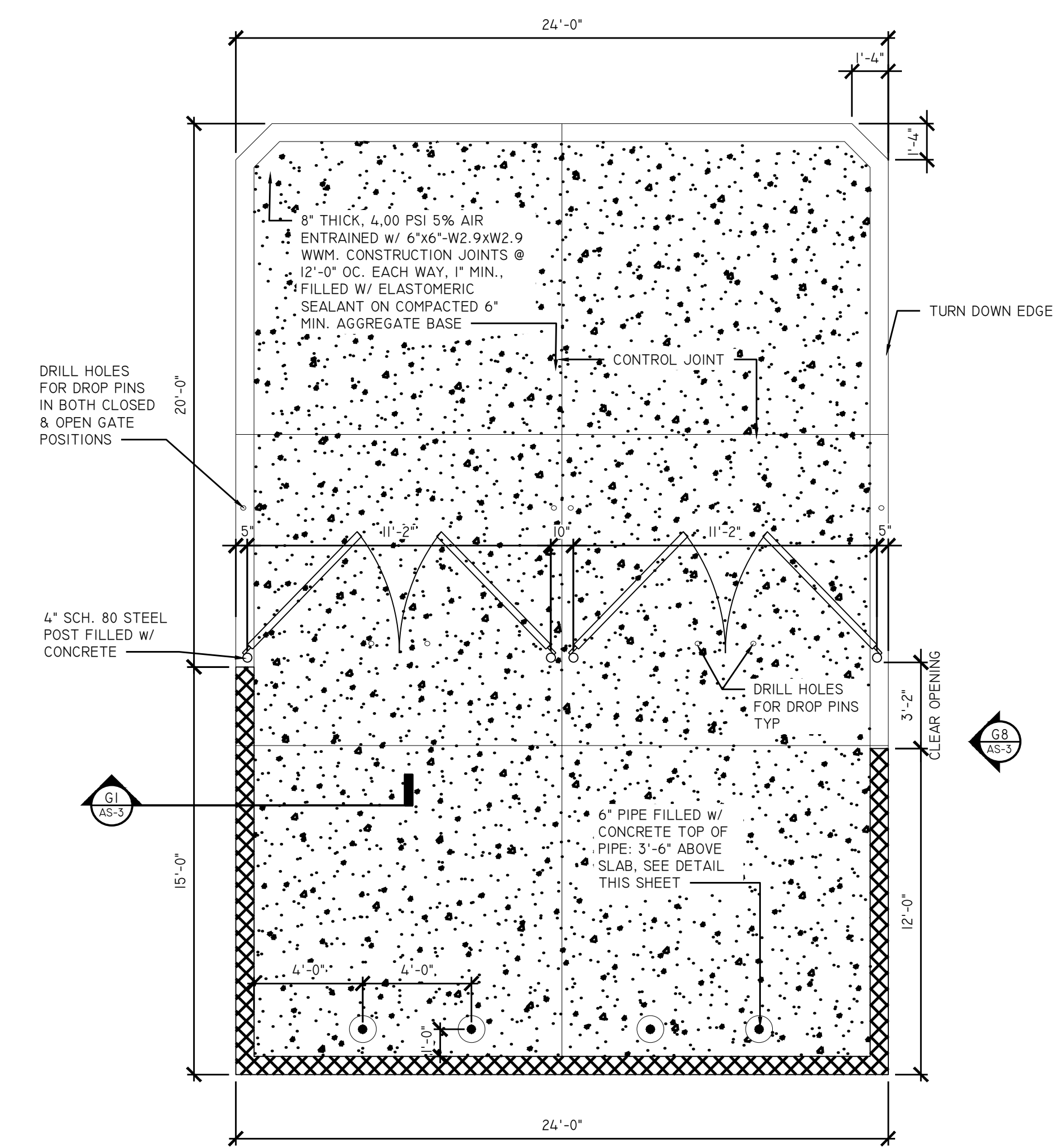
1. CONCRETE BLOCK: 6" MINIMUM IN SIZE. ASTM C90.
2. REINFORCING STEEL: ASTM 615. GRADE 40.
3. ACCEPTABLE SOIL TYPES:
 - A. MIN. ALLOWABLE BEARING: 1000 LBS./SQ. FT.
 - B. MIN. LATERAL BEARING: 100 LBS./SQ. FT./FT.
 - C. MAX EXPANSION INDEX: 20
4. CONCRETE: 2000 LBS./SQ. IN. IN 28 DAYS.
5. MORTAR: 1: 1/4:3 (PORTLAND CEMENT: HYDRATED LIME OR LIME PUTTY: SAND, BY VOLUME). MIXED TO PLASTIC CONSISTENCY. REFER TO UBC TABLE 24-A FOR OTHER TYPES OF CEMENT.
6. GROUT: 1:1/10:3 (PORTLAND CEMENT: HYDRATED LIME OR LIME PUTTY: SAND BY VOLUME). MIX TO FLOW WITHOUT SEGREGATION. GROUT MAY CONTAIN 2 PARTS PEA GRAVEL (3/8" MAX. SIZE). MINIMUM COMPRESSIVE STRENGTH: 2000 LBS/SQ. IN. REFER TO UBC TABLE 24-B FOR OTHER TYPES OF CEMENT.

GENERAL NOTES:

1. THIS DESIGN IS FOR AVERAGE CONDITIONS AND MAY NOT BE SUITABLE FOR ALL CASES. IT IS RECOMMENDED THAT A LICENSED CIVIL OR STRUCTURAL ENGINEER BE CONSULTED.
2. FOOTINGS SHALL BE IN NATURAL SOIL OR CERTIFIED FILL.
3. BLOCKS TO BE STAGGERED (RUNNING BOND).
4. ALL CELLS CONTAINING REINFORCING STEEL SHALL BE GROUTED.
5. APPROVED GROUT STOPS ARE REQUIRED BELOW HORIZONTAL STEEL IN PARTIALLY GROUTED WALLS. BAGS, NEWSPAPERS, ETC. ARE NOT APPROVED GROUT STOPS.
6. INITIAL BED JOINT SHALL BE 1/4" MIN. 1" MAX. SUBSEQUENT BED JOINTS SHALL BE 1/4" - MIN., 5/8" MAX.
7. VERTICAL CONTINUITY OF CELLS SHALL BE UNOBSTRUCTED. MORTAR PROJECTIONS SHALL NOT EXCEED 1/2" FOR 6" AND LARGER BLOCK. MORTAR DROPPINGS OR OTHER FOREIGN MATTER ARE NOT PERMITTED IN CELLS AND MUST BE REMOVED.
8. EXPANSION JOINTS REQUIRED AT 60'-0" MAX. INTERVALS.
9. REQUIRED BAR LAPS:
 - A. VERTICAL STEEL : 30 BAR DIAMETERS.
 - B. HORIZONTAL STEEL: 40 BAR DIAMETERS.
 - C. WIRE JOINT REINFORCEMENT IN THE MORTARED BED JOINT: 75 WIRE DIAMETERS OR IN ALTERNATE BED JOINTS OF RUNNING BOND. 54 DIAMETERS PLUS TWICE THE BED JOINT SPACING.
10. WHERE HORIZONTAL WIRE JOINT REINFORCEMENT IS REQUIRED OR UTILIZED. IT SHALL BE EQUIVALENT TO TWO 3/16" DIAMETER BARS CONNECTED AT 16" INTERVALS BY NO. 9 GAUGE WELDED WIRE.
11. FOR PIPES AND CONDUIT EMBEDDED IN MASONRY, REFER TO SEC. 24-07(G). UBC.
12. 3" MIN. COVER REQUIRED FOR REINFORCEMENT IN CONCRETE WHICH IS CAST AGAINST EARTH.

INSPECTION PROCEDURES:

1. FOUNDATION: AFTER TRENCHES ARE DUG, STEEL IS TIED IN PLACE AND BEFORE ANY CONCRETE IS POURED.
2. PREGROUT: AFTER ALL BLOCKS (EXCEPT CAP) ARE IN PLACE. VERTICAL AND HORIZONTAL STEEL IS IN PLACE, GROUT STOPS (FOR PARTIALLY GROUTED MASONRY) ARE IN PLACE, AND PRIOR TO GROUTING.
3. FINAL: AFTER GROUT IS IN PLACE AND PRIOR TO PLACEMENT OF CAP.



16 DUMPSTER ENCLOSURE DETAIL
NTS

REVISIONS
10/23/2023 ISSUED FOR BIDDING
11/20/2023 REVISION

DRAWN
BTT

APPROVED
GLM

MATRIX
CONSULTING ENGINEERS, INC.

1601 E. CESAR CHAVEZ AVE.
LANSING, MI 48906
PHONE (313) 487-2511
FAX (313) 487-2544
administrators@matrixinc.com
Market Report No. 23134.00

PROJECT

LONG JOHN SILVER'S

5481 W. MAIN ST.
KALAMAZOO, MI
49009

SHEET DESCRIPTION

**DUMPSTER
DETAILS**

DATE

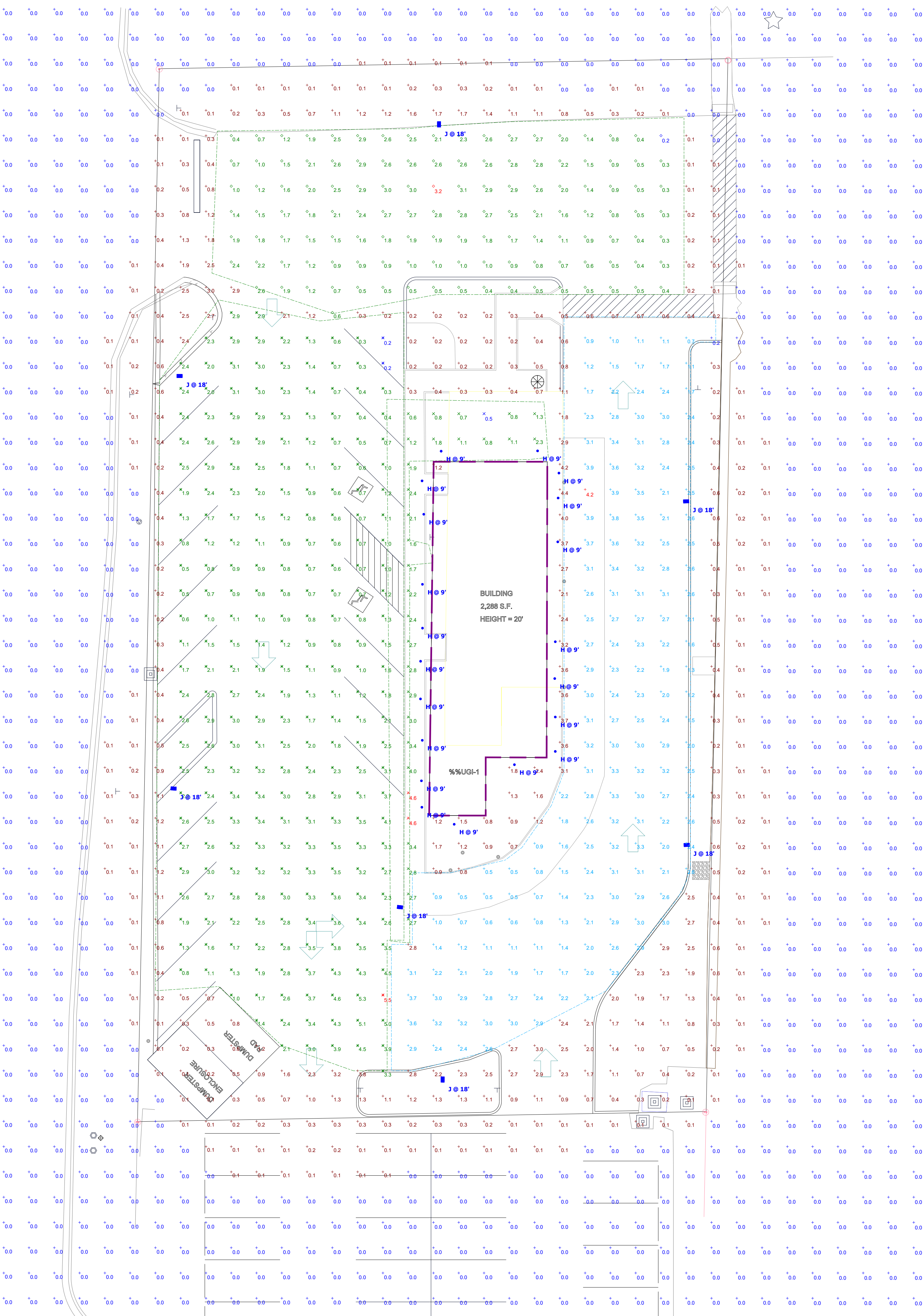
07.21.23

PROJECT NUMBER

23134.00

SHEET NUMBER

C-801



Plan View

Schedule											
Symbol	Label	Image	Quantity	Manufacturer	Catalog Number	Description	Number Lamps	Lumens Per Lamp	Light Loss Factor	Wattage	Plot
	H		20	Cree Lighting	A19-60W-B2-50K-E26-U4	A19 CREE BASIC 5000K	1	923	1	8.9	Max: 109cd
	J		7	COOPER LIGHTING SOLUTIONS - McGRAW-EDISON (FORMERLY EATON)	GALN-SA2A-740-U-T3-HSS	GALLEON AREA AND ROADWAY LUMINAIRE (2) 70 CRI, 4000K, 615mA LIGHTSQUARES WITH 16 LEDES EACH AND TYPE III OPTICS WITH HOUSE SIDE SHIELD	32	211	1	63	Max: 5629cd

Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Entrance/Exit		1.5 fc	3.2 fc	0.2 fc	16.0:1	7.5:1
Walk-In		2.2 fc	4.6 fc	0.5 fc	9.2:1	4.4:1
Drive-Thru		2.3 fc	4.2 fc	0.2 fc	21.0:1	11.5:1
Parking		2.1 fc	5.5 fc	0.2 fc	27.5:1	10.5:1

- NOTES:
1. All lights shall be cut-off fixtures.
 2. A minimum color rendering index (CRI) of 65.
 3. A Kelvin rating between 3,000-5,000K.

50330



Wall Lantern

Width (in): 12

Height (in): 12.5

Depth (in): 14

Glass Type: n/a

Bulb Type: Medium base

Number of Bulbs: 1 Provide with nominal 900 lumen bulb.

Wattage: 60

Finish Shown: Weathered Bronze, Silver

Available Finishes: Weathered Bronze (WB), Silver (SL)

UL Listed: Wet

Country of Origin: China

Material: Metal, Glass, Electrical

- Multiple finishes available

Job Name:

Job Type:

Contact:

Fixture Location:

Quantity:

Additional Notes:

Shipping Box	Width	Height	Depth	Ship Wt	Method	Oversize	Case QTY
Ship QTY: 1 ea					Ground	No	
UPC: by finish		Finish:		Warranty: 1 year limited parts, repair, and replacement			

Project		Catalog #		Type	
Prepared by		Notes		Date	



McGraw-Edison

GALN Galleon II

Area / Site Luminaire

Product Features



Interactive Menu

- Ordering Information page 2
- Mounting Details page 3
- Optical Distributions page 5
- Product Specifications page 5
- Energy and Performance Data page 6
- Control Options page 10

Product Certifications



Quick Facts

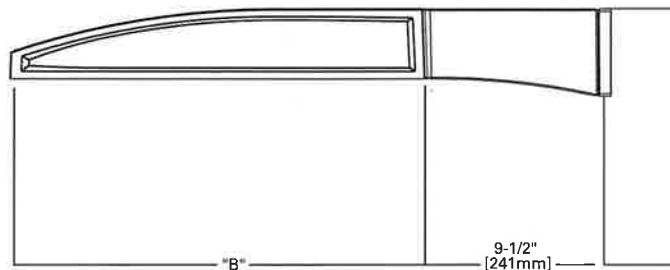
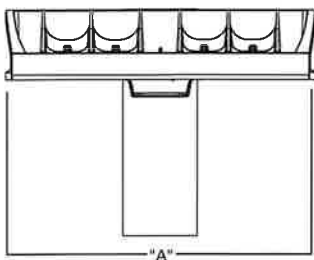
- Lumen packages range from 3,300 - 73,500 (33W - 552W)
- 16 optical distributions
- Efficacy up to 159 lumens per watt

Connected Systems

- WaveLinx Lite
- WaveLinx

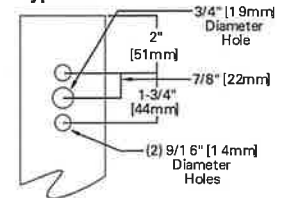
Dimensional Details

Standard Arm



Pole Drilling Patterns

Type "N"



Number of Light Squares	Width "A"	Housing Length "B"	Weight with Standard or QM Arm	EPA with Standard or QM Arm
1-4	16"	22"	29 lb	0.95
5-6	22"	22"	39 lb	0.95
7-9	22"	28-1/8"	48 lb	1.1

NOTES:
For arm selection requirements and additional line art, see Mounting Details section.

NOTES:
1. Visit <https://www.designlights.org/search/> to confirm qualification. Not all product variations are DLC qualified.
2. IDA Certified (3000K CCT and warmer only, fixed mounting options)


Ordering Information

SAMPLE NUMBER: GALN-SA4C-740-U-T4FT-GM

Product Family ^{1,2}	Light Engine		Color Temperature	Voltage	Distribution	Mounting	Finish
	Configuration	Drive Current					
GALN=Galleon II BAA-GALN=Galleon II Buy American Act Compliant ²⁷ TAA-GALN=Galleon II Trade Agreements Act Compliant ²⁷	SA1=1 Square SA2=2 Squares SA3=3 Squares SA4=4 Squares SA5=5 Squares SA6=6 Squares SA7=7 Squares SA8=8 Squares SA9=9 Squares	A=600mA B=800mA C=1000mA D=1200mA ^{4,17} Z=Configured ³³	722=70CRI, 2200K 727=70CRI, 2700K 730=70CRI, 3000K 735=70CRI, 3500K 740=70CRI, 4000K 750=70CRI, 5000K 760=70CRI, 6000K 827=80CRI, 2700K 830=80CRI, 3000K AMB=Amber, 590nm ^{15,17}	U=120-277V H=347V-480V ^{7,30} I=120V 2=208V 3=240V 4=277V 8=480V ⁷ 9=347V ^{7,30} DV=277V-480V DuraVolt Drivers ^{29,30,31}	T2=Type II T2R=Type II Roadway T3=Type III T3R=Type III Roadway T4FT=Type IV Forward Throw T4W=Type IV Wide 5NQ=Type V Narrow 5MQ=Type V Square Medium 5WQ=Type V Square Wide SL2=Type II w/Spill Control SL3=Type III w/Spill Control SL4=Type IV w/Spill Control SLL=90° Spill Light Eliminator Left SLR=90° Spill Light Eliminator Right RW=Rectangular Wide Type I AFL=Automotive Frontline	[Blank]=Standard Pole Mount Arm QM=Standard Pole Mount Arm with Quick Mount Adaptor PA=Pole Mount, Adjustable SP=Slipfitter, Adjustable ⁹ MA=Mast Arm, Fixed WM=Wall Mount, Fixed WA=Wall Mount, Adjustable UP=Upswept Arm	AP=Grey BZ=Bronze BK=Black DP=Dark Platinum GM=Graphite Metallic WH=White RALXX=Custom Color
Options (Add as Suffix)			Controls and Systems Options (Add as Suffix)		Accessories (Order Separately) ²⁸		
DIM=External 0-10V Dimming Leads ²⁹ F=Single Fuse (120, 277 or 347V Specify Voltage) FF=Double Fuse (208, 240 or 480V Specify Voltage) 20K=20KV UL 1449 fused surge protective device ¹⁰ 2L=Two Circuits ¹⁰ HA=50°C High Ambient HSS=Installed House Side Shield ¹⁸ GRSBK=Glare Reducing Shield, Black ²³ GRSWH=Glare Reducing Shield, White ²³ LCF=Light Square Trim Painted to Match Housing ²⁵ TH=Tool-less Door Hardware ⁵ CC=Coastal Construction finish ³ L90=Optics Rotated 90° Left R90=Optics Rotated 90° Right AHD145=After Hours Dim, 5 Hours ²² AHD245=After Hours Dim, 6 Hours ²² AHD255=After Hours Dim, 7 Hours ²² AHD355=After Hours Dim, 8 Hours ²² DALI=DALI Drivers			BPC=Button Type Photocontrol ⁶ PR=NEMA 3-PIN Photocontrol Receptacle PR7=NEMA 7-PIN Photocontrol Receptacle ²¹ FADC=Field Adjustable Dimming Controller ²² SPB2=Dimming Motion Sensor, 9'-20' mounting ²⁴ SPB4=Dimming Motion Sensor, 21'-40' mounting ²⁴ SPB2/X=Dimming Motion Sensor, limited square count, 9'-20' mounting ²⁴ SPB4/X=Dimming Motion Sensor, limited square count, 21'-40' mounting ²⁴ ZW=WaveLinX-enabled 4-PIN Twistlock Receptacle ¹⁹ ZD=SR Driver-enabled 4-PIN Twistlock Receptacle ¹⁹ ZW-WOBXX=WaveLinX Lite, Dimming Motion and Daylight, Bluetooth Programmable, 7' - 15' Mounting ^{15,12,13} ZW-WOFXX=WaveLinX Lite, Dimming Motion and Daylight, Bluetooth Programmable, 15' - 40' Mounting ^{15,12,13} ZD-WOBXX=WaveLinX Lite, SR Driver, Dimming Motion and Daylight, Bluetooth Programmable, 7' - 15' Mounting ^{15,12,13} ZD-WOFXX=WaveLinX Lite, SR Driver, Dimming Motion and Daylight, Bluetooth Programmable, 15' - 40' Mounting ^{15,12,13} ZW-SWPD4XX=WaveLinX Pro, Dimming Motion and Daylight, WAC Programmable, 7' - 15' Mounting ^{15,12,13} ZW-SWPD5XX=WaveLinX Pro, Dimming Motion and Daylight, WAC Programmable, 15' - 40' Mounting ^{15,12,13} ZD-SWPD4XX=WaveLinX Pro, SR Driver, Dimming Motion and Daylight, WAC Programmable, 7' - 15' Mounting ^{15,12,13} ZD-SWPD5XX=WaveLinX Pro, SR Driver, Dimming Motion and Daylight, WAC Programmable, 15' - 40' Mounting ^{15,12,13} DIM10-L20=Synapse Occupancy Sensor (9'-20' Mounting) ¹⁹ DIM10-L40=Synapse Occupancy Sensor (21'-40' Mounting) ¹⁹		OA/RA1013=Photocontrol Shorting Cap MA1252=10KV Surge Module Replacement MA1036-XX=Single Tenon Adapter for 2-3/8" O.D. Tenon MA1037-XX=2@180° Tenon Adapter for 2-3/8" O.D. Tenon MA1197-XX=3@120° Tenon Adapter for 2-3/8" O.D. Tenon MA1188-XX=4@90° Tenon Adapter for 2-3/8" O.D. Tenon MA1189-XX=2@90° Tenon Adapter for 2-3/8" O.D. Tenon MA1190-XX=3@90° Tenon Adapter for 2-3/8" O.D. Tenon MA1191-XX=2@120° Tenon Adapter for 3-1/2" O.D. Tenon MA1038-XX=Single Tenon Adapter for 3-1/2" O.D. Tenon MA1039-XX=2@180° Tenon Adapter for 3-1/2" O.D. Tenon MA1192-XX=3@120° Tenon Adapter for 3-1/2" O.D. Tenon MA1193-XX=4@90° Tenon Adapter for 3-1/2" O.D. Tenon MA1194-XX=2@90° Tenon Adapter for 3-1/2" O.D. Tenon MA1195-XX=3@90° Tenon Adapter for 3-1/2" O.D. Tenon SRA238=Adapter kit for mounting to 2-3/8" O.D. Tenon LS/HSS=Field Installed House Side Shield ^{5,18} LS/GRSBK-2PK =Glare Reducing Shield, Black ^{9,23} LS/GRSWH-2PK =Glare Reducing Shield, White ^{9,23} LS/PFS=Perimeter Shield, Black ¹⁶ WOLC-7P-10A=WaveLinX Outdoor Control Module ^{11,19} WOB-XX=WaveLinX Lite Sensor, Dimming Motion and Daylight, Bluetooth Programmable, 7' - 15' Mounting ^{12,13,14,19} WOF-XX=WaveLinX Lite Sensor, Dimming Motion and Daylight, Bluetooth Programmable, 15' - 40' Mounting ^{12,13,14,19} SWPD4-XX=WaveLinX Sensor, Dimming Motion and Daylight, WAC Programmable, 7' - 15' Mounting ^{12,13,14,19} SWPD5-XX=WaveLinX Sensor, Dimming Motion and Daylight, WAC Programmable, 15' - 40' Mounting ^{12,13,14,19}		

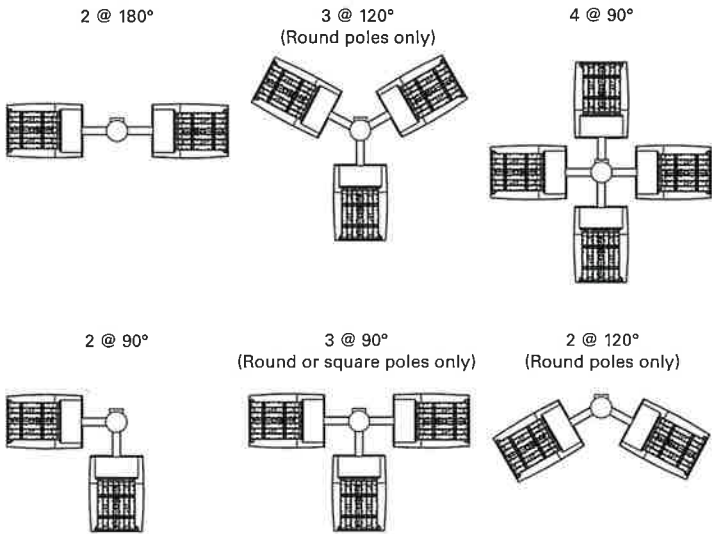
- NOTES:
- Customer is responsible for engineering analysis to confirm pole and fixture compatibility for all applications. Refer to our white paper WP513001EN for additional support information.
 - DesignLights Consortium® Qualified. Refer to www.designlights.org Qualified Products List under Family Models for details.
 - Coastal construction finish salt sprayed tested to over 5,000-hours per ASTM B117, with a scribe rating of 9 per ASTM D1654. Not available with TH option.
 - Drive current 1200mA not available with color temperatures 722, 727 or 830 when either HA or HSS options are selected.
 - TH option not 3G rated. Not available with Coastal Construction (CC) option.
 - Not available with voltage options H, 8 or 9.
 - Requires the use of an Internal step down transformer when combined with sensor options. Not available in combination with the HA high ambient and sensor options at 1A.
 - Adjustable Slipfitter arm limited to vertical 3" tenon. For mounting to 2-3/8" O.D. tenons, order accessory SRA238.
 - One required for each Light Square.
 - 2L is not available with SPB at 347V or 480V. Not available with WaveLinX or Enlghted sensors, or 20KV surge option.
 - Requires PR7.
 - Replace XX with sensor color (WH, BZ or BK)
 - WAC Gateway required to enable field-configurability: Order WAC-PoE and WPOE-120 (10V to PoE Injector) power supply if needed. WAC not required for LC Bluetooth sensors.
 - Requires ZW or ZD receptacle.
 - Narrow-band 590nm +/- 5nm for wildlife and observatory use. Choose drive current A; supplied at 500mA drive current only. Exact luminaire wattage available in IES files. Available with 5WQ, 5MQ, SL2, SL3 and SL4 distributions. Can be used with HSS option.
 - Set of 4 pcs. One set required per Light Square.
 - Not available with HA option.
 - Not for use with 5WQ, 5MQ, 5WQ or RW optics.
 - Cannot be used with other control options.
 - Low voltage control lead brought out 18" outside fixture. Not available with DALI or integrated controls options
 - Not available if any SPB, LWR, or WaveLinX sensor is selected. Motion sensor has an integral photocell.
 - Requires the use of BPC photocontrol or the PR7 or PR photocontrol receptacle with photocontrol accessory.
 - Not for use with T4FT, T4W or SL4 optics. See IES files for details.
 - Sensor configuration mobile application required for configuration. See controls page for details.
 - Replace X with number of Light Squares controlled by the SPB, referencing the "SPB/X Availability Table" on the controls page.
 - Not available with HSS, GRSWH or GRSBK.
 - Only product configurations with these designated prefixes are built to be compliant with the Buy American Act of 1933 (BAA) or Trade Agreements Act of 1979 (TAA), respectively. Please refer to DOMESTIC.PREFERENCES website for more information. Components shipped separately may be separately analyzed under domestic preference requirements.
 - For BAA or TAA requirements, Accessories sold separately will be separately analyzed under domestic preference requirements. Consult factory for further information.
 - DuraVolt drivers feature added protection from power quality issues such as loss of neutral, transients and voltage fluctuations. Visit www.signify.com/duravolt for more information.
 - 480V not to be used with ungrounded or impedance grounded systems.
 - Not available in 1 square configuration at 800mA or below. Not available with any control option except SPB.
 - Cannot be used with PR7 or other motion response control options.
 - Use GALN Product Configurator to specify lumen output, drive current and wattage. Not available with AMB.

LumenSafe Integrated Network Security Camera Technology Options (Add as Suffix)

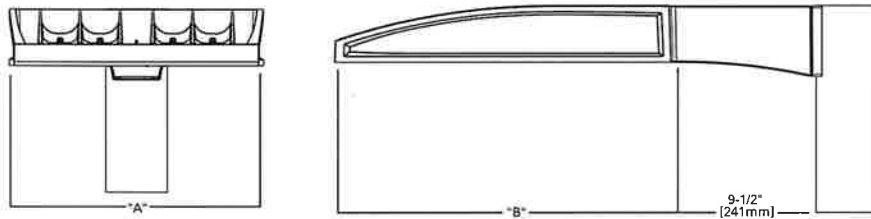
Product Family	Camera Type	Data Backhaul
L=LumenSafe Technology 	D=Standard Dome Camera H=Hi-Res Dome Camera Z=Remote PTZ Camera	C=Cellular, No SIM A=Cellular, AT&T V=Cellular, Verizon S=Cellular, Sprint R=Cellular, Rogers W=Wi-Fi Networking w/ Omni-Directional Antenna E=Ethernet Networking

Mounting Details

Pole Configuration Options

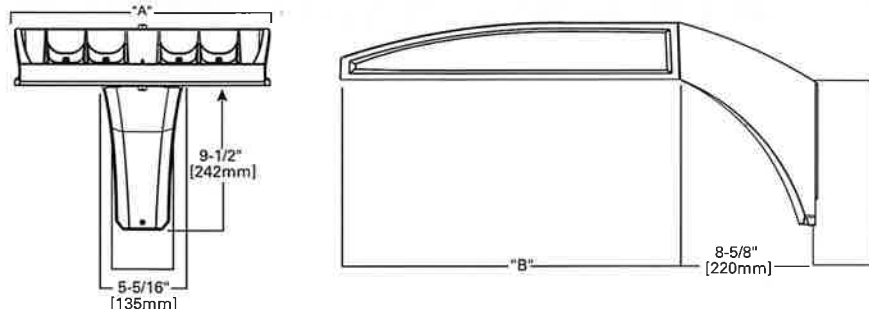


Quick Mount Arm (QM) *



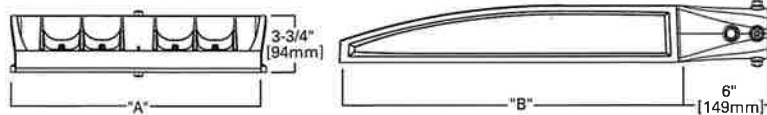
*NOTE: Use Type N drilling pattern

Upswept Arm (UP) *

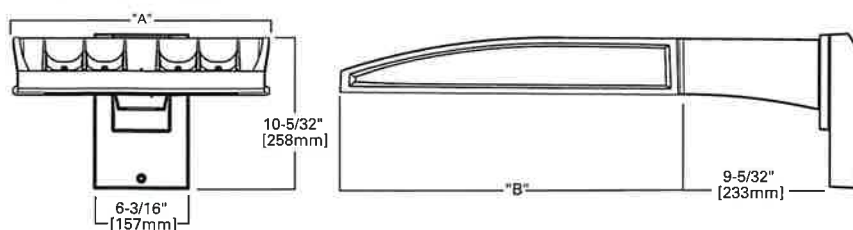


*NOTE: Use Type N, R or M drilling pattern

Mast Arm, Fixed (MA)

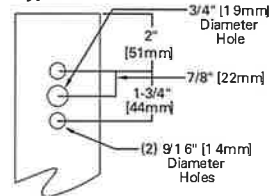


Wall Mount, Fixed (WM)

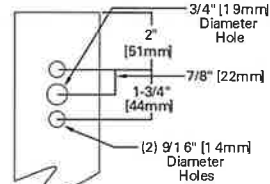


Pole Drilling Patterns

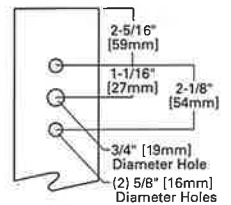
Type "N"



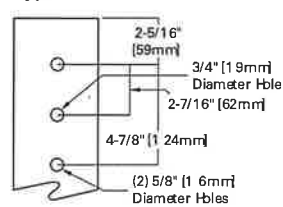
Type "N"



Type "R"

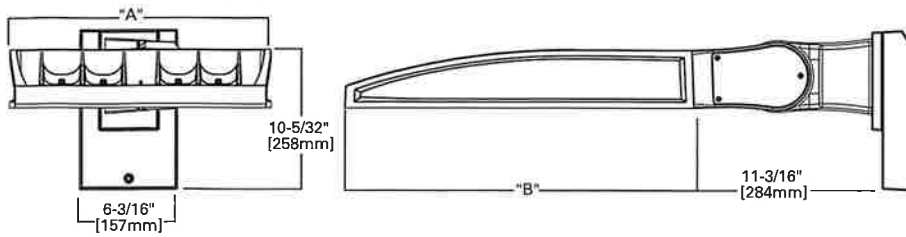


Type "M"



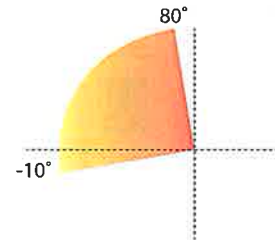
Mounting Details

Wall Mount, Adjustable (WA)

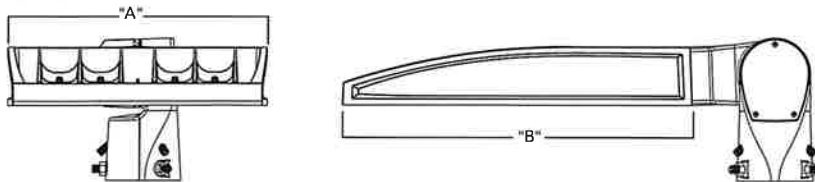


Adjustable Arm Range of Motion

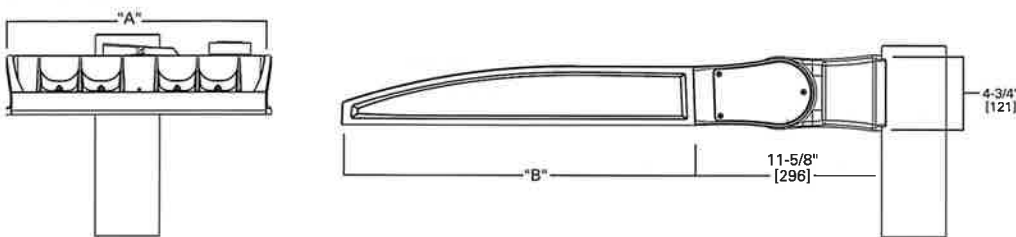
- Wall Mount (WA), Slipfitter (SP) and Pole Mount (PA)
- Adjustable in increments of 5°
- Must maintain downward facing orientation



Slipfitter, Adjustable (SP)



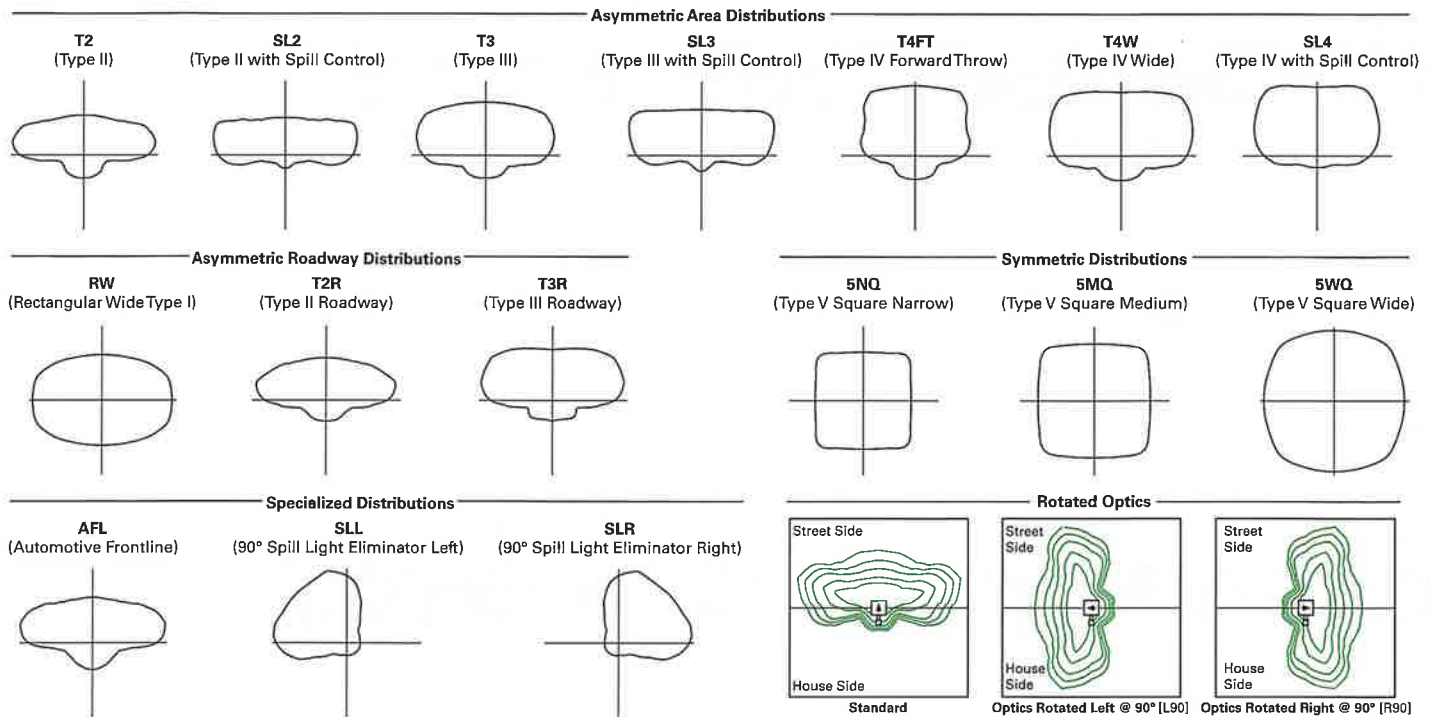
Pole Mount, Adjustable Arm (PA)



Fixture Weights and EPAs

Tilt Angle (Degrees)	Number of Light Squares	Weight	1 @ 90°	2 @ 180°	2 @ 90°	2 @ 120°	3 @ 90°	3 @ 120°	4 @ 90°
0°	1-4	33.5 lb (15.2 kg)	0.85	1.70	1.46	1.66	2.31	2.25	2.35
	5-6	43.5 lb (19.7 kg)	0.86	1.71	1.62	1.80	2.49	2.35	2.50
	7-9	52.5 lb (23.8 kg)	0.98	1.95	1.75	1.98	2.73	2.55	2.76
15°	1-4	33.5 lb (15.2 kg)	1.10	1.71	1.95	2.26	2.81	3.30	2.87
	5-6	43.5 lb (19.7 kg)	1.42	1.71	2.27	2.72	3.13	3.63	3.15
	7-9	52.5 lb (23.8 kg)	1.69	1.96	2.67	3.22	3.65	4.38	3.72
30°	1-4	33.5 lb (15.2 kg)	1.72	1.81	2.58	3.21	3.44	4.59	3.53
	5-6	43.5 lb (19.7 kg)	2.26	2.29	3.11	4.00	3.97	5.27	4.00
	7-9	52.5 lb (23.8 kg)	2.75	2.85	3.73	4.83	4.71	6.45	4.81
45°	1-4	33.5 lb (15.2 kg)	2.25	2.36	3.10	4.00	3.96	5.63	4.08
	5-6	43.5 lb (19.7 kg)	2.96	2.99	3.81	5.06	4.67	6.49	4.71
	7-9	52.5 lb (23.8 kg)	3.63	3.76	3.73	6.17	5.59	8.03	5.73
60°	1-4	33.5 lb (15.2 kg)	2.63	2.77	3.49	4.58	4.34	6.21	4.48
	5-6	43.5 lb (19.7 kg)	3.46	3.51	4.32	5.84	5.19	7.01	5.22
	7-9	52.5 lb (23.8 kg)	4.27	4.44	5.25	7.15	6.23	8.80	6.40

Optical Distributions



Product Specifications

Construction

- Die-cast aluminum housing and heat sink
- Three housing sizes, using 1 to 9 light squares

Optics

- High-efficiency injection-molded AccuLED Optics technology
- 16 optical distributions for area site and roadway applications
- 3 shielding options include HSS, GRS and PFS
- IDA Certified (3000K CCT and warmer only, fixed mounting options)

Electrical

- Removable power tray assembly includes drivers, surge modules and control modules for ease of maintenance and serviceability
- Standard with 0-10V dimming
- Standard with 10kV surge module, optional 20kV surge module

- Suitable for operation in -40°C to 40°C ambient environments. Optional 50°C high ambient (HA) configuration

Mounting

- Arms are factory installed, enabling closed-housing installation
- All arms suitable for round or square pole installation
- All arms provide clearance for multiple fixture installations at 90°

Finish

- 6 standard finishes use super durable TGIC polyester powder coat paint, providing 2.5 mil nominal thickness and salt-spray tested to 3,000 hours per ASTM B117
- RAL and custom color matches available
- Coastal Construction (CC) option salt-spray tested to 5,000 hours per ASTM B117, achieving a scribe rating of 9 per ASTM D1654

Typical Applications

- Outdoor, Parking Lots, Walkways, Roadways, Building Areas

Warranty

- Five year limited warranty

Energy and Performance Data

Lumen Maintenance (TM-21)

Drive Current	Ambient Temperature	25,000 hours*	50,000 hours*	60,000 hours*	100,000 hours**	Theoretical L70 hours**
Up to 1A	25°C	99.4%	99.0%	98.9%	98.3%	> 2.4M
	40°C	98.7%	98.3%	98.1%	97.4%	> 1.9M
	50°C	98.2%	97.2%	96.8%	95.2%	> 851,000
1.2A	25°C	99.4%	99.0%	98.9%	98.3%	> 2.4M
	40°C	98.5%	97.9%	97.7%	96.7%	> 1.3M

Lumen Multiplier

Ambient Temperature	Lumen Multiplier
0°C	1.02
10°C	1.01
25°C	1.00
40°C	0.99
50°C	0.97

* Supported by IES TM-21 standards
 ** Theoretical values represent estimations commonly used; however, refer to the IES position on LED Product Lifetime Prediction, IES PS-10-18, explaining proper use of IES TM-21 and LM-80.

Energy and Performance Data

Lumen Maintenance (TM-21)

Drive Current	Ambient Temperature	25,000 hours*	50,000 hours*	60,000 hours*	100,000 hours**	Theoretical L70 hours**
Up to 1A	25°C	99.4%	99.0%	98.9%	98.3%	> 2.4M
	40°C	98.7%	98.3%	98.1%	97.4%	> 1.9M
	50°C	98.2%	97.2%	96.8%	95.2%	> 851,000
1.2A	25°C	99.4%	99.0%	98.9%	98.3%	> 2.4M
	40°C	98.5%	97.9%	97.7%	96.7%	> 1.3M

Lumen Multiplier

Ambient Temperature	Lumen Multiplier
0°C	1.02
10°C	1.01
25°C	1.00
40°C	0.99
50°C	0.97

* Supported by IES TM-21 standards

** Theoretical values represent estimations commonly used; however, refer to the IES position on LED Product Lifetime Prediction, IES PS-10-18, explaining proper use of IES TM-21 and LM-80.

FADC Settings

SA1-SA3 (A, B, C, D Drive Current)

FADC Position	Percent of Typical Lumen Output
1	25%
2	48%
3	56%
4	65%
5	75%
6	80%
7	85%
8	90%
9	95%
10	100%

FADC Settings

SA4-SA6 (A, B, C, D Drive Current)

FADC Position	Percent of Typical Lumen Output
1	14%
2	25%
3	32%
4	43%
5	49%
6	57%
7	65%
8	72%
9	80%
10	100%

FADC Settings

SA7-SA9 (A, B, C, D Drive Current)

FADC Position	Percent of Typical Lumen Output
1	19%
2	38%
3	47%
4	63%
5	74%
6	85%
7	95%
8	97%
9	100%
10	100%

Performance Table, Drive Current "A" (615mA)

Number of Light Squares		1	2	3	4	5	6	7	8	9
Nominal Power (Watts)		33	63	93	121	154	182	215	244	274
Input Current @ 120V		0.283	0.529	0.778	1.058	1.310	1.556	1.839	2.089	2.335
Input Current @ 208V		0.165	0.309	0.460	0.618	0.771	0.919	1.082	1.240	1.379
Input Current @ 240V		0.143	0.270	0.398	0.540	0.671	0.796	0.944	1.078	1.194
Input Current @ 277V		0.125	0.237	0.352	0.473	0.581	0.705	0.818	0.962	1.057
Input Current @ 347V		0.098	0.181	0.272	0.362	0.454	0.544	0.636	0.738	0.816
Input Current @ 480V		0.073	0.133	0.200	0.267	0.335	0.400	0.470	0.554	0.600
Optics										
T2	4000K Lumens	4,654	9,249	13,730	18,194	23,032	27,273	32,034	37,138	41,694
	BUG Rating	B1-U0-G1	B1-U0-G2	B2-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5
	Lumens per Watt	141	147	148	150	150	150	149	152	152
T2R	4000K Lumens	4,716	9,372	13,913	18,437	23,340	27,637	32,462	37,634	42,251
	BUG Rating	B1-U0-G1	B1-U0-G2	B2-U0-G2	B2-U0-G2	B3-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4
	Lumens per Watt	143	149	150	152	152	152	151	154	154
T3	4000K Lumens	4,589	9,120	13,538	17,940	22,711	26,892	31,587	36,620	41,112
	BUG Rating	B1-U0-G1	B2-U0-G2	B2-U0-G2	B3-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4	B4-U0-G4
	Lumens per Watt	139	145	146	148	147	148	147	150	150
T3R	4000K Lumens	4,735	9,411	13,970	18,513	23,436	27,751	32,596	37,790	42,425
	BUG Rating	B1-U0-G1	B1-U0-G2	B2-U0-G3	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5
	Lumens per Watt	143	149	150	153	152	152	152	155	155
T4FT	4000K Lumens	4,617	9,176	13,622	18,051	22,851	27,058	31,782	36,847	41,366
	BUG Rating	B1-U0-G2	B2-U0-G2	B2-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5
	Lumens per Watt	140	146	146	149	148	149	148	151	151
T4W	4000K Lumens	4,631	9,203	13,662	18,104	22,918	27,138	31,876	36,955	41,488
	BUG Rating	B1-U0-G2	B2-U0-G2	B2-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B4-U0-G5
	Lumens per Watt	140	146	147	150	149	149	148	151	151
SL2	4000K Lumens	4,619	9,180	13,627	18,058	22,860	27,069	31,795	36,861	41,383
	BUG Rating	B1-U0-G2	B2-U0-G2	B2-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4	B3-U0-G5	B4-U0-G5
	Lumens per Watt	140	146	147	149	148	149	148	151	151
SL3	4000K Lumens	4,586	9,115	13,531	17,931	22,699	26,879	31,571	36,602	41,091
	BUG Rating	B1-U0-G1	B1-U0-G2	B2-U0-G3	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5
	Lumens per Watt	139	145	145	148	147	148	147	150	150
SL4	4000K Lumens	4,529	9,002	13,363	17,708	22,417	26,544	31,178	36,146	40,580
	BUG Rating	B1-U0-G2	B1-U0-G3	B2-U0-G3	B2-U0-G4	B2-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5
	Lumens per Watt	137	143	144	146	146	146	145	148	148
5NQ	4000K Lumens	4,829	9,598	14,247	18,880	23,901	28,301	33,242	38,539	43,266
	BUG Rating	B2-U0-G1	B3-U0-G1	B3-U0-G2	B4-U0-G2	B4-U0-G2	B4-U0-G2	B5-U0-G2	B5-U0-G3	B5-U0-G3
	Lumens per Watt	146	152	153	156	155	155	155	158	158
5MQ	4000K Lumens	4,853	9,645	14,318	18,974	24,020	28,442	33,407	38,731	43,482
	BUG Rating	B3-U0-G1	B3-U0-G2	B4-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G4
	Lumens per Watt	147	153	154	157	156	156	155	159	159
5WQ	4000K Lumens	4,843	9,625	14,288	18,934	23,969	28,382	33,337	38,649	43,390
	BUG Rating	B3-U0-G1	B4-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G4	B5-U0-G5
	Lumens per Watt	147	153	154	156	156	156	155	158	158
SLL/SLR	4000K Lumens	3,989	7,927	11,768	15,594	19,741	23,375	27,456	31,831	35,736
	BUG Rating	B1-U0-G2	B1-U0-G2	B2-U0-G3	B2-U0-G3	B2-U0-G4	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5
	Lumens per Watt	121	126	127	129	128	128	128	130	130
RW	4000K Lumens	4,774	9,488	14,085	18,665	23,628	27,979	32,863	38,100	42,774
	BUG Rating	B2-U0-G1	B3-U0-G1	B3-U0-G2	B4-U0-G2	B4-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G3
	Lumens per Watt	145	151	151	154	153	154	153	156	156
AFL	4000K Lumens	4,673	9,286	13,785	18,268	23,126	27,384	32,164	37,290	41,864
	BUG Rating	B1-U0-G1	B1-U0-G1	B2-U0-G2	B2-U0-G2	B3-U0-G2	B3-U0-G2	B3-U0-G3	B3-U0-G3	B3-U0-G3
	Lumens per Watt	142	147	148	151	150	150	150	153	153

* Nominal data for 70 CRI. ** For additional performance data, please reference the Galleon Supplemental Performance Guide.

Performance Table, Drive Current "B" (800mA)

Number of Light Squares		1	2	3	4	5	6	7	8	9
Nominal Power (Watts)		44	82	121	164	204	243	286	325	364
Input Current @ 120V		0.367	0.689	1.014	1.378	1.704	2.027	2.393	2.716	3.041
Input Current @ 208V		0.213	0.401	0.594	0.802	0.997	1.188	1.400	1.605	1.782
Input Current @ 240V		0.184	0.347	0.510	0.694	0.860	1.021	1.210	1.386	1.531
Input Current @ 277V		0.160	0.303	0.449	0.605	0.757	0.898	1.065	1.242	1.347
Input Current @ 347V		0.125	0.235	0.355	0.471	0.592	0.710	0.828	0.958	1.065
Input Current @ 480V		0.092	0.172	0.258	0.344	0.432	0.517	0.605	0.706	0.775
Optics										
T2	4000K Lumens	5,790	11,508	17,083	22,638	28,658	33,935	39,859	46,210	51,879
	BUG Rating	B1-U0-G2	B2-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B4-U0-G5
	Lumens per Watt	132	140	141	138	140	140	139	142	143
T2R	4000K Lumens	5,868	11,662	17,311	22,941	29,041	34,388	40,391	46,827	52,572
	BUG Rating	B1-U0-G1	B2-U0-G2	B2-U0-G2	B3-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4	B3-U0-G5
	Lumens per Watt	133	142	143	140	142	142	141	144	144
T3	4000K Lumens	5,710	11,347	16,845	22,322	28,258	33,461	39,303	45,565	51,155
	BUG Rating	B1-U0-G1	B2-U0-G2	B3-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B4-U0-G4	B4-U0-G5	B4-U0-G5
	Lumens per Watt	130	138	139	136	139	138	137	140	141
T3R	4000K Lumens	5,892	11,710	17,383	23,035	29,161	34,530	40,558	47,020	52,788
	BUG Rating	B1-U0-G2	B2-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B4-U0-G5
	Lumens per Watt	134	143	144	140	143	142	142	145	145
T4FT	4000K Lumens	5,745	11,418	16,949	22,460	28,433	33,668	39,546	45,847	51,471
	BUG Rating	B1-U0-G2	B2-U0-G3	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	131	139	140	137	139	139	138	141	141
T4W	4000K Lumens	5,762	11,451	16,999	22,526	28,517	33,767	39,662	45,982	51,622
	BUG Rating	B1-U0-G2	B2-U0-G2	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	131	140	140	137	140	139	139	141	142
SL2	4000K Lumens	5,747	11,422	16,956	22,469	28,444	33,681	39,561	45,865	51,491
	BUG Rating	B1-U0-G2	B2-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	131	139	140	137	139	139	138	141	141
SL3	4000K Lumens	5,707	11,342	16,836	22,311	28,244	33,444	39,283	45,542	51,129
	BUG Rating	B1-U0-G2	B2-U0-G3	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5
	Lumens per Watt	130	138	139	136	138	138	137	140	140
SL4	4000K Lumens	5,636	11,201	16,627	22,034	27,893	33,028	38,794	44,976	50,493
	BUG Rating	B1-U0-G2	B1-U0-G3	B2-U0-G4	B2-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5
	Lumens per Watt	128	137	137	134	137	136	136	138	139
5NQ	4000K Lumens	6,009	11,942	17,727	23,492	29,739	35,214	41,362	47,953	53,835
	BUG Rating	B2-U0-G1	B3-U0-G1	B4-U0-G2	B4-U0-G2	B5-U0-G2	B5-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G3
	Lumens per Watt	137	146	147	143	146	145	145	148	148
5MQ	4000K Lumens	6,039	12,001	17,816	23,609	29,887	35,389	41,568	48,191	54,103
	BUG Rating	B3-U0-G1	B4-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G4	B5-U0-G5
	Lumens per Watt	137	146	147	144	147	146	145	148	149
5WQ	4000K Lumens	6,026	11,976	17,778	23,559	29,824	35,315	41,480	48,090	53,989
	BUG Rating	B3-U0-G1	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G4	B5-U0-G5	B5-U0-G5
	Lumens per Watt	137	146	147	144	146	145	145	148	148
SLL/SLR	4000K Lumens	4,963	9,863	14,642	19,403	24,563	29,085	34,163	39,607	44,465
	BUG Rating	B1-U0-G2	B1-U0-G3	B2-U0-G3	B2-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5
	Lumens per Watt	113	120	121	118	120	120	119	122	122
RW	4000K Lumens	5,940	11,806	17,526	23,224	29,400	34,813	40,891	47,407	53,222
	BUG Rating	B3-U0-G1	B3-U0-G2	B4-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4
	Lumens per Watt	135	144	145	142	144	143	143	146	146
AFL	4000K Lumens	5,814	11,555	17,153	22,730	28,775	34,073	40,021	46,398	52,090
	BUG Rating	B1-U0-G1	B2-U0-G1	B2-U0-G2	B2-U0-G2	B3-U0-G3	B3-U0-G3	B3-U0-G3	B3-U0-G3	B3-U0-G4
	Lumens per Watt	132	141	142	139	141	140	140	143	143

* Nominal data for 70 CRI. ** For additional performance data, please reference the Galleon Supplemental Performance Guide.

Performance Table, Drive Current "C" (1050mA)

Number of Light Squares	1	2	3	4	5	6	7	8	9	
Nominal Power (Watts)	57	108	160	213	269	321	377	429	481	
Input Current @ 120V	0.478	0.905	1.338	1.810	2.244	2.675	3.150	3.584	4.013	
Input Current @ 208V	0.279	0.532	0.780	1.064	1.313	1.559	1.845	2.093	2.339	
Input Current @ 240V	0.243	0.458	0.664	0.916	1.123	1.328	1.582	1.788	1.991	
Input Current @ 277V	0.213	0.404	0.582	0.808	0.997	1.164	1.401	1.589	1.745	
Input Current @ 347V	0.164	0.322	0.471	0.644	0.795	0.943	1.117	1.269	1.414	
Input Current @ 480V	0.121	0.235	0.341	0.469	0.579	0.681	0.814	0.923	1.022	
Optics										
T2	4000K Lumens	7,154	14,219	21,107	27,970	35,408	41,927	49,247	57,094	64,098
	BUG Rating	B1-U0-G2	B2-U0-G2	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	126	132	132	131	132	131	131	133	133
T2R	4000K Lumens	7,250	14,408	21,389	28,344	35,881	42,487	49,905	57,857	64,954
	BUG Rating	B1-U0-G1	B2-U0-G2	B2-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	127	133	134	133	133	132	132	135	135
T3	4000K Lumens	7,054	14,020	20,812	27,580	34,914	41,342	48,560	56,297	63,203
	BUG Rating	B1-U0-G2	B2-U0-G2	B3-U0-G3	B3-U0-G4	B3-U0-G4	B4-U0-G4	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	124	130	130	129	130	129	129	131	131
T3R	4000K Lumens	7,280	14,468	21,477	28,461	36,029	42,663	50,111	58,096	65,222
	BUG Rating	B1-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	128	134	134	134	134	133	133	135	136
T4FT	4000K Lumens	7,098	14,107	20,941	27,751	35,130	41,598	48,860	56,646	63,594
	BUG Rating	B1-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	125	131	131	130	131	130	130	132	132
T4W	4000K Lumens	7,119	14,148	21,003	27,832	35,233	41,720	49,004	56,812	63,781
	BUG Rating	B1-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	125	131	131	131	131	130	130	132	133
SL2	4000K Lumens	7,101	14,112	20,949	27,761	35,144	41,614	48,879	56,668	63,619
	BUG Rating	B1-U0-G2	B2-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	125	131	131	130	131	130	130	132	132
SL3	4000K Lumens	7,051	14,013	20,802	27,566	34,897	41,321	48,535	56,269	63,172
	BUG Rating	B1-U0-G2	B2-U0-G3	B2-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	124	130	130	129	130	129	129	131	131
SL4	4000K Lumens	6,963	13,839	20,543	27,223	34,463	40,808	47,932	55,569	62,386
	BUG Rating	B1-U0-G2	B2-U0-G3	B2-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5
	Lumens per Watt	122	128	128	128	128	127	127	130	130
5NQ	4000K Lumens	7,424	14,755	21,903	29,025	36,743	43,508	51,104	59,247	66,515
	BUG Rating	B3-U0-G1	B3-U0-G2	B4-U0-G2	B5-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4
	Lumens per Watt	130	137	137	136	137	136	136	138	138
5MQ	4000K Lumens	7,461	14,828	22,012	29,169	36,926	43,725	51,359	59,542	66,846
	BUG Rating	B3-U0-G1	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G4	B5-U0-G5	B5-U0-G5
	Lumens per Watt	131	137	138	137	137	136	136	139	139
5WQ	4000K Lumens	7,445	14,797	21,966	29,108	36,849	43,633	51,250	59,417	66,705
	BUG Rating	B3-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G5	B5-U0-G5	B5-U0-G5	B5-U0-G5
	Lumens per Watt	131	137	137	137	137	136	136	139	139
SLL/ SLR	4000K Lumens	6,132	12,187	18,091	23,973	30,348	35,936	42,210	48,935	54,938
	BUG Rating	B1-U0-G2	B2-U0-G3	B2-U0-G3	B3-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5
	Lumens per Watt	108	113	113	113	113	112	112	114	114
RW	4000K Lumens	7,340	14,587	21,653	28,694	36,325	43,013	50,522	58,573	65,757
	BUG Rating	B3-U0-G1	B3-U0-G2	B4-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G4
	Lumens per Watt	129	135	135	135	135	134	134	137	137
AFL	4000K Lumens	7,183	14,276	21,193	28,084	35,552	42,098	49,448	57,327	64,359
	BUG Rating	B1-U0-G1	B2-U0-G2	B2-U0-G2	B3-U0-G2	B3-U0-G3	B3-U0-G3	B3-U0-G4	B3-U0-G4	B4-U0-G4
	Lumens per Watt	126	132	132	132	132	131	131	134	134

* Nominal data for 70 CRI. ** For additional performance data, please reference the Galleon Supplemental Performance Guide.

Performance Table, Drive Current "D" (1200mA)

Number of Light Squares	1	2	3	4	5	6	7	8	9	
Nominal Power (Watts)	65	125	184	245	309	368	433	493	552	
Input Current @ 120V	0.546	1.041	1.535	2.082	2.578	3.070	3.619	4.114	4.605	
Input Current @ 208V	0.318	0.610	0.893	1.219	1.504	1.786	2.113	2.397	2.679	
Input Current @ 240V	0.276	0.523	0.758	1.046	1.282	1.516	1.806	2.041	2.274	
Input Current @ 277V	0.241	0.460	0.662	0.920	1.133	1.325	1.593	1.807	1.987	
Input Current @ 347V	0.187	0.370	0.543	0.740	0.915	1.085	1.285	1.459	1.628	
Input Current @ 480V	0.138	0.269	0.391	0.537	0.663	0.782	0.932	1.057	1.173	
Optics										
T2	4000K Lumens	7,872	15,645	23,225	30,777	38,962	46,135	54,189	62,024	70,530
	BUG Rating	B1-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	121	125	126	126	126	125	125	127	128
T2R	4000K Lumens	7,977	15,854	23,535	31,188	39,482	46,751	54,913	63,663	71,472
	BUG Rating	B1-U0-G2	B2-U0-G2	B3-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G4	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	123	127	128	127	128	127	127	129	129
T3	4000K Lumens	7,762	15,427	22,901	30,348	38,418	45,491	53,433	61,947	69,546
	BUG Rating	B2-U0-G2	B3-U0-G3	B3-U0-G3	B3-U0-G4	B4-U0-G4	B4-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	119	123	124	124	124	124	123	126	126
T3R	4000K Lumens	8,010	15,920	23,632	31,317	39,645	46,944	55,139	63,925	71,767
	BUG Rating	B1-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	123	127	128	128	128	128	127	130	130
T4FT	4000K Lumens	7,810	15,522	23,043	30,535	38,655	45,772	53,763	62,330	69,976
	BUG Rating	B1-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	120	124	125	125	125	124	124	126	127
T4W	4000K Lumens	7,833	15,568	23,110	30,625	38,769	45,907	53,921	62,513	70,182
	BUG Rating	B2-U0-G2	B3-U0-G3	B3-U0-G4	B3-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	121	125	126	125	125	125	125	127	127
SL2	4000K Lumens	7,813	15,528	23,052	30,547	38,670	45,790	53,784	62,354	70,003
	BUG Rating	B2-U0-G2	B3-U0-G3	B3-U0-G4	B3-U0-G4	B4-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	120	124	125	125	125	124	124	126	127
SL3	4000K Lumens	7,758	15,419	22,889	30,332	38,398	45,468	53,406	61,916	69,511
	BUG Rating	B1-U0-G2	B2-U0-G3	B3-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B4-U0-G5	B4-U0-G5
	Lumens per Watt	119	123	124	124	124	124	123	126	126
SL4	4000K Lumens	7,662	15,228	22,605	29,955	37,921	44,903	52,742	61,146	68,646
	BUG Rating	B1-U0-G3	B2-U0-G3	B2-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5	B4-U0-G5
	Lumens per Watt	118	122	123	122	123	122	122	124	124
5NQ	4000K Lumens	8,169	16,235	24,101	31,938	40,431	47,874	56,232	65,193	73,190
	BUG Rating	B3-U0-G1	B3-U0-G2	B4-U0-G2	B5-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G4
	Lumens per Watt	126	130	131	130	131	130	130	132	133
5MQ	4000K Lumens	8,210	16,316	24,221	32,097	40,632	48,113	56,512	65,517	73,554
	BUG Rating	B3-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G5	B5-U0-G5	B5-U0-G5
	Lumens per Watt	126	131	132	131	131	131	131	133	133
5WQ	4000K Lumens	8,192	16,282	24,170	32,029	40,546	48,011	56,393	65,379	73,399
	BUG Rating	B3-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G5	B5-U0-G5	B5-U0-G5	B5-U0-G5
	Lumens per Watt	126	130	131	131	131	130	130	133	133
SLL/SLR	4000K Lumens	6,747	13,410	19,906	26,379	33,394	39,542	46,445	53,846	60,451
	BUG Rating	B1-U0-G2	B2-U0-G3	B2-U0-G4	B3-U0-G4	B3-U0-G5	B3-U0-G5	B3-U0-G5	B3-U0-G5	B4-U0-G5
	Lumens per Watt	104	107	108	108	108	107	107	109	110
RW	4000K Lumens	8,076	16,050	23,826	31,574	39,970	47,329	55,592	64,450	72,356
	BUG Rating	B3-U0-G1	B4-U0-G2	B4-U0-G2	B5-U0-G3	B5-U0-G3	B5-U0-G4	B5-U0-G4	B5-U0-G4	B5-U0-G5
	Lumens per Watt	124	128	129	129	129	129	128	131	131
AFL	4000K Lumens	7,904	15,709	23,320	30,902	39,120	46,323	54,410	63,079	70,817
	BUG Rating	B1-U0-G1	B2-U0-G2	B3-U0-G2	B3-U0-G3	B3-U0-G3	B3-U0-G3	B3-U0-G4	B4-U0-G4	B4-U0-G4
	Lumens per Watt	122	126	127	126	127	126	126	128	128

* Nominal data for 70 CRI. ** For additional performance data, please reference the Galleon Supplemental Performance Guide.

Control Options

0-10V (DIM)

This fixture is offered standard with 0-10V dimming driver(s). The DIM option provides 0-10V dimming wire leads for use with a lighting control panel or other control method.

Photocontrol (BPC, PR and PR7)

Optional button-type photocontrol (BPC) and photocontrol receptacles (PR and PR7) provide a flexible solution to enable "dusk-to-dawn" lighting by sensing light levels. Advanced control systems compatible with NEMA 7-pin standards can be utilized with the PR7 receptacle.

After Hours Dim (AHD)

This feature allows photocontrol-enabled luminaires to achieve additional energy savings by dimming during scheduled portions of the night. The dimming profile will automatically take effect after a "dusk-to-dawn" period has been calculated from the photocontrol input. Specify the desired dimming profile for a simple, factory-shipped dimming solution requiring no external control wiring. Reference the After Hours Dim supplemental guide for additional information.

Dimming Occupancy Sensor (SPB)

These passive infrared (PIR) sensors are factory installed in the luminaire housing. When the SPB sensor option is selected, the occupancy sensor is connected to a dimming driver and the entire luminaire dims when no motion is detected. After a period of time, the luminaire turns off, and when motion is detected, the luminaire returns to full light output. The SPB sensor default parameters are listed in the table below, and can be configured utilizing the Sensor Configuration mobile application for iOS and Android devices. The SPB/X is configured to control only the specified number of light squares. An integral photocontrol can be activated with the app for "dusk-to-dawn" control or daylight harvesting - the factory default is off. Three sensor lenses are available to optimize the coverage pattern for mounting heights from 8'-40'. Four sensor colors are available; Bronze, Black, Gray and White, and are automatically selected based on the luminaire finish as indicated by the table below.

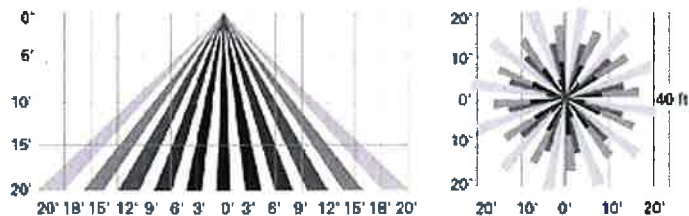
SPB sensor finish matched to luminaire finish		
Luminaire Finish		SPB Sensor Finish
WH	White	White
BK	Black	Black
GM	Graphite Metallic	Black
BZ	Bronze	Bronze
AP	Gray	Gray
DP	Dark Platinum	Gray

SPB/X Availability Table	
Fixture Square Count	Available SPB/X Square Count
1	Not Available
2	Not Available
3	Not Available
4	2
5	2 or 3
6	3
7	2, 3, 4 or 5
8	2, 3, 5 or 6
9	3 or 6

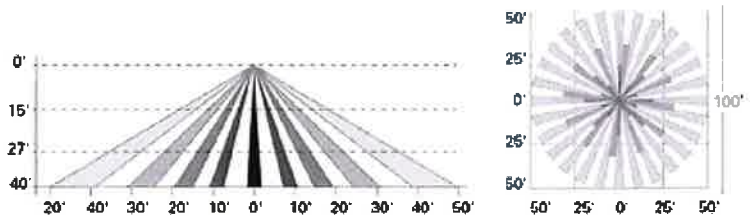
WaveLinx Wireless Control and Monitoring System

Operates on a wireless mesh network based on IEEE 802.15.4 standards enabling wireless control of outdoor lighting. WaveLinx and WaveLinx Lite sensors utilize the Zhaga Book 18 compliant 4-PIN receptacle (ZD or ZW), while the WOLC control module utilizes a 7-PIN receptacle. ZW option provides 4-PIN receptacle and control module to enable future installation of WaveLinx sensors. ZD option provides 4-PIN receptacle and sensor-ready (SR) driver to enable future installation of WaveLinx sensors, power monitoring, and advanced functionality. WaveLinx (SWPD4 to SWPD5) outdoor wireless sensors offer passive infrared (PIR) occupancy and photocell for closed loop daylight harvesting, and can be factory or field-installed. Sensors are factory preset to dim down to 50% after 15 minutes of no motion detected. Two lens options are available for mounting heights of 7' to 40'. Use the WaveLinx mobile application for set-up and configuration. At least one Wireless Area Controller (WAC) is required for full functionality and remote communication (including adjustment of any factory pre-sets). WaveLinx Lite (WOF and WOB) outdoor wireless sensors provide PIR occupancy and photocell for closed loop daylight harvesting, and can be factory or field-installed. Sensors are factory preset to dim down to 50% after 15 minutes of no motion detected. Two lens options are available for mounting heights of 7' to 40'. Use the WaveLinx Lite mobile application for set-up and configuration. WAC not required. WaveLinx Outdoor Control Module (WOLC-7P-10A) accessory provides a photocontrol enabling astronomic or time-based schedules to provide ON, OFF and dimming control of fixtures utilizing a 7-PIN receptacle. The out-of-box functionality is ON at dusk and OFF at dawn.

For mounting heights up to 15' (SWPD4 and WOB)



For mounting heights up to 40' (SWPD5 and WOF)



LumenSafe Integrated Network Security Camera (LD)

Cooper Lighting Solutions brings ease of camera deployment to a whole new level. No additional wiring is needed beyond providing line power to the luminaire. A variety of networking options allows security integrators to design the optimal solution for active surveillance. As the ideal solution to meet the needs for active surveillance, the LumenSafe integrated network camera is a streamlined, outdoor-ready fixed dome that provides HDTV 1080p video. This IP camera is optimally designed for deployment in the video management system or security software platform of choice.

Synapse (DIM10)

SimplySNAP integrated wireless controls system by Synapse. Includes factory installed DIM10 Synapse control module and FSP-201 motion sensor; requires additional Synapse system components for operation. Contact Synapse at www.synapsewireless.com for product support, warranty and terms and conditions.

FEB 1 2 23 PM '88

CROSS PARKING AGREEMENT

James H. Johnson

This agreement made and entered into this 24 day of ~~December~~, 1987, by and between MAPLE HILL ASSOCIATES, a Michigan co-partnership, consisting of STANLEY G. BOSKER and JOHN L. FEIRER, hereinafter referred to as "Maple" and MYERS, BURKHART AND BOSKER, a Michigan co-partnership consisting of WILLIAM D. MYERS, STANLEY G. BOSKER and WILLIAM K. BURKHART, hereinafter referred to as "Myers":

WHEREAS, Maple is the owner of all that certain piece or parcel situate and being in the Township of Oshtemo, County of Kalamazoo, State of Michigan and more particularly described as follows:

Lots 4, 5 and 6 and the South 50 feet of Lot 3 of Elks Plat No. 1 as recorded in Kalamazoo County Records

hereinafter referred to as "Maple Parcel"; and

WHEREAS, Myers is the owner of the north 250 feet of Lot 3 of Elks Plat No. 1 as recorded in Kalamazoo County Records hereinafter referred to as "Myers Parcel"; and

WHEREAS, the parties hereto have entered into certain oral agreements relating to parking and maintenance; which they desire to set forth in writing herein; and

NOW, THEREFORE, for and in consideration of mutual promises and agreements herein contained, the receipt and sufficiency of which are hereby acknowledged, it is expressly agreed by and between the parties hereto as follow:

1. Maple hereby expressly grants to Myers a perpetual non-exclusive easement to the free use of the parking areas, driveways, sidewalks, walkways on the South 50 feet of Lot 3 of Elks Plat No. 1 which is a portion of the Maple site for the purposes for which they are provided.

2. That Myers may grant to its respective tenants, licensees, and their respective officers, employees, agents, customers and invitees the right to use the easements granted in Paragraph 1 above if such grantee as listed herein so elects. If Myers makes such an election, it must serve written notice on

share such easement with its respective tenants, licensees, officers, employees, agents, customers and invitees. If Myers does not elect to share this easement with its tenants, licensees and their respective officers, employees, agents, customers and invitees they shall have no right to use same.

3. That both Maple and Myers agree not to erect any barrier along the border between their properties which would prohibit free access, ingress and egress between the Maple Parcel and the Myers Parcel.

4. That Maple and Myers agree to maintain and keep in a state of good repair any and all parking areas and/or sidewalks and driveways on its parcel.

5. That the easements granted herein are declared to run with the land and inure to the benefit of the parties hereto, and their grantees, heirs, successors and assigns.

6. That the terms of the easements granted herein shall be deemed to be perpetual.

IN WITNESS WHEREOF the parties hereto have set their hands and seals as of the day and year first above written.

WITNESSES:

Penny J. Hoyek
Penny J. Hoyek
Debra Lyn DeMink
Debra Lyn DeMink

WITNESSES:

Feggy M. Burkhart
Feggy M. Burkhart
Rosemary Stevens
Rosemary Stevens

"MAPLE"
MAPLE HILL ASSOCIATES

By Stanley G. Bosker
Stanley G. Bosker
Its partner

By John L. Feirer
John L. Feirer
Its partner

"MYERS"
MYERS, BURKHART & BOSKER

By William D. Myers
William D. Myers
Its Partner

By Stanley G. Bosker
Stanley G. Bosker
Its Partner

By William R. Burkhart
William R. Burkhart
Its Partner

STATE OF MICHIGAN)
) ss.
COUNTY OF KALAMAZOO

The foregoing instrument was acknowledged before me this
24th day of December, 1987 by MAPLE HILL ASSOCIATES
("Maple")

Nancy K. Brewer
Nancy K. Brewer
Notary Public, KALAMAZOO County,
Michigan
My commission expires: 11-16-88

STATE OF MICHIGAN)
) ss.
COUNTY OF KALAMAZOO

The foregoing instrument was acknowledged before me this
24th day of December, 1987 by MYERS, BURKHART AND BOSKER

Nancy K. Brewer
Nancy K. Brewer
Notary Public, KALAMAZOO County,
Michigan
My commission expires: 11-16-88

DRAFTED BY:

Robert R. Lennon
EARLY, LENNON, PETERS & CROCKER
900 Comerica Building
Kalamazoo, MI 49007
(616) 349-8765

[This page left intentionally blank]



December 8, 2023

Mtg Date: December 14, 2023
To: Planning Commission
From: Iris Lubbert, AICP, Planning Director
Subject: Public Hearing: 57.80 Keeping of Livestock and Honey Bees

Objective:

Consideration to adopt amendments to 57.80 - Keeping of Livestock and Honey Bees for recommendation to the Township Board.

Background:

While responding to an inquiry about the number and type of livestock permitted on a property, Staff became aware that the relevant Ordinance section was not clear and open to interpretation. To clarify and ensure consistent interpretation, the Planning Department and Legal Department are recommending Section 57.80 be amended.

Attachments: Proposed amendments to Section 57.80 (proposed changes are in red)

57.80 KEEPING OF LIVESTOCK AND HONEY BEES

The keeping of poultry, swine, horses, rabbits, sheep, goats, other similar livestock (collectively referred herein as animals), and honey bees for noncommercial purposes is permitted under the following conditions:

- A. Activities shall be for the private enjoyment of the property owners conducted for noncommercial purposes.
- B. Acreage requirements. Acreage requirements are cumulative for exclusive of each animal size, excluding honey bees, meaning the property must have the minimum required acreage for each animal s onsite.
 1. Large animals. One acre for each large animal (more than 200 pounds at maturity), including horses, cattle, and swine, or similar animals.
 2. Moderate-sized animals. One-quarter acre for each moderate-sized animal (35 to 200 pounds at maturity), including sheep, goats, or similar animals.
 3. Small animals. All parcels, lots, and building sites up to one acre shall be permitted up to five small animals (under 35 pounds at maturity), including chickens, ducks, turkeys, rabbits, or similar animals. One additional small animal shall be permitted for each additional one-quarter acre of property.
 4. Honey bees. The maximum allowed number of colonies permitted on a lot, parcel, or building site shall be limited as follows:

Lot, Parcel, or Building site Acreage	Maximum number of colonies permitted
Up to ¼ acre	2
¼ acre to ½ acre	4
½ to 1 acre	6

5. For a Lot, Parcel, or Building Site over an acre, 8 colonies are allowed plus 1 additional colony for each additional one-quarter acre.

...



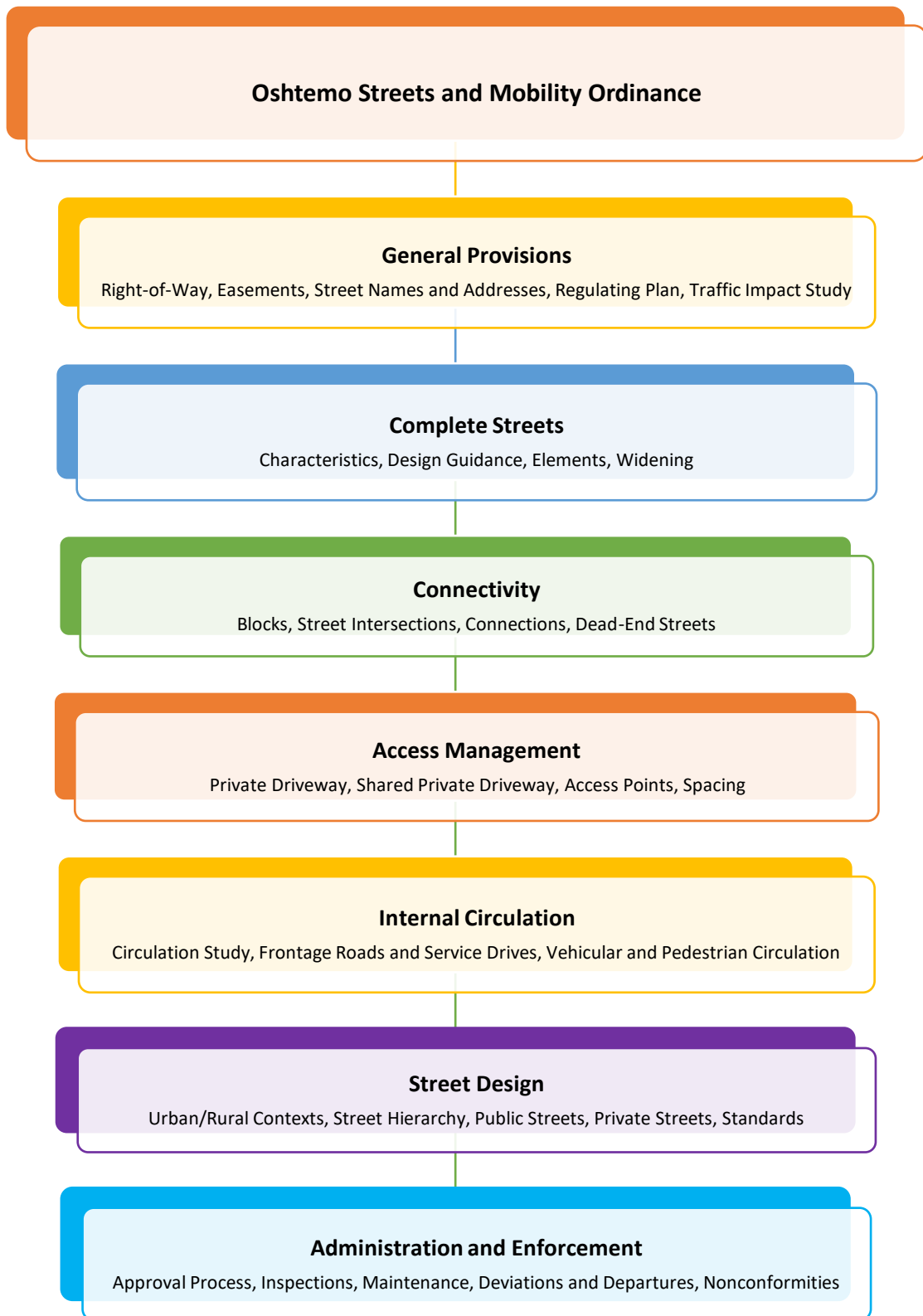
December 8, 2023

Mtg Date: December 14, 2023
To: Planning Commission
From: Iris Lubbert, AICP, Planning Director
Subject: Work Session – Private Street Ordinance

Suzanne Schulz, a representative from Progressive AE, will present the attached updated draft of the Private Street Ordinance, now the Oshtemo Streets and Mobility Ordinance, to the Planning Commission at their regular December 14th meeting.

The Planning Commission is asked to provide feedback on the ordinance and if confident, with any modifications discussed at the meeting and a final review by staff and the RCKC, provide a recommendation to forward the ordinance to the Township Board for consideration. It should be noted that the Oshtemo Streets and Mobility Ordinance will be under the General Ordinance, not the Zoning Ordinance, so a public hearing with the Planning Commission is not required.

Attachment: DRAFT Oshtemo Streets and Mobility Ordinance



OSHTEMO CHARTER TOWNSHIP STREETS AND MOBILITY ORDINANCE

Contents

ARTICLE 1 – TITLE, PURPOSE, AND SCOPE	5
Section 1.01 Title.	5
Section 1.02. Authority.....	5
Section 1.03. Purpose and Intent.	5
Section 1.04. Effective Date.....	6
Section 1.05. Applicability.	6
Section 1.06. Relationship to Adopted Plans.....	6
Section 1.07. Relationship to Other Laws and Agreements.....	6
ARTICLE 2 - DEFINITIONS	8
Section 2.01 Rules of Construction and Organization.	8
Section 2.02. Definitions.	8
Section 2.03. Measurements.	12
ARTICLE 3 - GENERAL PROVISIONS	13
Section 3.01. Applicability.	13
Section 3.02. Right-of-Way.	13
Section 3.03. Easements.	13
Section 3.04. Street Names.	13
Section 3.05. Street Addresses.....	14
Section 3.06. Street Signs.....	14
Section 3.07. Clear Vision Area.	14
Section 3.08. Regulating Plan.	15
Section 3.09. Traffic Impact Study.	16
Section 3.10. Infrastructure and Service Needs.	18
ARTICLE 4 – COMPLETE STREETS	20
Section 4.01. Purpose and Intent.	20
Section 4.02. Applicability.	20
Section 4.03. Characteristics.	21
Section 4.04. Design Guidance.	21
Section 4.05. Elements in Regulating Plan.	21

Section 4.06. Reserved.....	22
Section 4.07. Widening.....	22
ARTICLE 5 – CONNECTIVITY.....	23
Section 5.01. Purpose and Intent.....	23
Section 5.02. Applicability.....	23
Section 5.03. Blocks.....	23
Section 5.04. Street Intersections.....	24
Section 5.05. Connections.....	24
Section 5.06. Dead-End Streets.....	24
ARTICLE 6 – ACCESS MANAGEMENT.....	26
Section 6.01. Purpose and Intent.....	26
Section 6.02. General.....	26
Section 6.03. Private Driveway.....	26
Section 6.04. Shared Private Driveway.....	27
Section 6.06. Access Points.....	27
Section 6.07. Spacing.....	28
Section 6.08. Emergency Access.....	29
ARTICLE 7 – INTERNAL CIRCULATION.....	30
Section 7.01. Purpose and Intent.....	30
Section 7.02. Applicability.....	30
Section 7.03. Circulation Study.....	30
Section 7.04. Frontage Roads and Service Drives.....	31
Section 7.05. Vehicular Circulation.....	32
Section 7.06. Pedestrian Circulation.....	33
Section 7.07. Transit Access and Amenities.....	34
Section 7.08. Bicycle Facilities.....	34
ARTICLE 8 – STREET DESIGN.....	36
Section 8.01. Purpose and Intent.....	36
Section 8.02. Context Zones.....	36
Section 8.03 Types of Streets.....	37
Section 8.04. Reserved.....	37
Section 8.05. Public Streets.....	37
Section 8.06. Private Streets.....	39

Section 8.07. Private Streets Standards..... 40

Section 8.08 Grading..... 41

Section 8.09. Green Infrastructure..... 42

Section 8.10. Sidewalks..... 44

ARTICLE 9 – ADMINISTRATION AND ENFORCEMENT FOR PRIVATE STREETS..... 46

Section 9.01. Administration and Interpretation..... 46

Section 9.02. Fees..... 46

Section 9.03. Approval Process..... 46

Section 9.04. Inspections..... 49

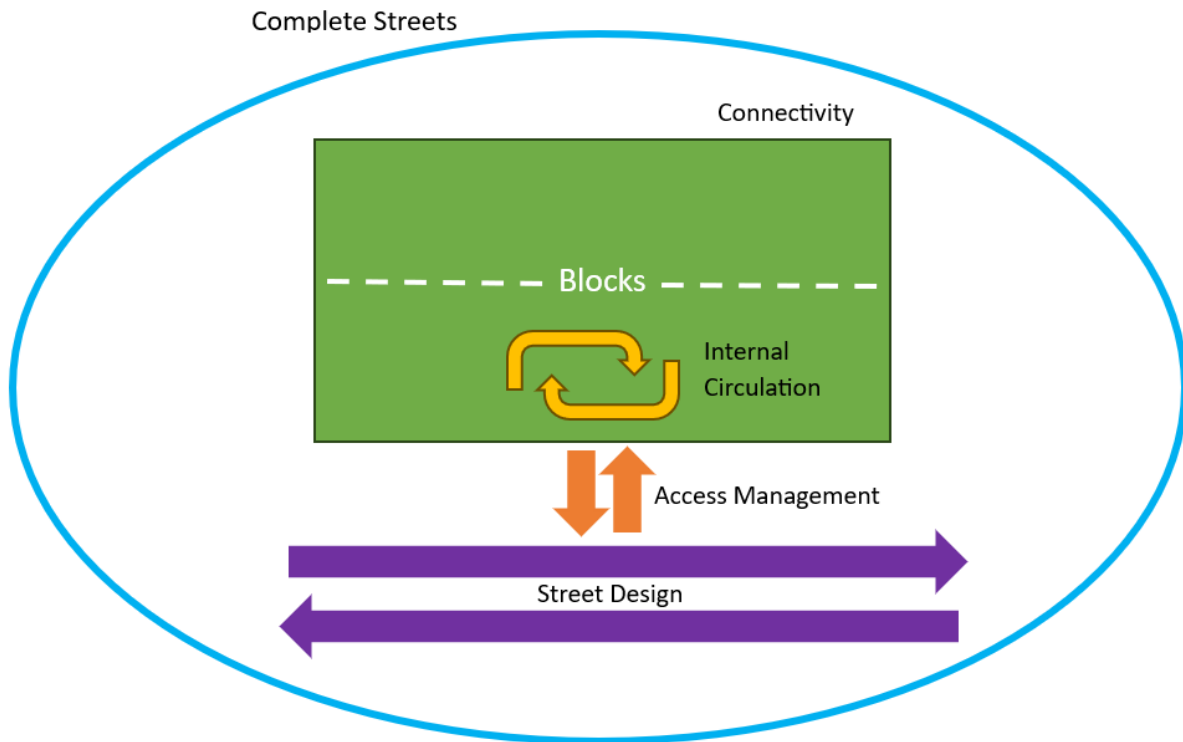
Section 9.05. Maintenance..... 49

Section 9.06. Deviations..... 50

Section 9.07. Administrative Departures..... 51

Section 9.08. Nonconforming Situations..... 52

Section 9.09. Violation and penalties..... 52



ARTICLE 1 – TITLE, PURPOSE, AND SCOPE

Section 1.01. Title.

This Ordinance shall be known as the "Transportation Ordinance" of Oshtemo Charter Township, Michigan.

Section 1.02. Authority.

This Ordinance is enacted pursuant to the authority granted by the Michigan Zoning Enabling Act 110 of the Public Acts of 2006, as amended.

Section 1.03. Purpose and Intent.

- A. In addition to promoting the purposes provided in the Zoning Enabling Act, this Ordinance is necessary to promote the public health, safety, and general welfare of Oshtemo Township as well as to implement the themes, policies and goals contained in officially adopted plans, including the Oshtemo Township Master Plan, Go! Green Oshtemo Plan, Access Management Plan, and Complete Streets Policy.
- B. Oshtemo Township will seek to enhance the safety, access, convenience, and comfort for all users of all ages and abilities through the planning, design, operation, enforcement, and maintenance of the transportation network. To that end, the Intent of this Ordinance is to:
 1. Expand an interconnected network of facilities accommodating each mode of travel consistent with the goals identified in the adopted Comprehensive Master Plan and other related planning documents.
 2. Eliminate traffic-related serious injuries and fatalities in keeping with Vision Zero for all people in our community to advance the public's interest in its health, safety, and welfare.
 3. Provide transportation options that are available, affordable, and reliable for all people to meet their daily travel needs regardless of age, ability, race, ethnicity, or economic status.
 4. Recognize that all streets are not the same: streets vary by type and intensity of adjacent land uses (context), number of travel lanes, posted speed limits, traffic volumes, available right-of-way, and other characteristics such as topography, soil types, and vegetation.
 5. Respect the fabric of the community by acknowledging and designing a transportation system best suited for urban and rural contexts, and the residential and commercial nature of uses, which co-exist with streets rights-of-way and street operations.
 6. Improve bicycle and pedestrian movement to reduce reliance on personal vehicle use, provide for improved circulation between and within development sites, create a transit-supportive environment, increase modal choice, and promote transportation options to reduce Vehicle Miles Traveled (VMT) and related vehicle emissions.
 7. Acknowledge the significant cost of "grey" infrastructure (streets and pipes) and the need to optimize the transportation system by avoiding the construction of unnecessarily over-sized streets and encouraging the use "green" infrastructure (trees and vegetation) as a method to manage stormwater, manage travel speed, and extend the life of street assets.
 8. Facilitate economic development for the purposes of sustaining and increasing tax revenue to support the maintenance and operations of public facilities and increasing property values.

9. Improve public health outcomes and recreational opportunities by promoting and enabling walking, bicycling, and other forms of active transportation to contribute reductions in childhood and adult obesity.
10. Practice responsible land development practices that will sustain Oshtemo Township's high quality of life over time by anticipating growth, appropriately sizing infrastructure, and considering the needs of all people in our community, particularly those who are too old, too young, disabled, of limited financial means, ineligible to possess a driver's license, or otherwise choose not to drive and/or own an automobile.
11. Provide for partnerships that establish clear, fair, and efficient development methods for applicants, residents, and community partners.

Section 1.04. Effective Date.

This Chapter shall take effect and be in force on and after **January X, 2024**.

Section 1.05. Applicability.

- A. This Ordinance will be used with all planning, design, funding, and approval involving any street construction, reconstruction, resurface, major repair or alteration, or right-of-way acquisition project.
- B. This Ordinance applies to all activities within public or private rights-of-way.
- C. This Ordinance is applicable to all land located within the Township. Mobility and the transportation system affects all properties, every building, structures, and uses.
- D. No street or road, or portion thereof, shall hereafter be designed, constructed, altered, maintained, or used except in conformity with this Ordinance unless otherwise preempted by a recognized Road Authority.

Section 1.06. Relationship to Adopted Plans.

The administration, enforcement and amendment of this Ordinance shall be consistent with the Township Comprehensive Master Plan, Go! Green Oshtemo Plan, Access Management Plan, Complete Streets Policy, and any adopted Area Specific Plans, and other plans that may be adopted by the Reviewing Body related to the future development of the Township. In the event this Ordinance becomes inconsistent with those plans, then this Ordinance shall be amended within a reasonable time to become or remain consistent.

Section 1.07. Relationship to Other Laws and Agreements.

- A. *Other Public Laws, Ordinances, Regulations or Permits.* This Chapter is intended to complement other municipal, state, and federal regulations that affect streets, street operations, connectivity, asset management, and land use. Unless otherwise noted in this Ordinance, where conditions, standards or requirements imposed by any provision of this Ordinance are more restrictive than comparable provisions imposed by other regulations, the provisions of this Ordinance shall govern.
- B. *Private Agreements.* This Ordinance is not intended to revoke or repeal any easement, covenant or other private agreement; provided, however, that where this Ordinance imposes a greater restriction or requirement, the provisions of this Ordinance shall control. Nothing in this Ordinance shall modify or repeal any private covenant or deed restriction, but any covenant or restriction shall

not be used to justify a lack of compliance with this Ordinance. The Township shall not be obligated to enforce the provisions of any easements, covenants, or agreements between private parties.

- C. *Township Authority.* Oshtemo Charter Township is granted broad police power under Article 7, Section 29 of the 1963 Michigan Constitution. Oshtemo Township contributes to road funding, and while it does not have direct control over public road classification or maintenance, it retains its police authority over the road right-of-way, having adopted the Uniform Traffic Code. It is important for transportation to be planned in a manner that is complementary to, and within the context of, existing and planned land uses in the Township and in consideration of the Township's Complete Streets philosophy.

ARTICLE 2 - DEFINITIONS

Section 2.01. Rules of Construction and Organization.

- A. The following words, terms and phrases, when used in this Article, shall have the meanings ascribed to them in this Chapter, except where the context clearly indicates a different meaning.
- B. Rules of Construction: The following rules of construction apply to this Chapter:
1. The language of this Chapter shall be read literally. Regulations are no more or less stringent than stated.
 2. The particular shall control the general. For terms used in this Chapter, the use of a general or similar term shall not be taken to be the same as the use of any other specific term.
 3. In case of any difference of meaning or implication between the text of this Chapter and any caption or illustration, the text shall control.
 4. Unless the context clearly indicates the contrary, where a regulation involves two (2) or more items, conditions, provisions or events connected by the conjunctions "and," "or" or "either . . . or," the conjunction shall be interpreted as follows:
 - a. "And" indicates that the connected items, conditions, provisions or events apply.
 - b. "Or" indicates that the connected items, conditions, provisions or events may apply singly or in any combination.
 - c. "Either ... or" indicates that the connected items, conditions, provisions or events apply singly but not in combination.
 5. Any reference to Chapter, Article or Section shall mean Chapter, Article or Section of Chapter XX of the Township Code, Streets Ordinance, unless otherwise specified.
 6. Terms not defined in this Chapter shall be given their ordinary and common meaning. Where not otherwise evident, terms shall have the meaning given in the latest edition of Merriam Webster's Collegiate Dictionary.

Section 2.02. Definitions.

ACCESS MANAGEMENT. A technique to minimize land access and/or traffic movement conflicts by reducing the number of access points, typically driveways, along major streets. Shared drives and the use of service roads or alleys, are characteristic access management methods.

ALLEY. A dedicated public or private way affording a secondary means of access to abutting property, not intended for general traffic circulation. See **Service Drive**.

BIKEWAY. "Bikeway" means a facility that explicitly provides for bicycle travel. A bikeway may vary from a completely separated facility to simple signed streets as follows: (a) "Shared-use path" (Class I Bikeway) is a facility for the exclusive use of bicycles, pedestrians and children's non-motorized vehicles separated from motor vehicle traffic except at bike crossings. (b) "Bike lane" (Class II Bikeway) is a marked lane contiguous to a travel lane within a roadway for the exclusive or semi-exclusive operation of bicycles in the same direction as the adjacent travel lane. The bike lane is physically separated from motor vehicle traffic by painted lines, pavement coloration, curbing, parked vehicles or

other barriers. (c) "Bike route" (Class III Bikeway) utilizes existing streets and roads. No separation of motor vehicle and bicycle traffic is provided as only signs are present to indicate the course of the bike route.

COMPLETE STREETS. Streets that meet the needs of all types of users – motorists, pedestrians (including those with disabilities), bicyclists, transit vehicles and users, freight haulers, emergency responders, and people of all ages and abilities. See the Township’s Complete Streets Policy.

COUNTY DRAIN COMMISSION. The Drain Commission of Kalamazoo County, Michigan.

COUNTY ROAD COMMISSION. The Road Commission of Kalamazoo County (RCKC), Michigan.

CUL-DE-SAC STREET. A short minor street having one end permanently terminated by a vehicular turnaround.

DEAD-END, PERMANENT. A permanent dead-end street is defined as one which is not reasonably anticipated to be extended.

FRONTAGE ROAD. A public or private drive which generally parallels an arterial public street between the right-of-way and the front setback line, linking two or more properties. See also **Service Drive** and **Private Street**.

LOT. A single unit or division of land contained in a platted subdivision, whether it be numbered, lettered, or otherwise designated, which has frontage on a public or private street or road.

MULTIFAMILY DEVELOPMENT. A building containing five or more separate dwelling units.

PATHWAY. An off-road shared use, non-motorized path, with paved surface, separate from the public road (though sometimes in the public right-of-way).

PRIMARY AND SECONDARY STREET. Where two (2) or more streets are compared for relative rank or importance, the arterial is the street with the higher(est) vehicle traffic counts.

PRIVATE DRIVEWAY. Any piece of privately owned and maintained property which is used for vehicular ingress and egress but is not open or normally used by the public. Intended to serve no more than one improved lot, parcel, or principal building of a residential or non-residential property.

PRIVATE SHARED DRIVEWAY. A privately owned and maintained drive, or any improved or unimproved easement, serving four (4) or fewer dwelling units, places of business, lots, parcels, or principal buildings, and which is not normally used by the public.

PRIVATE STREET. A privately owned and maintained drive, street, road, or any improved or unimproved easement, not dedicated to the county road commission as a public road, which provides access to five (5) or more dwelling units, places of business, lots, parcels, or principal buildings, and which is normally open to the public and upon which persons other than the owners located thereon may also travel. Private roads include roads within site condominium projects and those within office or industrial complexes. See also **Frontage Road** and **Service Drive**.

ROAD AUTHORITY. Public entity having assigned jurisdiction over specified roads in Oshtemo Township, consisting of either the Michigan Department of Transportation (MDOT), Road Commission of Kalamazoo County (RCKC), City of Kalamazoo, or other Act 51 agency.

SERVICE DRIVE. A public or private drive which is located generally behind buildings which front along an arterial street linking two or more properties, including additional lots behind those fronting the arterial street. See also **Frontage Road**, **Private Street**, and **Alley**.

SIDEWALK. A paved concrete surface, located in a road right-of-way or within a dedicated easement that is designed, constructed, and designated for pedestrian travel.

STREET. An existing or planned public or private right-of-way that is designed, dedicated, or used principally for vehicular traffic and providing access to abutting properties. The term street includes avenue, boulevard, circle, court, cul-de-sac, drive, place, road, or any other similar term.

STREETSCAPE. The various components that make up the street, both in the right-of-way and on private property including pavement, shoulders, gutters, sidewalks, permitted signs, parking spaces, landscaping and street trees, streetlights, etc.

STREET FRONTAGE. The distance that a lot line adjoins a public or private street from one (1) lot line intersecting the street to the furthest lot line intersecting the same street.

STREET FURNISHINGS. Elements within the Streetscape. Outdoor amenities, including but not limited to tables, chairs, umbrellas, landscape pots, wait stations, valet stations, bicycle racks, planters, benches, bus shelters, kiosks, waste receptacles and other similar items that help to define pedestrian use areas.

STREET AREAS. Streets consist of three areas: the travel way area, the parking area, and the pedestrian area.

1. **TRAVEL WAY AREA.** Accommodates vehicles and consists of vehicle travel lanes, bike lanes, turning lanes and medians (if provided).
2. **PARKING AREA.** Consists of vehicle parking lanes, and potentially of public transit elements. Transit facilities would be accommodated by replacing some parking spaces with bus bulbouts.
3. **PEDESTRIAN AREA.** The area between the curb and the edge of the right-of-way or road easement, although the pedestrian area may extend into an easement on private property depending on the width of the right-of-way and the existing conditions. The pedestrian area is separated into three subareas:
 - a. The edge area is the space adjacent to and including the curb. The minimum width of the edge area should be two and one-half feet to allow for door swings and snow storage. Along Local roads and Collectors, it may be landscaped.
 - b. The walkway/furnishings area accommodates streets trees, planters, street furniture, outdoor dining, and the clear and unobstructed pedestrian walkway.
 - c. The frontage area is the portion of the walkway at the boundary between the right of way and private property. This is the area of the sidewalk that is closest to the building. The frontage area accommodates door openings, window shoppers, and display.

STREET CLASSIFICATIONS. A hierarchy of street types that describes the Township’s transportation network, which consists of both existing, new, and planned streets, to provide the public with a connected and efficient circulation system. Functional classification defines the role that a particular roadway plays in serving the flow of vehicular traffic through the network. Roadways are assigned to one of several possible functional classifications within a hierarchy, according to the character of travel service each roadway provides.

1. **ARTERIALS.** A major street of significant continuity, which is intended to serve higher volumes of traffic for both the Township and the region, and which forms the basis around which the circulation system is designed. Typical traffic volumes are more than 10,000 vehicles per day. Arterials in Oshtemo Township include West Main, Stadium Drive, KL Avenue, South Ninth Street, Parkview, and Drake Road (north of Parkview).
2. **COLLECTORS.** A street that provides shorter distance movements within the Township, collects traffic from Local Streets and connects them with Arterials. Major collectors distribute and channel trips between locals and arterials usually more than ¼ mile in distance, have fewer access points, and may have more travel lanes and higher speed limits such as North 9th and 10th Streets (M-43 to H Avenue), South 6th Street (South of Stadium Drive), and West Michigan Drive (Venture Dr to Drake Road). Minor collectors typically have greater access and lower speeds such as North 2nd Street, North 6th Street, and Kalamazoo Avenue (8th to 4th Street).
3. **LOCAL STREETS.** Streets that provide direct access to homes, businesses, parking, and other land uses abutting the street right-of-way and serve short travel distances. Most subdivision streets fall in the Local Street classification.

STREET CONTEXT. The general land use characteristics, development patterns, access points and connectivity which infer usage of the street by certain transportation modes and users, thereby influencing street facility design. Urban and rural areas have fundamentally different characteristics. The use of context to determine street design criteria is consistent with national best practices and direction, including the 2018 American Association of State Highway and Transportation Officials (AASHTO) Greenbook and the National Cooperative Highway Research Program (NCHRP) Report 855: An Expanded Functional Classification System for Highways and Streets.

1. **URBAN.** A street surrounded by heavy residential, mixed-use, and/or commercial development that serves a wide spectrum of modes and users - oftentimes highly pedestrianized and transit-supportive while also experiencing high vehicular demands. These areas generally include curb and sidewalk sections, often with building fronts adjacent to or near the back of sidewalk. Frequent intersections, crosswalks, street trees, and on-street parking are usually present and operations more closely resemble that of Local Streets.
2. **RURAL.** Small concentrations of developed areas are immediately surrounded by agricultural lands, woodland, wetlands, and other natural areas with infrequent curb cuts. There tends to be moderate to high posted speeds on streets as a result and few traffic signals. Due to lacking density, travel is more auto oriented.
3. **RESIDENTIAL.** A street that serves residences and provides an important link for pedestrians and bicyclists in neighborhoods. Truck and other heavy vehicle traffic may be restricted.

4. **COMMERCIAL.** A street that serves mostly non-residential uses including retail, office, and industrial businesses as well as institutional uses such as hospitals or churches. In an urban setting, mixed-use buildings with residential above retail or offices may be present.

RIGHT-OF-WAY (ROW). An area owned or maintained by Oshtemo Charter Township, Kalamazoo County, State of Michigan, federal government, public utility, railroad, or private concern for the placement on, below, or above of utilities or facilities for the passage of vehicles or pedestrians, including roads, streets, pedestrian walkways, utilities, or railroads.

USERS. Legal users of the roadways and public right-of-way, including pedestrians, bicyclists, motor vehicle drivers, public transportation riders and drivers, and people of all ages and abilities including children, youth, families, older adults, and individuals with disabilities.

Section 2.03. Measurements.

- A. All measures are taken along the street centerline.
- B. Driveway spacing is measured from centerline to centerline.
- C. Driveway width measurements shall be made at the right-of-way line.
- D. A cul-de-sac will be measured from the nearest point to the property line.
- E. The distance of a block face shall be the length of the street creating the side of the block between two adjacent intersections.

ARTICLE 3 - GENERAL PROVISIONS

Section 3.01. Applicability.

All Streets. Standards and requirements for Complete Streets, connectivity, access management, internal circulation, and other street design standards in this Chapter shall apply to all public and private streets within Oshtemo Township unless indicated otherwise. Such provisions are considered necessary to construct and improve transportation facilities that address safety for all modes, advance self-enforcing design to prioritize vulnerable road users and the general health and well-being of the public, facilitate an efficient road network that balances accessibility and mobility needs, minimize environmental impacts, reduces our carbon footprint, and respects the existing built environment while anticipating future settlement patterns necessary to accommodate Oshtemo Township's growth.

Section 3.02. Right-of-Way.

- A. Road right-of-way shall provide for ingress, egress, drainage, and installation and maintenance of public and private utilities.
- B. Easements shall be provided for the entire width and length for all utilities and services as may be necessary to provide all public services to the lots which benefit from the public or private road.
- C. Road right-of-way to be publicly dedicated for public streets will meet the requirements of the Road Commission of Kalamazoo County. Private street right-of-way requirements are found in Article 6, Section 6.06 of this Ordinance.
- D. All Arterial right-of-way within or abutting plats hereafter recorded, shall provide a 50-foot half-width. The other half of the Arterial Street shall be platted with a private easement within such property or tract, or as identified in the Regulating Plan, per Section 3.08.

Section 3.03. Easements.

- A. A private Collector Street shall have a recorded easement permitting passage by the public within the right-of-way, in accordance with the intended function of the street type.
- B. All private streets shall have a recorded permanent easement for the entire length and width of the street, including the frontage on a public road. The easement shall expressly permit public and private utilities and services as necessary to provide all public services to the lots that benefit from the private road.
- C. The minimum easement radius for a turn-around shall be of such size that is able to encompass emergency vehicle turn-around.

Section 3.04. Street Names.

- A. The Township has the authority to assign street names. Except in Part B of this Section, Township staff will refer to the 'Kalamazoo County Street Naming and Addressing Policy, Procedures and Guidelines' and appropriate review procedures and workflow to name streets.
- B. A developer/applicant may propose street names for any proposed streets within a new development as part of the initial application process to establish a subdivision or site condominium.

December 2, 2023

- C. A letter of approval for street names shall be obtained from the Kalamazoo County Department of Planning and Community Development or appropriate agency, whether proposed by Township staff or a developer/applicant.
- D. Street names shall be finalized prior to final approval of a proposed subdivision or site condominium.
- E. Street names shall be sufficiently different in sound and spelling from other road names in the Township so as not to cause confusion. A street or road name duplicating one already used in Kalamazoo County is prohibited.
- F. Any extension of a street, or planned extension, shall retain the name of the existing street.
- G. All new streets shall be named as follows: Streets with predominant north-south directions shall be named, "Street;" streets with predominant east-west directions shall be named, "Avenue;" meandering streets shall be named, "Drive," "Lane," "Path," "Road" or "Trail," etc., and cul-de-sacs shall be named, "Circle," "Court," "Way" or "Place," etc.

Section 3.05. Street Addresses.

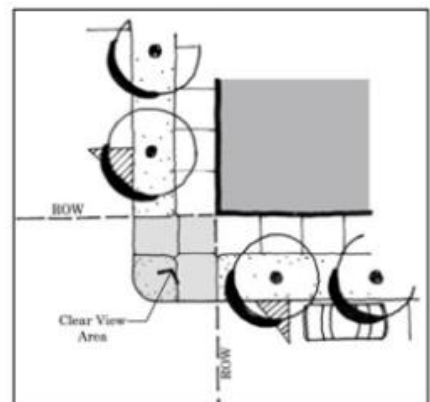
- A. The Oshtemo Township Planning Department will assign all street addresses.
- B. Township staff will refer to the 'Kalamazoo County Street Naming and Addressing Policy, Procedures and Guidelines' and appropriate review procedures and workflow to addressing streets.
- C. A letter of approval for street names shall be obtained from the Kalamazoo County Department of Planning & Community Development
- D. Corner lots will be assigned two addresses, one for each street, the final address for the corner lot will be the direction that the front of the house faces.

Section 3.06. Street Signs.

Signs identifying the intersection of a public and private street shall be paid for, posted, and thereafter maintained by the homeowner's association, condominium association, or other assigned party responsible for maintaining the private street. Intersection signs shall include stop signs on private streets and identifiable street name signs.

Section 3.07. Clear Vision Area.

- A. *Requirement.* Clear vision areas are required in locations where an unobstructed view of approaching traffic is necessary for the safety of pedestrians, bicyclists and drivers. A clear vision area is typically, but not exclusively, a triangular area at the intersection of two (2) streets, or a street and a driveway; however, clear vision areas may be required at other locations.
- B. *Public streets.* A clear vision area shall be maintained at all intersections. Nothing shall be located to cause a hazard to vehicular or pedestrian traffic by depriving the pedestrian or driver of a clear and unobstructed view of approaching, intersecting, or merging traffic. The clear vision standards of the Road Authority shall be satisfied where streets or drives intersect with public streets.



- C. *Private streets.* At the intersection of two (2) streets or the intersection of a private street and a driveway, the required clear vision area shall be established as follows:
1. *Street corners.* For streets, twenty (20) feet along each lot line starting at the intersection of the lot lines, and connected by a straight line to form a triangular area. In the case of a rounded corner, the measurement shall be taken from the intersection of the front lot lines extended.
 2. *Driveways.* For driveways, ten (10) feet along the lot line and the driveway starting at the intersection of the lot line and the closest edge of the driveway and connected by a straight line to form a triangular area.
 3. *Landscaping.* No plantings, signs, fences, walls, or other structures exceeding thirty (30) inches in height shall be established or maintained in clear vision areas.
 4. *New buildings or structures.* The Public Works Director may require a reduction in the height or location of a new building or structure to ensure an adequate clear vision area for driveways and streets. The limitation shall only be required for that portion of the building or structure necessary to provide an adequate clear vision area.
 5. *Administrative departure.* A new building or structure may be located within a clear vision area on a private street, provided the Public Works Director concurs with the findings of an independent engineering study provided by the applicant that demonstrates that siting of the new building or structure allows proper stopping sight distance as defined in *A Policy on Geometric Design of Highways and Streets*, as amended, by the American Association of State Highway and Transportation Officials (AASHTO).
- D. *Responsibility.* Where any street intersection will involve earth banks or existing vegetation inside any lot or building site corner that would create a traffic hazard by limiting visibility, the property owner shall clear such ground and/or vegetation (including trees) in connection with the grading of the public or private right-of-way to the extent deemed necessary by the appropriate Road Authority.

Section 3.08. Regulating Plan.

- A. *Purpose and Intent.* Oshtemo Township has an adopted Master Plan, comprised of multiple sub-area plans and documents which serve as its Regulating Plan. The Regulating Plan defines the location of planned future streets to ensure that the existing street network does not become overwhelmed from additional development that would cause excessive street widening, congestion, delay, and be counterproductive to creating a walkable community that is safe for vulnerable road users. The identification of future streets will ensure improved traffic flow, an enhanced street network, alternative traffic routing, access for emergency services, and comprehensive access management.
1. Streets should be developed consistent with the locations shown on the Regulating Plan. The exact location of streets on a particular site may be varied from those shown on the Regulating Plan to account for site-specific factors such as natural or man-made features provided that the general layout and number of streets and connection points for the continuation of streets on adjacent parcels are consistent with the Regulating Plan.
 2. Additional streets that are not shown on the Regulating Plan may be developed as necessary to create the required block system, and to distribute vehicular and pedestrian traffic.

3. To provide access to properties in conjunction with higher density development, a secondary street network may be developed to distribute traffic. The Regulating Plan will illustrate the street hierarchy and road purposes.
4. Local streets may be removed with the approval of the Reviewing Body provided the proposed development plan continues to maintain the required block system and meets other required standards. Any variations to the location or layout of streets as shown on the Regulating Plan shall require the approval of the Reviewing Body.
5. No land split or permit will be issued for, and no building or structure, or part thereof, shall be erected on any land located within proposed future street right-of-way. The Planning Commission may authorize the granting of a permit for the erection of a building or structure, or part thereof, within the defined area of the proposed public way, upon appeal by the owner of any affected land. Such appeal may be granted, based upon the following findings:
 - a. The entire property of the appellant, located in whole or in part within the lines of such street or public way, cannot yield a reasonable return to the owner unless the permit is granted; and,
 - b. Balancing the interest of the township in preserving the integrity of the Regulating Plan, and the interest of the owner of the property in the use and benefits of his property, the granting of the permit is required by considerations of justice and equity.
 - c. An alternative plan has been provided, that is demonstrably feasible and identifies any potential benefits and challenges associated with the different approach. The alternative will solve for the reasons that necessitated the deviation from the Regulating Plan and describe why the new route is either equal or superior to the originally planned street.
6. Before taking any such action, the Reviewing Body shall hold a public hearing, following not less than ten days' prior notice to the appellant by mail at the address specified by the appellant in his petition. The Reviewing Body shall have the power to specify the exact location, alignment, land area, and other details and conditions to be altered or permitted.

Section 3.09. Traffic Impact Study.

- A. *Intent.* The intent of conducting a traffic impact study is to understand the potential impact of a proposed development project on the surrounding transportation network, which may include but is not limited to truck routes, emergency routes, State and County roads, non-motorized network(s), and public transit. Information gleaned from the impact study is to assist in determining what, if any, mitigation measures may be necessary to support mobility within the township and to better understand the choices the Planning Commission, Township staff, and Township Board may need to make as the community continues to develop and grow.
- B. *Applicability.* A Traffic Impact Study (TIS) will be required under one of the following circumstances:
 1. A development project is 20-acres or greater in size;
 2. More than 100 housing units are proposed;
 3. More than 100 peak hour trips will be generated;
 4. A drive-through use will generate more than 20 cars per hour;

5. An assembly use will host 150 people or more for an event;
 6. A secondary access point is requested; or
 7. At the discretion of the Public Works Director for circumstances that resemble similar situations to the above given the characteristics of the proposed use, surrounding context, and/or anticipated types of traffic.
- C. *Review.* The TIS shall meet the requirements of the Road Authority’s guidelines for Traffic Impact Studies and Assessments Evaluating Traffic Impact Studies and be conducted by a licensed professional traffic operations engineer.
- D. *Scope.* Prior to commencing a TIS, the Public Works Director or Township-designated Traffic Engineer, in consultation with the Michigan Department of Transportation and Road Commission of Kalamazoo County, will coordinate with the applicant/developer to develop an understanding of the scope of work. Contents may include, but not be limited to, study area limits, existing and proposed traffic volumes, and crash data. Expectations for inputs of forecasted trips and volumes, which may include other approved and pending developments, will be defined. The TIS shall consider the highest proposed use for each designated area within the development site plan. Development that is proximate to a major intersection or interchange shall also include movements into and/or through the intersection or interchange.
- E. *Planning Commission.* Formal review and approval from the Road Authority shall be provided to the Township prior to the formal Planning Commission review of a project request.
- F. *Level of Service.* Declines in the Level of Service (LOS) within the right-of-way shall be reasonably mitigated by proposed solutions during site design phase. LOS is typically defined as a qualitative measure describing operational conditions for vehicular traffic; described in terms of such factors as speed and travel time, traffic interruptions, convenience, and safety. The Township’s Complete Streets approach expands LOS to include “Quality” of service to include the LOS experienced by all users and all modes, not only that of motor vehicles. Slow traffic speeds, for example, may equate to a high-quality environment for pedestrians but may cause delays for cars – presenting as two different LOS scores. Service levels are given letter designations, from A to F, with LOS A representing the best operating conditions and LOS F the worst.
1. A decline in vehicular LOS may be acceptable if improvement(s) to the non-motorized network is the direct cause and improved pedestrian LOS can be shown.
 2. Required operational changes and/or other mitigation measures shall be part of the MDOT and RCKC permit approval process.
 3. Mitigation efforts may be broken into phases tied directly to the corresponding phases of the comprehensive development plan. Phases must be clearly outlined within the phasing plan; item 10 within this Section. Preliminary designs shall be required.
 4. If the required traffic improvements identified within the TIS are already planned as part of an adopted comprehensive plan to be implemented by MDOT, the Kalamazoo County Road Commission, the Kalamazoo Area Transportation Study (KATS) or the Township, some or all of the mitigation requirements may be deferred or coordinated within a reasonable timeline. Any deferments or coordination shall require the support of the Public Works Director or representative. Short term or temporary efforts may be required to ensure the safety of the public during the deferment period. If the required mitigation efforts increase the scope of the already planned improvements by the local agency, the increase in cost to modify the plans

and construct the improvements shall be collected from the applicant. A memorandum of understanding shall be executed and recorded.

5. The Township will work with the developer/applicant in good faith effort to seek solutions and approvals necessary. This does not imply any financial commitment on the Township's part.

Section 3.10. Infrastructure and Service Needs.

- A. *Purpose and Intent.* The purpose of this Section is to permit development projects the ability to proceed at a faster pace than current Township resources are capable of constructing, installing, modifying, or improving existing infrastructure and/or service capacities to accommodate the development project. The project may itself be the sole reason for the infrastructure and service needs, or it may contribute to a heightened demand for infrastructure and services which are nearing or already at capacity. Inadequately sized infrastructure or insufficient service to the development project would result in one (1) or more declining levels of traffic safety, roadway capacity, reduced Level of Service (LOS) or water, sewer, energy, communications or other utility service reductions in the system. It is the intent of this Section to allow for development while ensuring that the project site and all customers that use and rely upon sufficient infrastructure and services within the community are properly accommodated.
- B. *Basis for Project Denial.* The inability of the Township to provide or enhance the available level of infrastructure or services to accommodate the development project may serve as the basis to deny a project request due to insufficient or increasingly insufficient infrastructure capacity if the project were to be constructed. Project denial due to insufficient infrastructure or services is not a desired outcome as development and redevelopment projects often improve the economic capacity of the community by increasing tax revenue, raising property values, and providing employment opportunities. Alternatives to improve infrastructure and/or service insufficiency are preferable to project denial. In these cases, the Township may offer an alternative to project denial by accepting the offer of voluntary support by the project's owners to undertake or contribute towards the cost of providing the needed infrastructure or service changes for future conditions created or contributed to as a result of the development project.
- C. *Alternatives to Advance.* In general, infrastructure or service changes are quantifiable in terms of capacity and cost. Needed changes may require study, planning, design, phasing or other efforts before being undertaken. In these situations, the Reviewing Body could, by contract with the project's owners, accept contributions to fund the work. The Reviewing Body would set aside the funds for use only to address the particular infrastructure and/or service changes associated with the development project. For example, when area streets and intersections are or will be functioning at low levels, undertaking or funding street and intersection improvements may be appropriate. Sometimes, however, street and intersection improvements may not be practical or may be insufficient to address the concerns. Due to topography, the impracticality of acquiring needed additional right-of-way, area-wide traffic patterns, jurisdictional issues or other limitations, different approaches such as non-motorized improvements, transit service extensions, remote parking, or other approaches may be the only feasible and reasonable alternatives to ameliorate anticipated infrastructure and service burdens imposed by the development upon the public. A particular project may provide the necessary impetus for these alternative approaches, particularly in relation to public health and safety, while itself providing insufficient support or justification. However, together with reasonably foreseeable additional projects, it may form the basis for addressing the need by these approaches.

- D. *Existing and Future Conditions Evaluation.* The applicant or property owner shall be informed of any inadequately sized infrastructure or insufficient services within the proposed project area that currently exists or that will be created or contributed to by the proposed development project. The Township will provide a basis for the determination that a development project, either by itself or in conjunction with other reasonably foreseeable projects, will:
1. Overload infrastructure or municipal services;
 2. Measurably degrade the level of infrastructure or public services to levels that adversely affect public health, safety or quality of life; or
 3. Place additional strains on infrastructure or public services that already are at levels that adversely affect public health, safety or quality of life.
- E. *Alternatives Evaluation.* The applicant may be encouraged to propose particular designs or improvements, cost estimates and other related information to recommend or identify changes on the project site, in the immediate project area or in locations which would assist in supporting the necessary infrastructure or services to sustain the development.
- F. *Determination.* Upon review of the alternatives to support the needed infrastructure and/or services to support the development project, the applicant may:
1. Appeal a determination made by Township staff to the Reviewing Body.
 2. Discontinue the project.
 3. Redesign the project to address the concerns.
 4. If it is acceptable to all Township and other governmental officials of competent jurisdiction, agree to:
 - a. Undertake and construct the needed infrastructure improvements according to plans and specifications approved and overseen by the Township or applicable jurisdiction;
 - b. Fund the needed infrastructure or service improvements pursuant to a written agreement approved by the Reviewing Body with the amount of the payment determined based on the actual costs of the improvements;
 - c. Contribute to a fund to be used by the Township to address the infrastructure or service concerns pursuant to a written agreement approved by the Reviewing Body with the amount of that contribution determined based on what the Reviewing Body reasonably determines to be the applicant's proportionate share of the reasonably anticipated costs of the improvements.

ARTICLE 4 – COMPLETE STREETS

Section 4.01. Purpose and Intent.

The purpose of this Article is to establish mobility and circulation standards that give equal treatment to all modes of travel. Improving connectivity between the current and future network of sidewalks, bike lanes, shared use paths, public transit routes, and roadways is important to ensure that Oshtemo Township serves all residents, regardless of age, ability, or income. A well-connected and well-designed network will provide safe access for all users, allow reasonable access to properties, promote healthy living, maintain and/or increase the capacity and efficiency of the roadway network, ensure safe access for emergency vehicles, and reduce negative environmental impacts. It is important that a well-connected network include safe and convenient transitions from one mode of transportation to another and from one jurisdiction to another.

Section 4.02. Applicability.

- A. *Implementation.* The Township shall endeavor to make complete streets practices a routine part of design, strive to make every street project accessible for all users, and coordinate efforts with other agencies and jurisdictions to achieve complete streets. The Township shall work in partnership with the Kalamazoo County Road Commission and Michigan Department of Transportation to facilitate implementation of Complete Streets to the extent feasible, pursuant to this Ordinance.
- B. *Applicability.* Any project involving the construction, reconstruction, capacity enhancement, or preservation of a roadway, bridge, public parking facility, or other transportation right-of-way within the Township, are subject to these provisions, except as provided in Part D of this section. All street projects requiring funding and/or approval by the Township Board shall:
1. Evaluate the effect of the proposed project on safe travel by all users.
 2. Identify measures to mitigate potential adverse impacts on travel. Vehicular level of service is not considered to be the only measure for which mitigation would be required, quality of service for pedestrians and other non-motorized users is also important.
 3. Achieve implementation of Complete Street infrastructure to the extent feasible.
- C. *Exclusions.* Infrastructure that supports a Complete Streets approach may be excluded upon agreement of the Township Board and the applicable Road Authority, subject to each agency's powers and duties, where documentation and data indicate that:
1. An affected roadway prohibits a specified user by law, in which case a greater effort shall be made to accommodate those specified users elsewhere.
 2. An alternative route, such as a multi-use trail in the immediate vicinity provides an option for non-motorized transportation.
 3. A wide shared-shoulder designed to accommodate both bicycle and pedestrian use is available in the street right-of-way.
 4. There is insufficient right-of-way to accommodate an improvement at the time. The Township should attempt to acquire the right-of-way to accommodate the Complete Street element.
 5. The activities, such as spot repair or crack seal, are ordinary maintenance activities designed to keep assets in serviceable condition, which meet MDOT's ADA upgrade exceptions.

6. Inclusion of new facilities would be inappropriate when associated with capital preventive maintenance projects such as thin overlays (less than 1.5- inch), micro-seals, and chip seals.
7. Safety projects which are funded only for specific safety features identified by crash patterns, due to the funding parameters of the program.
8. The cost would be excessively disproportionate to the need or probable future use over the long term as documented in the adopted Future Use Plan, or other present and/or anticipated market conditions, indicating an absence of existing or future need (e.g. sparse population).
9. Implementation of Complete Streets infrastructure is not possible due to physical constraints imposed by the project area, such as topography, historic building placement, etc.
10. There will be an adverse impact on existing environmental resources such as wetlands, floodplains, creeks, or historic structures, or the presence of an environmental resource will add significant cost of a project.

Section 4.03. Characteristics.

Streets will be capable of accommodating multiple modes of transportation and will facilitate the creation of a public realm designed primarily for people, characterized by:

- A. Pedestrian-friendly design that places a high priority on walking, bicycling, and use of public transit;
- B. Streets and blocks arranged to allow for comfortable walking distances, to disperse traffic and to reduce the length of vehicle trips;
- C. A connection to, and enhancement of, the existing street network;
- D. A recognition of the role of buildings and landscaping that contributes to the physical definition of streets as civic places, and
- E. Residential and business uses that have convenient access to existing and future shared use paths and transit stops, where applicable.

Section 4.04. Design Guidance.

The design guidelines promulgated in State standards and guidelines, or as may be required by the funding source, shall be referred to and considered in the construction, rehabilitation, and maintenance of non-motorized transportation facilities and private streets in the township. Such sources include, but are not limited to, the Michigan Manual on Uniform Traffic Control Devices (MMUTCD), MDOT Best Design Practices for Walking and Bicycling in Michigan, MDOT Bicycle and Pedestrian Resources for Transportation Professionals (2016), NACTO Urban Bikeway Design Guide (2014), AASHTO Guide for the Development of Bicycle Facilities (2012), NACTO Urban Street Design Guide (2013), NACTO Transit Street Design Guide (2014), FHWA Separated Bike Lane Planning and Design Guide (2015), FHWA Small Town and Rural Multimodal Networks (2016), NACTO Designing for All Ages and Abilities (2017), and FHWA Bikeway Selection Guide (2019).

Section 4.05. Elements in Regulating Plan.

The Regulating Plan of the Township's Comprehensive Master Plan and the Non-Motorized Facilities Plan will include recognized and appropriate Complete Streets elements for implementation into the Township's street and non-motorized trail network. Transportation improvements will include facilities

and other amenities that are recognized as contributing to Complete Streets, which may include, but not necessarily limited to, one or more of the following:

- A. Sidewalks (new construction, gap construction, repair or replacement, and Americans with Disabilities Act (ADA) improvements).
- B. Pedestrian refuge islands or crosswalk improvements.
- C. Traffic calming measures, where appropriate.
- D. Street and/or sidewalk lighting.
- E. Multi-use trails.
- F. Accessibility improvements consistent with the ADA, particularly around transit.
- G. Metro Transit stop improvements.
- H. Bicycle facilities, including designated bike lanes, separated facilities, and widened travel lanes.

Section 4.06. Reserved.

Section 4.07. Widening.

Any project that would widen an existing street or intersection shall be submitted to the Planning Commission for review and comment. The Planning Commission will review the proposed plans for compliance with this Ordinance and if any adverse impacts may be anticipated that would affect other legal road users such as pedestrians and bicyclists. If there will be negative effects arising from the widening, mitigation measures will be reviewed and discussed. The Planning Commission will then transmit a report to the Township Board.

ARTICLE 5 – CONNECTIVITY

Section 5.01. Purpose and Intent.

The arrangement of streets shall be interconnected with each other and with streets on abutting properties in a systematic grid pattern. Street connectivity and continuity is necessary to circulate traffic, provide emergency service access, ensure network reliability and redundancy, develop a logical system to facilitate the movement of all transportation system modes and users, and sustainably and efficiently manage uses of land and the provision of utilities and public services. Connectivity, for the purposes of this Ordinance, refers to structure of the transportation network of Oshtemo Township consisting of blocks, intersections, and connecting points.

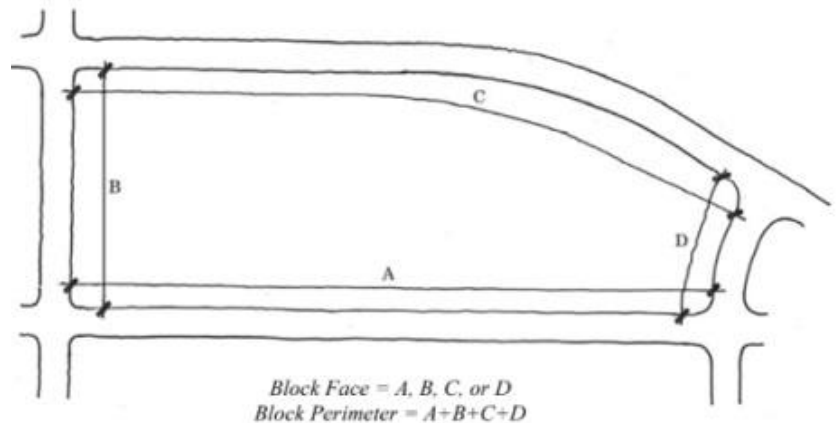
Section 5.02. Applicability.

Development shall occur using a combination of both the existing street network and new streets added to establish an improved circulation system. Streets may be public or private and be established in a manner consistent with the Township's Master Plan and Regulating Plan. Where existing streets are being improved, these standards along with the other standards overseeing such road development shall guide the design of the improvements.

Section 5.03. Blocks.

Requirements. The following requirements shall apply to ensure that Oshtemo Township's street network develops as an interconnected network of streets.

- A. The street network shall be laid out in defined blocks in an orthogonal manor to intersect as nearly as possible to 90 degrees.
- B. Where adjoining areas are not subdivided, the arrangement of streets shall be required to be extended to the boundary line of the project to make provision for the future projection of streets into adjoining areas.
- C. A minimum of one stub street shall be provided for each 660 feet or fraction thereof along such property lines.
- D. No block perimeter, measured along the block face at the public right-of-way or private road easement, shall exceed 2,400 feet.
- E. Blocks should be designed so that at the terminus of street intersections, street alignment, or the curvature of a street produces "terminal vistas" of civic buildings, public art, play fields, meadows, wetlands, or other notable structures or natural features.
- F. Privately-held reserve strips controlling access to streets shall be prohibited.



Section 5.04. Street Intersections.

- A. Streets shall intersect as nearly as possible to 90 degrees and in no case less than 80 degrees. Curved streets, intersecting with primary roads, will have a tangent section of centerline 50 feet in length, measured from the right-of-way line of the primary road.
- B. Not more than two streets can intersect at any one point unless specifically approved by the Township in cases of a traffic circle or roundabout.
- C. Where the proposed continuation of a street at an intersection is not in alignment with the existing street, it must not intersect such cross street closer than 200 feet from such opposite existing street, as measured from the centerline of said streets.
- D. Concrete curbing will be installed throughout the entirety of an intersection within the public right-of-way or private street easement. Curb cuts and sidewalk ramps shall be integral to the construction of the concrete curb.

Section 5.05. Connections.

- A. When a Collector or Local street will link different land ownerships or different public streets, either currently or in the future, it shall, regardless of whether it is public or private, be constructed and dedicated as a public street or, if approved by the Township, it may be a private street located upon a right-of-way or easement granted to the Township for public ingress and egress.
- B. Private roads shall not interconnect with the public street network in a manner that will preclude the logical, orderly, and efficient development of the overall public street network. In making such a determination, consideration shall be given to the circulation pattern and traffic volumes on nearby public streets, existing and proposed land uses in the general area, trend of development, and the recommendations contained within the Oshtemo Charter Township Comprehensive Master Plan, Regulating Plan, and other plans by Road Authorities, as applicable.
- C. Frontage streets, service drives, private shared driveways, and public or private streets shall connect to existing outlots in adjacent developments and shall provide outlots or other provisions for future connections to adjacent land that is presently undeveloped. This may necessitate the upgrading of a private shared driveway to a private street, per the process described in Article 9 of this Ordinance.
- D. Where adjoining areas are undeveloped and the street must temporarily dead-end, the right-of-way shall be extended to the property line to make provision for the future projection of street.

Section 5.06. Dead-End Streets.

- A. A permanent dead-end street shall only be permitted where the topography of the area, lakes, streams, other natural features or existing adjacent development of the area causes practical difficulties or extreme hardship in connection and can be granted without creating any safety concerns. A supportive recommendation from the Public Works Department shall be required in order to be considered by the Reviewing Body. The applicant shall clearly provide evidence of hardship to be considered.
- B. A dead-end public right-of-way or private street easement (whether temporary or permanent) in excess of 660 feet in length, as measured from the nearest public right-of-way or private street easement to the dead-end street, shall be prohibited except upon recommendation from the Public

December 2, 2023

Works Department and approval from the Reviewing Body for the reasons cited in 5.05.A., and an extension can be granted without creating a safety hazard.

- C. A cul-de-sac turnaround will be provided at the end of a permanent dead-end street or a temporary dead-end street (and associated temporary right-of-way). The Township may require an easement or a reservation of easement to accommodate drainage facilities, pedestrian access, or utilities.
- D. Where a public right-of-way or private street easement is not intended to extend beyond the boundaries of the subdivision/site condominium and its continuation is not required by the Township for access to adjoining properties, its terminus shall be at least 50 feet from such boundary.
- E. *Administrative Departure.* The Public Works Director may find a hammerhead “T” or a continuous loop layout acceptable to terminate a private dead-end street where public or private utilities are unaffected and sufficient space is provided for vehicle maneuvering.

ARTICLE 6 – ACCESS MANAGEMENT

Section 6.01. Purpose and Intent.

Access management techniques increase traffic safety by directing traffic to appropriate turning locations, reduce accident frequency and/or severity, provide reasonable access, help maximize road capacity, lessen congestion, encourage orderly development, and protect non-motorized users by limiting driveway crossings. These regulations are based on considerable research in Michigan and nationally and were prepared concurrent with guidelines promoted by the Michigan Department of Transportation (MDOT). The major elements of access management are driveway design (geometrics), limited number of driveways, driveway spacing, shared access, and side street access. This Article addresses the point where private property accesses a public or private road.

Section 6.02. General.

- A. *Access Management Plan.* The Osthemo Charter Township Access Management Plan, as amended, should be referenced in conjunction with the provisions of this Ordinance. Where there is a conflict between the two, the provisions of this Ordinance shall apply.
- B. *Nonconforming driveways.* In the case of expansion, alteration or redesign of an existing development where existing driveways do not comply with the requirements of this Ordinance, the closing, relocation, or redesign of the driveway may be required.
- C. *Access agreement.* A copy of a written and recorded agreement for shared access between two or more property owners shall be provided to the Township.
- D. *Drainage.* Driveways shall be constructed such that drainage is channeled away from the street right-of-way.
- E. *Driveway approvals.* A Driveway Permit shall be obtained prior to connecting a driveway to any public or private street. Driveways associated with site plan reviews per Article 64 of the Zoning Ordinance shall follow the Township’s site plan review process. An access determination letter from the corresponding road agency shall be provided to the Township prior to the formal consideration of the project by the Reviewing Body.
- F. *Condo projects.* Condominium developments and all associated units shall be serviced by an interior transportation network. Individual units shall not have direct access to an arterial or collector street, and must be served by a public or private local street, service drive, frontage road, or private shared driveway.
- G. *Greater authority.* The appropriate Road Authority may require a more restrictive standard than contained herein.

Section 6.03. Private Driveway.

- A. *One premises.* A private driveway may serve only one premises, parcel, unit, or lot.
- B. *Minimum width.* The improved surface of the driveway shall have a total minimum width of ten feet.
- C. *Approach.* Driveway approaches shall only be installed in conjunction with approved curb cuts and access management spacing requirements.
- D. *Administrative departures.* A second driveway may be permitted under the following circumstances.

1. Where a development has access to a signalized location, the approach volume of the driveway traffic should be double that shown for unsignalized locations to warrant consideration of a second access location. Warrants shall be based on trip generation calculations.
2. For developments that can demonstrate that their combined driveway approach volumes (entering and existing) will exceed 3,000 during an average day (or will be used by 300 vehicle during the peak hour of traffic for either street or the use), and lacking access to a secondary street, a second driveway may be allowed along the major street provided that the additional driveway can meet the spacing requirements.

Section 6.04. Shared Private Driveway.

- A. *Number.* One (1) shared private driveway is allowed for up to four (4) or fewer dwelling units, places of business, lots, parcels, or principal buildings. An “eyebrow” access is allowed as a shared private driveway.
- B. *Right of Way.* Thirty (30) feet of right-of-way shall be preserved for the shared driveway, with easements granted to all parties for access.
- C. *Location.* The shared private driveway shall be constructed along a joint property line and/or a written easement is provided which allows traffic to travel across parcels for access.
- D. *Construction.* An engineered drawing shall be prepared for the review of the Public Works Director that includes information on anticipated vehicle types, traffic volumes, adjacent land uses, stopping locations, and other pertinent information that would assist in determining if the plans as proposed will reasonably ensure a durable, long-lasting shared private driveway.
- E. *Connections.* Vehicular connections between parking lots and vacant sites for future development should be included in project design where practicable.
- F. *Administrative Departures.* A second shared private driveway may be permitted in accordance with Section 6.03.D, a second shared private driveway may be permitted under the following circumstances.
 1. Where a development has access to a signalized location, the approach volume of the driveway traffic should be double that shown for unsignalized locations to warrant consideration of a second access location. Warrants shall be based on trip generation calculations.
 2. For developments that can demonstrate that their combined driveway approach volumes (entering and existing) will exceed 3,000 during an average day (or will be used by 300 vehicle during the peak hour of traffic for either street or the use), and lacking access to a secondary street, a second driveway may be allowed along the major street provided that the additional driveway can meet the spacing requirements.

Section 6.06. Access Points.

- A. *Number.* The number of access points shall be limited to the minimum needed to provide reasonable access. Access points shall be designed and located to minimize conflicts with traffic operations along the street and be placed as far from intersections as practical. Land divisions shall not be permitted that may prevent compliance with the access location standards of this Article.
- B. *Adjoining lands.* Where the subject site adjoins land that may be developed or redeveloped in the future, including adjacent lands or potential out lots, the access shall be located to ensure the

adjacent site(s) can also meet the access location standards in the future. Site development shall be done to provide for future cross-access to adjacent lots. A cross access connection or cross-access easement may be required as part of development approval.

- C. *Primary access.* Where property has frontage or access on more than one roadway, access shall be provided from the lesser traveled street. Where spacing requirements can be met, high traffic volumes will be generated, or the subject side street is inappropriate for non-residential traffic, access onto the main roadway will be considered.
- D. *Arterials.* New access points on Arterials is to be minimized, with internal circulation patters designed to channelize traffic flow via adjacent streets, frontage streets, or service roads, with dedicated access points and signalized intersections that function as gateways.
- E. *25 residential units or less.* A private road cul-de-sac that has only one point of connection to a public road or another private road may provide vehicular access to a maximum of 25 dwelling units.
- F. *Second access and/or emergency access.* Any private street that will serve land uses generating a combined daily volume of traffic of 3,000 trips per day or more or exceeds a length of 1,500 feet shall have at least two means of direct access to public roads. The second means of access may be used for emergency access purposes only with approval from the Reviewing Body. Each access shall be built and maintained to the standards required for private streets.
- G. *Parking facilities.* Adequate ingress and egress to a parking facility shall be provided by clearly defined driveways. Driveway design and placement must be in harmony with internal circulation and parking design such that the entrance can reasonably absorb inbound traffic during a normal peak traffic period.
- H. *Interference with public facilities.* No access point shall interfere with municipal facilities such as streetlights, traffic signal poles, signs, fire hydrants, crosswalks, bus loading zones, utility poles, fire alarm supports, drainage structures, or other necessary street structures. The Township is authorized to order the removal or reconstruction of any driveway which is constructed in conflict with street structures. The cost of reconstructing or relocating such driveways shall be at the expense of the adjoining property owner.

Section 6.07. Spacing.

- A. *Access points.* All spacing of access points shall comply with appropriate Road Authority standards and the provisions of this Article. Spacing will be based on posted speed limits along the property frontage in accordance with Table 6.07.A.

Table 6.07.A. Required Driveway Spacing Distances	
Posted Speed Limit (MPH)	Driveway Spacing (feet)
30	125
35	150
40	185
45	230
50	275
55	350

- B. *Street alignment.* Where possible, the approaches of side roads from opposite sides of a through road should be in direct alignment.

- C. *Driveway alignment.* Driveways shall be aligned with those from opposite sides of the street. Where this is not possible, driveways shall be offset a minimum of 150 feet, measured from centerline to centerline, to minimize left turn conflicts.
- D. *Crashes.* In areas where accidents and congestion due to left turn movements have created a demonstrated crash pattern where public health and safety are at risk, designs to discourage left turn ingress and/or egress may be considered by the Public Works Director or Road Authority.
- E. *Intersections.* Minimum spacing of access points from intersections, shall comply with Township Public Works and Road Authority standards. Spacing shall be subject to the schedule outlined in Table 6.07.E. Distance is measured from the nearest edge of the driveway throat to the nearest edge of the intersection.

Table 6.07.E. Required Driveway Spacing from Intersections			
	Intersecting Street	Full Movement Driveway (feet)	Channelized for right-in/ right-out turn only (feet)
Driveways Along Arterials			
	Arterial	250	100
	Signalized Non-Arterial	125	75
	Other Street	100	75
Driveways Along Side Streets Intersecting Arterials			
	Arterial	200	100
	Signalized Non-Arterial	100	75
	Other Street	75	75

- F. *Administrative Departure.* If the amount of street frontage is insufficient to meet these criteria, the driveway shall be constructed adjacent to the property line furthers from the intersection.

Section 6.08. Emergency Access.

- A. *Access and Occupancy.* The private street shall be readily accessible to and usable by emergency vehicles in all weather. An occupancy permit required for a dwelling or other building, the primary access to which is to be provided by a private street, shall not be issued until the private street has been constructed with sufficient width, surface and grade to ensure the safe passage and maneuverability of emergency service vehicles.
- B. *Gated Access.* All gates blocking access to a private road shall have an access code determined by the fire department, and be equipped with a keyed switch which will keep the gate open. The keyed switch must use a Knox Box Key.

ARTICLE 7 – INTERNAL CIRCULATION

Section 7.01. Purpose and Intent.

As the township continues to grow and develop, and large parcels are subdivided into smaller lots, it is within the public interest to ensure that development patterns are accomplished in an orderly and logical way, the function of internal circulation systems work well so as not to overburden the public road network, travel movements are predictable and consistent, and the road hierarchy works as intended. The large scale of modern development patterns requires purposeful intent to provide dignified and safe pathways for those who are too young, too old, physically unable, too poor, or simply choose not to drive a car if Oshtemo Township is to provide a high quality of life for all residents.

Section 7.02. Applicability.

Internal circulation systems are a component of the township's street network. It is for this reason that frontage roads and service drives are classified as private Local streets, akin to alleys. Internal site circulation directs a resident, patron, guest, employee, and others literally to the front door of a commercial business, place of assembly, multi-family residential building, mixed-use complex, or other use or structure. All attached single-family and multiple family residential, non-residential, and mixed-use developments shall comply with the following requirements.

Section 7.03. Circulation Study.

- A. *Intent.* The intent of conducting a circulation study is to understand existing and/or future site operations and the ability of the public to travel to, from, and through any given location safely and efficiently, and how a location interacts with adjoining land uses, structures, and mobility infrastructure within a given context.
- B. *Criteria.* The internal circulation system of existing or proposed development may be reviewed by the Public Works Director and Planning Director under one of the following circumstances:
 1. Site plan submittal or an amendment thereto;
 2. Addition of a new driveway or access point;
 3. Increase in the intensity of a land use on the site or on a connecting site with shared access;
 4. When a new or amended cross-access agreement is enacted;
 5. Parking lot repaving/resurfacing; or
 6. Where a concern has been expressed by law enforcement, transit authority, Road Authority, or other entity responsible for public safety associated with vehicular operations.
- C. *Scope.* Prior to commencing an evaluation, the Public Works Director or Township-designated Traffic Engineer, will coordinate with the applicant/developer to develop an understanding of the scope of work. The scope shall consider the safety of all users (motorists, pedestrians, transit riders, bicyclists, persons with disabilities) and their ability to travel to/from the front door of the use or structure being served by the parking lot, frontage road, service drive, or other connecting component on the site to nearby uses and buildings, public or private right-of-way, and abutting properties. Direct routes, particularly for non-motorized users, will be considered.
- D. *Improvements.* The Public Works Director, upon reviewing the results of the circulation study, shall determine if improvements or modifications are needed to the internal circulation system, items

may include but not be limited to: parking lot restriping to modify drive lane alignment or delineate pedestrian walking areas; the installation of a stop sign, sidewalks, or curb ramps; assignment of a left-hand turn-lane; and the addition or relocation of a bus stop or shelter, etc.

Section 7.04. Frontage Roads and Service Drives.

A. *Applicability.*

1. In areas where frontage roads or service drives exist, access to individual properties shall be provided by these drives rather than direct access.
2. The Reviewing Body may require a frontage road or rear service drive where such facilities can provide access to signalized locations, the number of driveways may be minimized, as a means to ensure that traffic is able to move more efficiently, or provide a greater degree of safe ingress and egress.

B. *Placement.* Frontage roads and service drives shall be set back as far as reasonably possible from the intersection of the access driveway with the public or private street. A minimum distance that equals the required front yard (setback) or the major road setback, whichever is a greater distance from the right-of-way, shall be maintained between the public street right-of-way and the pavement of the frontage road, with a minimum fifty (50) feet of throat depth provided at the access point. The Reviewing Body may extend throat length if modeled vehicle queues necessitate expansion.

C. *Alignment.* The alignment of the service drive can be refined to meet the needs of the site and anticipated traffic conditions, provided the resulting terminus allows the drive to be extended through the adjacent site(s).

D. *Building arrangement.* Each building or group of buildings used for non-residential purposes, and its parking or service area, shall be physically separated from public streets by a greenbelt, curb, or other suitable barrier against unchanneled motor vehicle access or egress, except for access ways authorized herein.

E. *Construction and design.* Frontage roads and service drives shall be constructed and designed in accordance with private road standards for Urban Collector or Urban Local streets, as determined by the Public Works Director and Planning Director based upon the anticipated amount of traffic and its characteristics and in accordance with Article 8 Street Design.

1. *Streetscape.* Major drive aisles and entry drives within and between significant developments and their parking facilities may be required by the Reviewing Body to be treated similarly as public roadways in terms of streetscape. Similar street trees and light fixtures shall be located on those routes designated to carry traffic in, out, and between large development areas and parking lots. This shall be included as part of the landscape plan.
2. *Delineation.* Curbing, signage, islands, or some other means to guide the route of through traffic shall be provided. If a non-conforming situation exists, it shall be remedied when 50% or greater of the total surface area of the parking lot abutting the drive location is reconstructed over the course of the life of the parking lot.
3. *Driveway spacing.* Minimum driveway spacing standards shall not apply to frontage roads and service drives.
4. *Parking.* Parking may be allowed along a frontage road or service road so long as it is designed to meet on-street parking standards for urban collector streets.

5. *Administrative departure.* In the case of expansion, alteration, or redesign of an existing development where it can be demonstrated that pre-existing conditions prohibit installation of a frontage road or service drive in accordance with this Ordinance, the Reviewing Body may allow alternative cross access between adjacent parking areas through the inter-connection of main circulation aisles.
- F. *Temporary direct access.* In cases where a frontage road or service drive will be used for access, but is not yet available, temporary direct access may be permitted, provided the plan is designed to accommodate the future service drive, and a written agreement is submitted that the temporary access will be removed by the property owner, when the alternative access system becomes available.
 1. The Township may require posting of a financial performance guarantee to ensure compliance.
 2. If the Township approves a provisional access, the developer shall provide an adequate surety bond or other guarantee deemed acceptable to the Township in an amount sufficient to cover construction costs associated with the closing of the provisional road access.
 3. The site plan shall show the proposed layout of the site when the provisional access drive is removed.
- G. *Administrative departure.* The Planning Department may reduce required lot size and road frontage requirements by up to ten (10) percent under one of the following conditions:
 1. A driveway is established to serve two or more parcels, and where such parcels are not served by any other access point.
 2. When a frontage road or service drive is created that serves two or more parcels.
 3. Evidence of a binding cross-access or shared parking agreement regarding two or properties is provided.

Section 7.05. Vehicular Circulation.

- A. *Access.* Unobstructed vehicular access to and from a public street will be provided for all off-street parking spaces. Vehicular access shall be provided in such manner as to protect the safety of persons using such access or traveling in the public or private street from which such access is obtained and in such manner as to protect the traffic-carrying capacity of the public or private street from which such access is obtained.
- B. *Circulation routes.* Parking lots shall provide well-defined circulation routes for vehicles, bicycles and pedestrians.
- C. *Traffic control devices.* Standard traffic control signs and devices shall be used to direct traffic where necessary within a parking lot.
- D. *Orientation.* Parking bays shall be perpendicular to the land uses they serve to the maximum extent feasible. Large parking lots shall include walkways that are located in places that are logical and convenient for pedestrians in accordance with Section 7.06.
- E. *Landscape islands.* To the maximum extent feasible, landscape islands with raised curbs shall be used to define parking lot entrances, the ends of all parking aisles and the location and pattern of primary internal access drives, and to provide pedestrian refuge areas and walkways.

- F. *Points of Conflict.* The lot layout shall specifically address the interrelation of pedestrian, vehicular and bicycle circulation in order to provide continuous, direct pedestrian access with a minimum of driveway and drive aisle crossings.
- G. *User needs.* Layout and design shall anticipate the needs of users and provide continuity between vehicular circulation, parking, pedestrian and bicycle circulation. Pedestrian drop-off areas shall be provided where needed, especially for land uses that serve children or the elderly.
- H. *Pavement.* All vehicle circulation and off-street parking areas will be surfaced with asphalt or concrete.
- I. *Trucks.* Truck access and circulation routes shall be designed to minimize potential traffic and noise conflicts with adjacent sites, walkways between sidewalks and principal building entrances, and internal circulation routes.
- J. *Drive-through.* Queuing and a by-pass lane shall be provided for drive-through establishments described in Table 7.05.A. Minimum Queuing Standards.
 1. Each queuing space shall be a minimum of ten feet by 20 feet, clearly defined and designed so as not to conflict or interfere with other traffic using the site.
 2. Queuing shall be measured from the front of the stopped vehicle located at the point of service to the rear of the queuing lane.
 3. One additional queuing space shall also be provided after the point of service for all uses.
 4. A by-pass lane a minimum of ten feet wide shall be provided before or around the point of service.

Table 7.05.A Minimum Queuing Standards		
Drive-Through Use	Number of Spaces	Required By-Pass
Financial institution	3	No
Restaurant, low-volume (less than 10/hour)	3	No
Restaurant, high-volume	7	Yes
Carwash, automatic	5	No
Carwash, self-service	3	Yes
General retail	4	Yes

- K. *Administrative departure.* A by-pass lane may not be required if the queuing lane is adjacent to a vehicular use area which functions as a by-pass lane.

Section 7.06. Pedestrian Circulation.

- A. *Purpose.* Pedestrian access shall be required for all sites to improve the health, safety and welfare of the public by providing clear pedestrian pathways at perimeter and internal site locations to reduce pedestrian and vehicular conflicts, improve accessibility for persons with disabilities, and establish a multi-modal environment that is supportive of walking, biking and transit use. These requirements are also intended to promote healthier lifestyles by encouraging walking and bicycling over the use of a private vehicle for many daily activities, and to provide the means by which residents and visitor can be more engaged with their neighbors, coworkers, and fellow visitors.

- B. *Walkways.* Continuous pedestrian walkways shall be provided to connect off-street surface parking areas with the primary entrances of main buildings. To the maximum extent feasible, pedestrians and vehicles shall be separated through the provision of a sidewalk or walkway.
 - 1. The required walkway must be at least five (5) feet wide, shall not be within a driving aisle, and, where possible, shall be within a landscaped island running perpendicular to the primary building façade. If parking spaces are adjacent to a sidewalk, the sidewalk must be at least seven (7) feet to allow for vehicular overhang.
 - 2. Dedicated sidewalks will be raised above the surface of the parking lot, or, if at the same level as the parking lot, the walkway will be distinguished from driving surfaces through the use of durable, low-maintenance surface materials such as pavers, bricks, or scored/stamped concrete or asphalt.
 - 3. To minimize potential hazards and enhance safety, in addition to a change in materials other tactics should be employed such as clearly marked striping, tactile alerts, landscaping, bollards, lighting and other means to clearly delineate pedestrian areas.
- C. *Network connections.* At least one (1) pedestrian walkway with a minimum width of five (5) feet shall be provided from the internal pedestrian walkway network to the public sidewalk or trail system. In the case of corner lots, connections shall be made to the sidewalks of both streets. Where trails exist or are planned, paths or sidewalks shall connect building entries to the trail system.
- D. *Additional.* Each surface parking area that has fifty (50) or more parking spaces, or has any parking spaces more than three hundred fifty (350) feet from the front façade of the main building, shall have at least one (1) pedestrian walkway or sidewalk allowing pedestrians to pass from the row of parking furthest from the main building façade to the primary building entrance.

Section 7.07. Transit Access and Amenities.

- A. *Transit Stops.* Where public transit service is available or planned, convenient access to transit stops shall be provided by means of public or private sidewalks or walkways. Any provided seating shall not obstruct a public sidewalk.
- B. Where transit shelters are provided, they shall be placed in highly visible and well lighted locations for purposes of safety, subject to review by The Rapid.
- C. Landscaping. Landscape and/or plaza areas are encouraged at transit stops.

Section 7.08. Bicycle Facilities.

- A. *Bicycle Parking.* Bicycle parking shall be within fifty (50) feet of the nearest publicly accessible building entrance, in well-lit areas clearly visible from the front door, and on the same lot as the use being served. Facilities may be indoors or outdoors. If indoors, access shall be available to the public during business hours, at a minimum.
 - 1. Bicycle parking and access facilities may be placed in vehicle parking areas but shall not take the place of a required vehicle parking space or conflict with internal parking aisles.
 - 2. A minimum of five (5) bike parking spaces shall be provided.

December 2, 2023

- B. *Connection to front door.* A pedestrian-accessible walkway shall be available between the outdoor bicycle parking area and the primary building entrance. Public sidewalks may be used to meet this requirement. An aisle width of at least five (5) feet shall be provided adjacent to any bicycle parking facilities to allow for maneuvering.
- C. *Bike racks.* Designs of bicycle racks, docks, posts, and lockers are encouraged to be decorative, unique, and appropriate to the surrounding area or related to the use being served, and shall be maintained in good repair. They will allow a bicycle to be locked to a structure, attached to the pavement, building, or other permanent structure, with two (2) points of contact to an individual bicycle frame.
- D. *Administrative Departure.* Required bicycle parking may be reduced by an Administrative Departure when it is demonstrated that the level of bicycle activity at that location warrants a different amount. In no case shall fewer than two (2) spaces be provided.

ARTICLE 8 – STREET DESIGN

Section 8.01. Purpose and Intent.

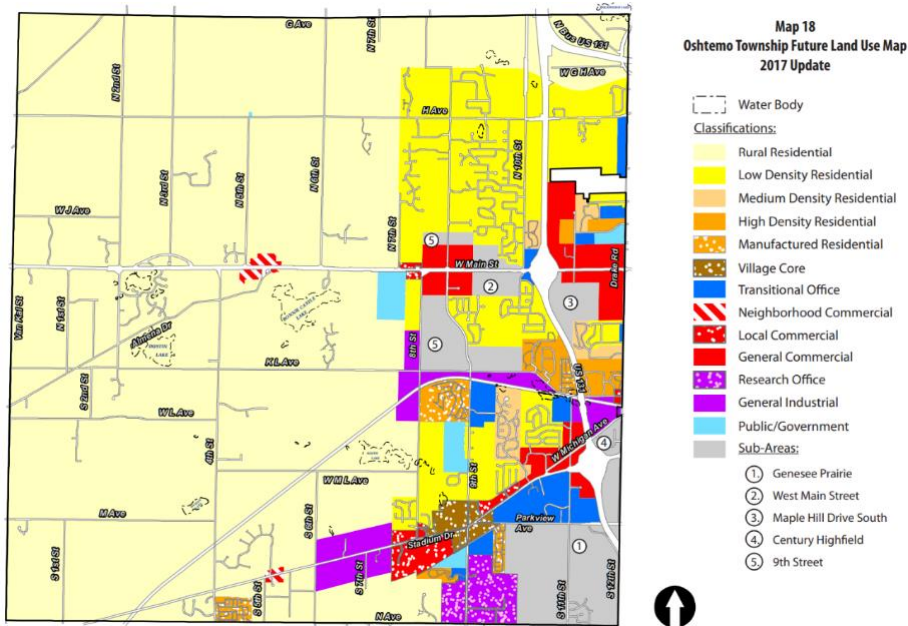
Land use and transportation are inextricably linked. Land uses generate the need for travel and connectivity, and their arrangement dictates travel choices. Compact settings, with a variety of proximate land uses connected by streets that accommodate a variety of modes, will have a higher portion of trips made by walking, biking or transit. Areas with more dispersed settlement patterns require longer trips and are more auto-dependent. The need to consider context in transportation design has long been recognized. The AASHTO “Green Book” includes two place types, rural and urban.

Street design best practices acknowledge that many factors influence travel speed, crashes, and how a street can support, or conflict, with a community’s vision – affecting economic development, quality of place, and even residents’ physical activity. The Federal Highway Administration has developed “Proven Safety Countermeasures” to reduce roadway fatalities and serious injuries. These strategies support self-regulating design, an approach focused on managing speed, which in turn improves safety. Tactics include reducing street width, carefully setting design speed, road diets, pavement markings and signs, street trees and landscaping, and other tactics that manage driver behavior and street operations.

Oshtemo Township is committed to ensuring the transportation system which serves its residents, businesses, and visitors operates safely and efficiently. Due to the dramatic contrast in the township’s character from rural residential to intense commercial and residential at the edge of the City of Kalamazoo, we must consider context in street design. In addition, streets serve a variety of different purposes. Street classification allows for a clearer definition of street hierarchy and different modal types that we should be considered in design.

Section 8.02. Context Zones.

- A. For the purposes of this Streets Ordinance, there are two (2) context zones: “Urban” and “Rural”. The Urban context zone is comprised of all portions of the township that are not classified as “Rural Residential” on the Oshtemo Township 2017 Future Land Use Map. All other lands identified as “Rural Residential” on the Future Land Use Map are in the Rural context zone.



- B. All streets shall meet the design requirements of this Ordinance within the Urban or Rural context zone as assigned, except as noted in Section 8.04.A.

Section 8.03. Types of Streets.

- A. The street network shall consist of three main types of streets: Arterial, Collector, and Local Road.
 - 1. Arterials are the basis around which the circulation system is designed. Arterials are main streets with through movements. Delivery trucks, personal vehicles, transit, pedestrians, and bicyclists use Arterials. All Arterials shall be public.
 - 2. Collectors provide a connection between Arterials and Local Streets, carrying traffic with trips ending in a specific commercial or residential areas. Collectors may intersect with any street type. All modes of travel use Collectors to get to their destination. Collectors shall be public, unless the criteria in Section 8.06.C. is met.
 - 3. Local Streets provide direct property access to homes, businesses, parking, loading, or other service areas of a site. Local Streets may intersect with any street type, but most often Collectors. Intended to be slow-moving streets where the presence of pedestrians and bicyclists is expected. Local Streets may be public or private.

Section 8.04. Reserved.

Section 8.05. Public Streets.

- A. A public street under Road Authority jurisdiction shall follow the design requirements of that Road Authority (see Table 8.05 Public Streets Standards).
- B. Upon receipt of a developer’s preliminary conceptual plan, the Planning Director will ask if a public street is desired. If the answer is in the affirmative, then staff will determine if the project is in the Urban context zone. If it is, then Township staff shall do the following:
 - 1. Evaluate whether the conceptual plan and design requirements of the Road Authority align with Urban context characteristics, advance the stated purpose and intent of this Ordinance and its provisions, and support self-enforcing design. If it is determined that Road Authority standards support the Township’s desired outcomes, then no further action is required.
 - 2. If the Road Authority’s default design requirements do not support the Urban context then, with the consent of the developer, the Public Works Director will seek design exceptions in accordance with the Road Authority’s rules for those items that would advance the Intents of this Ordinance, the Oshtemo Township Comprehensive Master Plan, and Go! Green Oshtemo Plan. Costs associated with design exception review process will be at the Township’s expense.
 - 3. A report will be prepared at the conclusion of the design exception process by the Public Works Director and transmitted to the Township Board. The report shall contain information concerning the requested design exception(s), associated costs, whether exceptions were granted or denied, and if denied the justification for denial.

Table 8.05 Public Streets Standards								
		Urban				Rural		
RCKC Designation		Commercial	Commercial	Commercial	Commercial or Residential	County Primary	County Primary	County Local
Oshtemo Township Designation		Arterial	DDA	Collector	Local	Arterial	Collector	Local
Parameters	ROW width	100'	66'	66'	66'	100'	100'	66'
	ROW w/boulevard	120'	n/a	100 – 120'	100 – 120'	n/a	n/a	n/a
	Cul de sac ROW	140'	140'	140'	140'	140'	140'	140'
	Target speed (mph)	25 – 55 mph			25 – 55 mph	25 – 55	25 – 55	25 – 55
	Surface	paved	paved	paved	paved	paved	paved	paved
	Drainage	curb & gutter	curb & gutter	curb & gutter	c & g or valley	ditches	ditches	ditches
	Grade	See Section XX.XX						
Street	Lane width w/valley gutter	n/a			12'	n/a	n/a	12'
	Valley gutter	n/a			2'	n/a	n/a	2'
	Shoulder	n/a			2'			
	Lane width w/curb	16'			n/a			
	Curb and pan	2'			n/a			
	Vehicle lane width	16'			11' – 16'	12'	11'	11'
	Cul de sac radius	50'			50'			
Green Infrastr.	Median	-	11'6"	-	11'6"	-	-	11'6"
	Minimum parkway	10'	w/ sidewalk	6'	6'	12'	10'	6'
	Tree placement	n/a	6' from curb		n/a			
	Tree spacing	n/a						
Non-Motorized	Bike lanes	n/a						
	Shared use path	12'	n/a	12'		12'		
	Sidewalk placement	1' from ROW				1' from ROW		
	Sidewalk width	6'	14'	6'	5'	5'		
	Mid-block crossing	n/a				n/a		
Streetscape	On-street parking	n/a	8'	n/a				
	Decorative crosswalk	n/a	red brick paint	n/a				
	Traffic calming	n/a						
	Street furniture	n/a	6' from curb	n/a				
	Outdoor dining	n/a						
	Curb extensions	n/a	allowed	n/a				
	Decorative lighting	n/a	6' from curb	n/a				
Street art	n/a							
n/a - not applicable or not allowed by Road Authority								

Section 8.06. Private Streets.

- A. *Purpose and Intent.* The purpose of this section is to provide access to residential, nonresidential, and site condominium developments, and as tracts of land are divided, sold, and transferred. The Township determines it is in the best interest of the public health, safety and welfare to regulate the design, construction, improvement, extension, relocation, maintenance, and use of private streets to ensure they provide for the safe passage and maneuverability of emergency vehicles and multiple public and private users; and that such streets are constructed of suitable materials to ensure their durability. Private streets may also help preserve safe and efficient traffic movement by providing reasonable access to public roadways.
- B. *Applicability.* Private streets shall be permitted where there is limited or no opportunity or potential to establish a public street or plat the land, or where the design requirements for creating a public street are particularly incompatible and do not advance the vision and goals of the Oshtemo Township Master Plan and Regulating Plan. The provisions of this Article shall apply to:
1. All private streets designated or constructed on and after the effective date of this Ordinance.
 2. Where access is provided to five (5) or more existing or proposed dwelling units, places of business, lots, parcels, or principal buildings. In commercial areas this may be a service drive or frontage road.
 3. An existing private street, including the portion thereof existing prior to the adoption of this Ordinance, when it is extended by an increase in its length, or if lots or parcels of land with access to the existing private street are added.
- C. *Authorization.* Private streets shall not be constructed, extended or relocated without express written approval by the Public Works Director and Planning Director as deemed necessary attesting that the proposed private street complies with all Township standards (see also Article 9 Administration and Enforcement). If approved as a private street, the Township shall have no obligation or liability for the private street or maintenance thereof.
- D. *Township agreement.* All improvements installed or constructed as required under the terms of this Ordinance shall be made and maintained at the expense of the property owner(s) or developer. The Township may enter into an agreement with the owner/developer of the private street that would also benefit the public and the Township for reasons of additional access, connectivity, and mobility.
- E. *Street Frontage.* All lots and parcels of land with access to a private street shall have frontage on the approved private street right-of-way equal to the minimum lot width requirement of the Zone District in which the lot is located.
1. A parcel with frontage on both a private and public street shall be considered a corner lot.
 2. The driveways for corner lots shall be constructed on the private road, except in the case of an existing lot or structure or where topography, such as wetlands or steep slopes make such access unfeasible or where prohibited by the Road Commission of Kalamazoo County.

Section 8.07. Private Streets Standards.

- A. *Purpose and Intent.* Private streets offer an alternative to public street design standards . It is the intent of these regulations to offer a greater variety of design choices, provide safe facilities for all road users as appropriate, and allow opportunities for placemaking to encourage economic investment.
- B. *Collector streets.* A private Collector street shall meet the construction specifications of RCKC for public streets (base, asphalt mix, etc.) but may be designed in accordance with the standards provided in Table 8.07. for private streets if an easement is provided to the Township which grants public access within the private street right-of-way to ensure the intended function of the Collector is as represented in exchange for the additional design flexibility offered by these standards.
- C. *Local streets.* An engineered drawing shall be prepared for the review of the Public Works Director for a private Local street that includes information on anticipated traffic types and volumes, adjacent land uses, and other pertinent information that would assist in determining if the pavement plans as proposed meet professional standards and will reasonably ensure a durable private street.
- D. *Radii.* Turning radii shall be determined based on Fire Department standards.
- E. *Design standards.* A private street under Township jurisdiction shall follow the design requirements of this Ordinance, particularly Table 8.07 Required Private Streets Standards. However, the Public Works Director is granted the authority to exercise engineering judgement when appropriate to determine the best design approach for unique situations through the Administrative Departure process.

		Urban			Rural			Section
Oshtemo Township Designation		Collector - Commercial	Collector - Residential	Local	Collector - Commercial	Collector - Residential	Local	
Parameters	ROW width	66'	66'	50'	66'	66'	50'	
	ROW w/boulevard	100'	80'	66'	100'	80'	66'	
	Cul de sac ROW	60'	50'		60'	50'		
	Target speed	30 mph		20 mph	35 mph		25 mph	
	Surface	6" HMA		3" HMA	4" HMA	4" HMA	3" HMA	
	Drainage	curb & gutter		gutter or ditch	gutter or ditch		ditch	
	Grade	Up to 5%		Up to 8%	Up to 7%		Up to 10%	Section 7.08
Street	Vehicle lane width	10'-11'		9'-10'	11'	10'-11'	9'-10'	
	Curb and pan	18"		12"	18"		12"	
	Valley gutter	n/a	n/a	2'	2'	2'	2'	
	Shoulder	n/a	n/a	n/a	3'		2'	
	Cul de sac radius	40'		30'	50'		40'	
Green Infrastr.	Median	10'	8'	6'	12'	10'	6'	
	Min. parkway	6'		6'	8'		6'	
	Tree placement	3'			3'		3'	Section 7.09
	Tree spacing	25'-40'			25'-40'			
Non-Motorized	Bike lanes	6'			6'			
	Shared use path	12'			12'			
	Sidewalk placement	Both sides, w/parkway (may be back of curb)			w/parkway		w/parkway	Section 7.10
	Sidewalk width	6'-14'	6'	5'	5'			
	Mid-block crossing	allowed		allowed	allowed		allowed	
On-street parking	allowed		allowed	allowed		allowed		

Table 8.07 Required Private Streets Standards								
		Urban			Rural			Section
Oshtemo Township Designation		Collector - Commercial	Collector - Residential	Local	Collector - Commercial	Collector - Residential	Local	
	Decorative crosswalk	allowed		allowed	allowed	-	-	
	Traffic calming	allowed		allowed	-	-	allowed	
	Outdoor seating	allowed		-	-	-	-	
	Curb extensions	allowed		allowed	-	-	-	
	Decorative lighting	allowed		allowed	-	-	-	
	Street art	allowed		allowed	-	-	-	

Section 8.08 Grading.

- A. *Limits.* It is desirable to work with the land to preserve natural features and slopes rather than cause extensive grading during the development process, while still ensuring safe travel and property access.
 1. The grade of private streets will be related appropriately to the original topography but will not exceed a maximum grade of six percent (6%), unless otherwise granted an exception under Part D of this Section.
 2. Any proposed street approaching an existing or proposed street shall be constructed with the approach profile grade between 0% and 2% for a minimum 50 feet in length, as measured from the edge of the through street. For proposed road grades of 6% to 7%, the 0% to 2% approach profile grade length shall be a minimum 100 feet in length.
 3. The minimum grade for any concrete curb and gutter street shall be 0.75%. The minimum grade for any HMA valley gutter street shall be 1.00%.
 4. A cul-de-sac turnaround shall be constructed to ensure a minimum grade of 1.25% along the flow line of the gutter.

- B. *Curves.* Private streets may curve to provide traffic calming measures so long as road width and other self-enforcing design techniques are used to ensure that travel speeds will remain slow.
 1. Ability to maintain the street for snow plowing, trash removal, deliveries, and access for emergencies services must be demonstrated during the design process.
 2. Horizontal and vertical curve alignment shall be equal to the highest standard that is compatible with topographical details and reasonable ROW width. Vertical curve K Values for sags and crests shall be designed according to AASHTO.

- C. *Relationships.* The interplay of the private street to other properties, facilities, buildings, and even door entries is important for a successful built environment.
 1. Site grading shall not preclude the ability to align with abutting properties for service roads, non-motorized facilities, or other aspects to facilitate the continued growth and development of the community.
 2. All streets shall be arranged to locate as many lots or building sites as possible at, or above, the grades of the streets. Lots or building sites with grades lower than the street may be allowed provided adequate drainage is demonstrated.

3. Curb height and the finished floor of a building shall match where a building is located twenty (20) feet or less from the back of curb.
- D. Administrative Departure. Notwithstanding the preceding, the Public Works Director may allow a maximum grade of up to ten percent (10%) for a private street if it is reasonably determined that such increased grade meets all of the following standards:
 1. The private street will be safe;
 2. The increased grade will not hinder the ability of firefighting equipment, ambulances and other emergency vehicles and personnel to reach all portions of the development;
 3. The private road has a maximum grade of four percent (4%) for a minimum distance of thirty (30) feet from its intersection with a public right-of-way or another private road; and
 4. The developer demonstrates that automobile traffic will be able to easily and safely go up and down the grade at all times of the year, including when ice and snow are present.

Section 8.09. Green Infrastructure.

- A. *Purpose and Intent.* Streetscape improvements are intended to mitigate the effect of the built environment on the natural environment and to support a healthy environment for people. Street trees, in particular, provide shade and visual relief, improve air quality, contribute to noise reduction, calm vehicular traffic, reduce stormwater effects, and improve property values.
- B. *Applicability.* Streetscape improvements shall be applicable to all private street construction or reconstruction projects (non-maintenance), development projects requiring a building permit, and/or where sidewalks or other facilities in the right-of-way are to be installed or replaced. Installation of streetscape improvements, particularly street trees, is highly encourage in the public right-of-way.
- C. The design of the storm water management and drainage facilities should seek to be comparable in function and appearance to common natural drainage systems and runoff patterns, including wetlands. The private street shall be constructed with such storm water drainage easements, storm water runoff, culverts, and drainage contours necessary to ensure adequate drainage and runoff.
- D. All areas disturbed by the construction of the private road shall be provided with topsoil, seeded with perennial grass and protected against erosion.
- E. *Green Infrastructure.* The following rules shall apply.
 1. At least one (1) small tree shall be planted for each twenty-five (25) feet of lot frontage or any fraction over twenty-five (25) feet. As an alternative, one (1) medium or large tree may be planted for each thirty-five (35) feet of lot frontage or for any fraction of forty (40) feet.
 2. Clustering of ornamental, medium, or large trees is permitted when utility conflicts or required clear vision areas prevent compliance with minimum spacing requirements.
 3. In addition to the required plantings within the right-of-way, the remainder of the tree lawn shall be landscaped with grass, ground cover, shrubs, and other organic landscape materials.
 4. A minimum 6-foot-wide parkway (tree lawn) shall be provided on Collector and Local streets. A minimum 10-foot-wide parkway will be provided on those properties fronting Arterial streets.

5. Street trees located along the same street frontage shall be aligned in a consistent row along the street.
6. Where space permits, cul-de-sacs shall be designed with a central island where vegetation can be preserved/established.
7. Parking lots adjacent to street rights-of-way shall provide shrubs at a ratio of 1.5 shrubs for every one (1) parking space. Shrubs that reach a mature height of at least three (3) feet shall be utilized and they shall be in groupings spaced at least three (3) feet on center to screen the parking lot from the right-of-way.

D. *Soil.* Topsoil per Table 8.07 Streetscape Planting Requirements shall be used for tree installation.

Table 8.07 Streetscape Planting Requirements				
Plant Material		Minimum Plant Size	Spacing on Center (ft.)	Soil Volume (cu. ft.)
Canopy/Shade Trees	Small	1.5 in caliper	25 ft.	250
	Medium	2.5 in caliper	35 ft.	500
	Large	2.5 in caliper	45 ft.	750
Ornamental Trees		2.0 in caliper	15 ft.	200
Evergreen Trees		5.0 ft height	15 ft.	200
Shrubs		3.0 ft height	3 ft.	N/A

E. *Root Barrier.* Root barriers shall be installed within areas where there is less than seven (7) feet between the back of curb and the sidewalk to prevent root penetration and destruction of curbs and sidewalks.

F. *Maintenance and Replacement by Property Owner.*

1. The property owner shall be required to maintain street trees for two (2) years after the trees are planted and replace any tree which fails to survive or does not exhibit normal growth characteristics of health and vigor, as determined by the Planning Director. The two (2) year period after the approval of the Planning Director shall begin at each planting and shall recommence as trees are replaced.
2. All street trees must be maintained with a minimum clearance height of 15 feet to the first branch at maturity.

G. *Administrative Departures.* An Administrative Departure that reduces, modifies, or eliminates landscape improvements may be granted where:

1. The tree lawn is too narrow or is otherwise not conducive to the planting of trees or other streetscape improvements; or
2. If there are trees growing along but not within the right-of-way which are intended to be preserved and comply with the intent of these regulations; or
3. The affected area is small in comparison to the overall project, which generally complies with the regulations of this Article, and the modification does not materially impair the public realm.

Section 8.10. Sidewalks.

- A. Sidewalks required on all streets in the designated urban context zone on both sides of the street.
- B. Sidewalks will be a minimum of six (6) feet wide on arterials and urban commercial connectors and five (5) feet wide on residential or lower volume streets.
- C. Sidewalks shall be aligned horizontally and vertically with existing sidewalks on adjacent properties. and designed to maintain the existing direction and flow of storm water and to avoid damming or flooding and meet ADA requirements. Maximum allowable slope is two percent (2%).
- D. *Sidewalks at driveway crossings.* The appearance of the sidewalk shall be maintained across the driveway to indicate that the area traversed by a vehicle remains a part of the pedestrian zone and that pedestrians have the right-of-way. The driveway shall retain the elevation of the sidewalk.
- E. *Detectable warning surfaces.* Detectable warning surfaces shall be installed wherever a sidewalk or shared use path approaches an intersection with a street or driveway.
- F. In Rural areas, a widened-shoulder may substitute for sidewalks on Collector and Local streets.
- G. *Shared use path.* A twelve (12)-foot shared-use path, reflective of locations identified in the Regulating Plan contained in the Oshtemo Township Comprehensive Master Plan and the Non-Motorized Facilities Plan, shall be installed within Arterial corridors and Collector streets.
- H. *Residential development.* Sidewalks not less than five feet in width shall be included within the dedicated non-pavement portion of the right-of-way on both sides of all roads within a subdivision, site condominium, or multi-family development.
- I. *Petition.* Existing residential neighborhoods without sidewalks, and for which public sidewalks are not planned under the Township Master Plan, may petition for sidewalks to be built within the neighborhood.
 - 1. The residents of the neighborhood will be responsible for initiating the petition and, if successful, present the petition to the Reviewing Body for approval.
 - 2. Upon Reviewing Body approval, the streets of the neighborhood will be added to the list of streets to receive sidewalks and the sidewalks will be built at Township expense according to the Township's Prioritization Policy.
 - 3. Residents can petition to establish a special assessment district to pay for the sidewalk.
- J. Within commercial and industrial areas, non-motorized facilities deemed necessary may be installed upon the motion of the Board in accordance with Michigan law with or without the establishment of a special assessment district.
- K. The Township, on the motion of the Board, may, at its expense, proceed with sidewalk improvements to fill gaps in the existing sidewalk system as redevelopment and infill development occurs.

December 2, 2023

- L. The Board may, concurrent with capital improvement projects, construct the sidewalks, other non-motorized facilities, and other appropriate safety improvements along primary pedestrian routes to schools, parks, or other activity destinations upon motion of the Board at the Township's expense. The Township will host public meetings to obtain resident input before committing to the Complete Street elements with the capital improvement projects.
- M. *Administrative departure.* The width and location requirements may be adjusted upon finding that another location would be more appropriate because of the location of utilities, existing landscaping or trees, the location of connecting sidewalks or pathways on adjacent parcels, or other site considerations.

ARTICLE 9 – ADMINISTRATION AND ENFORCEMENT FOR PRIVATE STREETS

Section 9.01. Administration and Interpretation.

The Public Works Director and Planning Director will jointly perform or assign the following responsibilities and authorities:

- A. Administer the street standards;
- B. Interpret the provisions of the street standards;
- C. Modify the street standards where special conditions indicate that such modification will best meet the purpose and intent of this Ordinance and provide an adequate and safe street for all users. If there is a conflict between this Ordinance and the individual requirements, the Public Works Director and Planning Director shall determine which requirements control. Should the Public Works Director and Planning Director differ in their opinions, then the Township Supervisor shall make the final determination.

Section 9.02. Fees.

The Township shall, by resolution of the Township Board, adopt a fee schedule for Private Street Permits. The fees shall be imposed to cover Township administrative costs, as well as engineering review, field inspection, planning review, and legal and other professional services. The Township may also require escrow fees for field inspections. The balance of any escrow amount shall be refunded to the applicant upon final approval. Should the Township's costs exceed the fees submitted and/or the escrow amount, the applicant shall be responsible for payment of such amounts prior to the issuance of the certificate of completion.

Section 9.03. Approval Process.

- A. *Commencement.* No construction shall be commenced on a street (or extension or addition thereto) until and unless all approvals under this Ordinance and any other applicable ordinance sections have been obtained from the Township through the Site Plan Review process as described in Article 65 of the Zoning Ordinance.
- B. *Authority.* Only the Township shall have the authority to approve or deny applications for permits for private street construction. Permits issued by other governmental entities shall not serve as a substitute.
- C. *Existing private streets and permits.* A Private Street Permit shall not be required for the issuance of a building permit for a principal dwelling on an existing lot or site condominium unit which derives its access from a private street existing as of the effective date of this ordinance.
- D. *Process.* Upon receiving plan approval, the applicant may file for a permit for the private street's construction. At such time, the following must be submitted to the Township Clerk:
 1. The applicant shall deposit with the Township Treasurer a sum of money, bank letter of credit or certified check in the amount sufficient to guarantee that the applicant shall perform the terms and conditions of the permit, including the payment of required fees. Upon issuance of certificate of completion any unused portion of the deposit shall be refunded to the applicant.
 2. Proof that the Road Authority and Drain Commission have reviewed the plan. Proof can be in the form of a letter or permit.

3. Proof that an agreement, outlined in Part E, regarding the interconnection, access rights, maintenance and improvements of the right-of-way, has been recorded with the Kalamazoo County Register of Deeds.
- E. *Agreement.* An agreement regarding the interconnection, access rights, maintenance and improvements of the right-of-way and roadway shall be submitted to the Township for review. It will be in a form that will allow it to be recorded with the Kalamazoo County Register of Deeds. Such agreement will address the following:
1. The agreement shall run with the land and specifically address the liability and responsibility of the parties to said agreement to maintain the private street pursuant to the specifications of this article. This shall include but is not limited to provisions for annual maintenance, snow removal, and the eventual repair or reconstruction of the street.
 2. The agreement shall include rules regarding voting rights and the responsibilities of parties to the agreement in relation to road maintenance and improvements.
 3. The agreement shall include detailed legal descriptions of the private street and all properties allowed to use the road.
 4. A method of initiating and financing such road and/or easements improvements in order to keep the road in a reasonably good and usable condition.
 5. A workable method of apportioning the costs of maintenance and improvements; including that required by an extension of the private street.
 6. Easements to the public for purposes of public and private utilities, emergency and other public vehicles for whatever public services are necessary.
 7. A notice that no public funds of Oshtemo Charter Township are to be used to build, repair, or maintain the private street, including road cuts, curbs and gutters that may be required at the entry of the private road onto a public street.
 8. A requirement that all future amendments to the maintenance agreement, easement agreement, master deed and/or deed restrictions shall be provided to the Township and recorded with the Kalamazoo County Register of Deeds.
 9. Parcel numbers and legal descriptions of all parcels that have legal access to the private road easement.
 10. The agreement shall state that "the private street system may be connected to future public or private street networks when stub streets are shown on the approved plan for private roads."
 11. The following statement shall be included in any deed or other instrument of conveyance recorded for any lots or other parcels of land served by a private street: "This property does not abut or front upon a public street. If a public street does not abut or serve the property, the street abutting or serving the property is a private street, and it is therefore not required to be maintained by the Road Commission of Kalamazoo County or Oshtemo Charter Township."
 12. A notice that if repairs and maintenance are not made, the Township Board may bring the road up to the design standards specified in this ordinance and assess owners of parcels on the private street for the improvements, plus an administrative fee in the amount of 20% of the total cost of the improvements.

13. A provision that the owners of any and all of the properties using the street shall not prohibit, restrict, limit, or in any manner interfere with normal ingress and egress and use by any of the other owners. Normal ingress and egress and use shall include use by family, guests, invitees, tradesmen, and others bound to or returning from any of the properties having a right to use the street.
- F. *Review Standards.* Township staff and/or consultants will review the plans and agreements to assure that the request is in keeping with construction standards of this and other Township ordinances, the Township Comprehensive Master Plan, the intent of this Ordinance, compliance with curb cuts and potential connections, street names, and other applicable provisions. The following review standards shall be applied:
 1. The private street will be safe for traffic and pedestrians and advances the Township's priority of Complete Streets.
 2. The proposed development will not adversely affect adjacent uses of properties and shall be designed, constructed and maintained in a manner harmonious with the character of adjacent properties and the surrounding area.
 3. The private street will not place demands upon public services and facilities in excess of their current capacities.
 - G. *Issuance.* Upon receipt of the required funds and information, the Township Clerk shall issue the permit pursuant to the terms established by the Approving Body's resolution.
 - H. *Permit Duration.* A permit for private street construction shall be valid for a period of one year, or such longer period as determined by the Approving Body, from the date of issuance of the required improvements. If the improvements have not been completed within the allotted time frame, then the permit shall be null and void and of no force and effect and all deposits shall be forfeited to Oshtemo Charter Township.
 - I. *Final drawings.* Upon completion of construction of a private street (or addition or modification thereof), at least one complete set of record construction drawings signed by a licensed engineer or contractor shall be submitted to the Township indicating that construction of the private street was observed and found to be generally in compliance with the approved plans and other Township approvals. These drawings shall indicate any changes to the original private street plans previously approved by the Township, the correct location, size, etc. of both preexisting and new utilities shall also be specified.
 - J. *Building permits.* A building permit shall not be issued for any building or structure that derives its access from a private street that is subject to this Ordinance unless one of the following has occurred:
 1. A Private Street Permit has been issued by the Township and the street has been fully completed in accordance with the approved permit; or
 2. The private street has been nearly completed in accordance with the approved permit with the exception of the wearing course of a depth deemed suitable by the Public Works Director, and the applicant has provided the Township with a performance guarantee, cash, or irrevocable letter of credit as described in Part 3, below; or
 3. The applicant for the building permit or owner(s) of the private street right-of-way have provided the Township with cash or irrevocable letter of credit in an amount determined by the Reviewing Body to insure construction of the private street in accordance with the approved

private street permit within one (1) year from the issuance of the building permit. The letter of credit shall contain a provision that the Township shall have the right to access the letter of credit if such letter is not renewed 30 days before the expiration date of the letter.

Section 9.04. Inspections.

- A. All required improvements shall be inspected by the Public Works Director or assignee at various stages of construction as part of the Private Street Permit.
- B. The applicant's engineer shall certify to the Public Works Director, before the final inspection and report thereon are made, that the required improvements were made in accordance with this article and all approved plans.
- C. Upon completion of construction of the leveling course the Public Works Director shall make a final inspection and shall report the results of the final inspection to the Township Clerk in writing. Upon favorable recommendation from the Public Works Director, the Township Clerk shall issue a tentative certificate of completion.
- D. No building permit shall be issued for any lot fronting on a private road unless a tentative certificate of completion has been issued by the Township.
- E. Upon completion of construction of the wearing course the Public Works Director shall make a final inspection and shall report the results of the final inspection to the Township Clerk in writing. Upon favorable recommendation from the Public Works Director, the Township Clerk shall issue a final certificate of completion.

Section 9.05. Maintenance.

- A. *Responsibilities.* A private street shall be continuously maintained in a manner that it does not constitute a danger to public health, safety and welfare. All costs associated with the repair of a private street shall be the responsibility of the individuals and/or the property owners association(s) comprised of land owners served by the street. Maintenance activities shall include, but not be limited to:
 - 1. Road surfaces are to be graded and graveled or paved to assure vehicle transit at all times of the year.
 - 2. Drainage facilities shall be maintained to be open and freely draining.
 - 3. A clear, unobstructed envelope shall be maintained at a minimum height of at least fourteen (14) feet above the entire minimum required road surface.
 - 4. Road signs and traffic control signs, etc. shall be maintained and replaced by the owners/s of the private road.
- B. *Snow Removal.* The individuals and/or the property owners association(s) comprised of land owners served by the street shall be responsible for plowing snow when there is two or more inches of accumulation. The private street shall be kept free of ice so that firefighting and emergency vehicles can access all portions of the private street at all times.
 - 1. Snow and ice must be removed on any sidewalk adjacent to a lot or parcel owned by the landowners within 24 hours after the ice and/or snow has formed or fallen.
 - 2. Snow and ice shall be removed from the entire constructed width and length of the sidewalk.

3. Failure to comply can result in a municipal civil infraction citation and any associated costs or fines.
- C. *Agreement.* The developer, individuals, and/or the property owners association(s) comprised of land owners served by the street shall produce enforceable documents that the Township may call upon at its option. These documents shall provide that, if the private street is not maintained to the requirements of this Section, all owners shall be deemed to have consented to a special assessment district being created by the Township to maintain or upgrade the private street. The agreement shall also provide that, alternately, the Reviewing Body, at its option, can improve and maintain the private street so that it meets the requirements of this Section, and the Township can charge owners for the reasonable costs thereof, and such costs shall be secured by either placing a property lien or by placing the costs thereof on the tax roll.

Section 9.06. Deviations.

- A. When there are practical difficulties or unnecessary hardships in the way of carrying out the strict letter of this Ordinance, such as topographical and other physical characteristics that cause unusual difficulty, the Planning Commission shall have the power to vary or modify the application of the provisions of this Ordinance when it finds that two or more of the following criteria apply:
1. A proposed private road extension or improvement is of a very modest scale affecting not more than three parcels such that full compliance with construction, design and development standards of this Ordinance would be inordinately burdensome as to be unwarranted.
 2. Construction of a new private street or the extension or improvement of an existing private road in conformance with the standards of this section would result in severe degradation to important natural features that may be preserved and protected with a lessening of the standards. Provided, that a safe alternative design will still result from such reduced standards.
 3. That the condition or situation of the specific piece of property for which the variance is sought is not of so general or recurrent a nature as to make reasonably practicable the formation of a general regulation for such conditions or situations.
 4. That by reason of exceptional narrowness in width, breadth, length or shape of a specific piece of property, or by reason of exceptional topographic conditions or other extraordinary situations or conditions of the land, or of property immediately adjoining the property in question the literal enforcement of the requirements of this Ordinance would involve practical difficulties or would cause undue hardship.
 5. That authorizing such a variance would not be of substantial detriment to adjacent property and would not materially impair the intent and purposes of this Ordinance or the public interest. The possibility of increased financial return shall not of itself be deemed sufficient cause for authorizing a variance.
 6. That practical difficulties or unnecessary hardships would occur if the strict letter of this Chapter is observed, provided that public safety is secured.
 7. The granting of such waiver will not adversely affect the purpose or intent of the Oshtemo Comprehensive Master Plan or the provisions of this Ordinance.
- B. The Planning Commission may attach reasonable conditions in granting any deviation from any provision of this article, and the breach of any conditions or the failure of any applicant to comply with the conditions shall void the deviation.

- C. If construction of the road has not commenced within two years from the date that a deviation was granted, then the deviation shall be null and void. The Planning Commission may grant a longer time frame and may grant an extension when so requested by the applicant.

Section 9.07. Administrative Departures.

- A. *Purpose.* Administrative Departures are provided to permit access and/or the development of lots or properties that generally fall within the requirements of this Ordinance, but, due to minor site characteristics or other related conditions, a limited degree of flexibility to meet the spirit and purpose of this Ordinance is appropriate.
- B. It is not intended to be a general waiver or lessening of regulations. Rather, the procedure permits a site-specific plan that is equal to or better than the strict application of a design standard. It is not intended as a substitute for a variance or as a means for relief from requirements of this Ordinance.
- C. *Applicability.* Only those Administrative Departures that are specifically noted may be requested and approved.
- D. *Application Procedure.* Requests for Administrative Departures shall be submitted with the applicable application and shall include the following:
 - 1. Information and materials, as listed in the application form, in sufficient detail to indicate the nature and necessity of the request, and a scaled drawing, if appropriate. Requested Administrative Departures shall be separately listed and clearly noted on the proposed plan.
 - 2. The applicable fee established by resolution of the Approving Body.
- E. *Review Standards.* The Public Works Director shall consider whether the proposed alternative meets the following standards.
 - 1. *Streets Ordinance.* The proposed Departure is consistent with the purpose and intent of the Ordinance, Article, and Section, and the specific requirements and conditions of the Administrative Departure approval criteria,
 - 2. *Area.* The proposed Departure will be compatible with adjacent properties and the neighborhood,
 - 3. *Environment.* The proposed Departure will retain as many natural features of the landscape as possible and/or cause lesser intrusion into the landscape,
 - 4. *Public Facilities.* The proposed Departure will not place a burden on existing infrastructure and services, and
 - 5. *Other.* The Departure request is necessitated by a condition of the site or structure, and not as a means to reduce costs or inconvenience.
- F. *Decision.* The Public Works Director may approve, approve with conditions, or deny the request. If the Public Works Director determines that the extent of the requested Departure(s) requires additional community review and input, the Engineer may refer the application to the Planning Commission for a public hearing. The fee for the Administrative Departure shall then be applied to the costs of application to the Planning Commission.
- G. *Prior to Other Approval.* Decisions on Administrative Departures shall be made prior to consideration of other approvals required by this Chapter.

December 2, 2023

- H. *Appeal.* A decision regarding an Administrative Departure may be appealed to the Zoning Board of Appeals. Individual conditions imposed as part of an Administrative Departure approval cannot be separately appealed.

Section 9.08. Nonconforming Situations.

Private roads, legally constructed prior to the adoption of this article may continue in use subject to the following:

- A. No such road shall be enlarged or extended unless the entire road is brought into compliance with the standards of this Ordinance.
- B. No additional lots or home sites shall be created which are accessed by a nonconforming private road unless the entire road is brought into compliance with the standards of this Ordinance.
- C. Lots lawfully created prior to the adoption of this Streets Ordinance, which are accessed by a nonconforming private road may be used in accordance with the requirements of the Zoning Ordinance.

Section 9.09. Violation and penalties.

Any person who violates any of the provisions of this Ordinance shall be deemed guilty of a misdemeanor and shall be punished by a fine of not more than \$500.00 or by imprisonment in the county jail for a period not to exceed 90 days or by both such fine and imprisonment. Any access which is used in violation of the terms of this article may be abated, restrained, enjoined and prohibited upon the commencement of an appropriate action in the Circuit Court.

Applicable Sections

- Village Form-Based Code Overlay Zone (34.70)
- 9th Street and West Main Overlay Zone (35.50)
- Planned Unit Development (41.80)
- Residential Condominium Development Standards (42.30)
- Open Space Community (43.50)
- Open Space Preservation Residential Development Option (44.50)
- Conditions for Specified Permitted User (48.100)
- Special Uses (49.200 NOTE: It should be Section 49.180)
- C-R: Local Commercial District, Restricted (21.50)
- Access Management Guidelines Section 51.10
- Definition Of A Private Street Or Road Section 2.20
- Subdivision, Site Condominium, And Land Division Ordinance Section 29.000
- Regulation For The Use Of Public Rights-Of-Way By Telecommunications Providers Section 125.000
- Truck Route Ordinance Section 153.000
- Landscaping Section Section 53.60
- Access Management Plan
- Complete Streets Policy

Goals

1. Define road types
2. Clear standard for the design, construction, and maintenance of private roads and private streets
3. Clear and distinct construction standards for properly scaled rural and urban contexts, in commercial and residential districts
4. Connectivity
5. Access management standards
6. Complete Streets for all users and all modes, equal access for all users

Desired Outcomes

1. Network optimization (efficient, effective, and provides ROI)
2. Ensure public health (promote health and well-being), safety (appropriate facilities, lower speed, reduce points of conflict), and welfare (a well-functioning community that provides a high quality of life and serves the common good)
3. Self-enforcing design that prioritizes safety for all users

Supporting Documents

[2003 Access Management Plan](#)

[2018 Complete Streets Policy](#) (Multi-modal, Context Sensitive, Network Connectivity)

[2019 Go! Green Oshtemo](#)

[2011 Oshtemo Township Master Plan](#) (see Chapter 5 Transportation & Community Services)

2022 [Zoning Ordinance and Subdivision Ordinance](#)